FHWA-Indiana Environmental Document

CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM

GENERAL PROJECT INFORMATION

Road No./County:	State Road (SR) 66/Lloyd Expressway/ Vanderburgh County
Designation Number(s):	1900268 and 1900217
Project Description/Termini:	SR 66/Lloyd Expressway Intersections Improvement Project at Vann Avenue (Des. 1900268) and Stockwell Road (Des. 1900217), from 1.8 miles east of United States (US) 41 to 2.7 miles west of Interstate 69 (I-69)

	Categorical Exclusion, Level 2 – Required Signatories: INDOT DE and/or INDOT ESD
	Categorical Exclusion, Level 3 – Required Signatories: INDOT ESD
X	**Categorical Exclusion, Level 4** – Required Signatories: INDOT ESD and FHWA
	Environmental Assessment (EA) – Required Signatories: INDOT ESD and FHWA
	Additional Investigation (AI) – The proposed action included a design change from the original approved environmental document. Required Signatories must include the appropriate environmental approval authority

Approval

N/A
INDOT DE Signature and Date

Drew Paesmore July 28, 2023
INDOT ESD Signature and Date

PATRICK ALLEN CARPENTER Digitally signed by PATRICK ALLEN CARPENTER Date: 2023.08.09 10:35:26 -04'00'
FHWA Signature and Date

Release for Public Involvement

N/A
INDOT DE Initials and Date

ADWP February 2, 2023
INDOT ESD Initials and Date

Certification of Public Involvement

Brian Malone 5/2/2023
INDOT Consultant Services Signature and Date

INDOT DE/ESD Reviewer Signature and Date:

07/28/2023

Name and Organization of CE/EA Preparer: Jennifer Graf/Parsons

Note: Refer to the most current INDOT CE Manual, guidance language, and other ESD resources for further guidance regarding any section of this form.

Part I – Public Involvement

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

	Yes	No
Does the project have a historic bridge processed under the Historic Bridges PA*?		X
If No, then:		
Opportunity for a Public Hearing Required?	X	

**A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.*

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Notice of Entry letters were mailed to potentially affected property owners near the project area on June 10, 2021, notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Entry letter is included in Appendix G-2 to G-3.

An initial draft Public Involvement Plan (PIP) was prepared by the project team on December 6, 2021. The draft PIP was updated to reflect changes in the project and Indiana Department of Transportation's (INDOT'S) public involvement guidance in June 2022. A copy of the updated draft PIP is provided in Appendix G-5 to G-45. The PIP and most of the related public involvement activities include the proposed intersection improvements covered by this environmental document, as well as related projects that are being completed as a combined group of projects called "TheLloyd4U". The related projects are being completed under separate environmental documents.

Stakeholder Meetings: Public involvement activities were initiated early in the project development process with a meeting with local officials held on October 14, 2020. The purpose of the meeting was to present an overview of the TheLloyd4U projects, the project development process, and the purpose of the proposed improvements. The proposed corridor improvements were discussed, as well as the factors impacting the need for the improvements. A summary of the meeting including the list of attendees, handout, and presentation are provided in Appendix G-47 to G-58. Comments received regarding the intersection improvements at Vann Avenue inquired whether they would impact the nearby Walnut Street improvement project (Des. 1801726), which includes a trail. The Walnut Street project is located two blocks south of the proposed intersections improvements; therefore, no impacts are expected. Additionally, questions regarding traffic diversions, specifically northbound (NB) Boeke Road to westbound (WB) SR 66/Lloyd Expressway, were raised.

A virtual local officials briefing was held on March 4, 2021. The purpose of the meeting was to provide an overview of the TheLloyd4U projects, an update of the projects' activities and the projects' schedules. The preliminary designs and traffic operations of the proposed intersection improvements were presented. A summary of the meeting including a list of attendees is provided in Appendix G-59 to G-61. Comments received regarding the intersection improvements at Vann Avenue and Stockwell Road focused on traffic operations.

A virtual stakeholder briefing occurred on March 10, 2021. The purpose of the meeting was to present the preliminary design and traffic operations of the proposed TheLloyd4U intersection improvements. A summary of the meeting including a list of attendees and the presentation slides are provided in Appendix G-62 to G-72. Comments received regarding the intersection improvements at Vann Avenue and Stockwell Road focused on traffic movements, access to businesses, and pedestrian safety.

A hybrid in-person and virtual transportation management plan (TMP) meeting with local stakeholders was held on October 14, 2021. The purpose of the meeting was to discuss the proposed transportation plan and maintenance of traffic plan for the TheLloyd4U projects. A summary of the meeting including a list of attendees is provided in Appendix G-72 to G-73. During the meeting a clarification of the Stockwell Road intersection improvements was requested. The Evansville Fire Department inquired how the improvements would align with the ongoing signal pre-emption project for the city.

A hybrid in-person and virtual TMP meeting with local stakeholders was held on March 24, 2022. The purpose of the meeting was to provide an update of the proposed TheLloyd4U improvements, explain the functionality of the intersection designs, and maintenance of traffic plan during construction. A summary of the meeting including a list of attendees and the presentation slides are provided in Appendix G-74 to G-81. Comments received regarding the intersection improvements at Vann Avenue and Stockwell Road focused on traffic movements, access to businesses, and construction.

Public Involvement Meetings: The first TheLloyd4U public information meetings (PIMs) were held on April 20 and 22, 2021. The PIMs were advertised via Evansville local television stations, press releases in the *Evansville Courier & Press*, project website, e-blasts, direct mailings, and advertisements on Facebook and Twitter. The April 20, 2021 meeting was held virtually and was attended by 73 members of the public. A list of meeting attendees is presented in Appendix G-82 to G-84. The presentation covered the draft purpose, an overview of the project, discussion of various alternatives, and the next steps (Appendix G-105 to G-111). Comments received for this project focused on eliminating traffic signals along SR 66/Lloyd Expressway, eliminating the left-turn at Vann Avenue, and upgrading Boeke Road to accommodate additional traffic coming from Vann Avenue. Comments from the virtual meeting on April 20, 2021 are provided in Appendix G-85 to G-87.

The PIM on April 22, 2021 was held at the Crescent Room at Milestones located at 621 South Cullen Avenue in Evansville. It was an open house format held from 4:30 to 7:00 p.m., with presentations at 5:00 and 6:00 p.m. The presentation was the same as the one given at the April 20, 2021 PIM. A total of 15 people attended the in-person meeting. Sign-in sheets from the meeting are provided in Appendix G-88 to G-91. At the meeting, display boards and a presentation were provided as well as handouts and comment cards, these meeting materials are provided in Appendix G-103 to G-113. A total of four comments were received regarding the Vann Avenue and Stockwell Road intersections. The comments focused on constructing a frontage road from the American Trace Council Boy Scouts of America to Stockwell Road; concerns that the changes at the Vann Avenue intersection will have an adverse effect on businesses, residents, and participant in sport activities at the athletic fields; and eliminating the left-turn from Vann Avenue onto SR 66/Lloyd Expressway will increase traffic on residential side streets, which are not designed to accommodate more traffic. Comments from the April 22, 2021 meeting are provided in Appendix G-92 to G-102.

A second round of TheLloyd4U PIMs was held on March 29 and 31, 2022. The PIMs were advertised via Evansville local television stations, press releases in the *Evansville Courier & Press*, project website, e-blasts, direct mailings, and advertisements on Facebook and Twitter. The goal of the meetings was to share preliminary design concepts for the intersections, answer questions, and gather feedback. The same presentation was given at both meetings, which provided a project overview, project activities update, proposed intersection improvements, right-of-way (ROW) impacts and next steps. Presentation slides, display boards, and the handout are provided in Appendix G-141 to G-151.

The March 29, 2022 PIM was an in-person meeting held at the City View at Sterling Square located at 210 North Fulton Avenue, Evansville. It was an open house format held from 5:00 to 6:30 p.m. with a presentation at 5:30 p.m. A total of 51 people attended the in-person meeting. Sign-in sheets from the meeting are provided in Appendix G-114 to G-121. Comments from the March 29th meeting are provided in Appendix G-122 to G-133. The March 31, 2022 meeting was held virtually and was attended by 87 members of the public. A list of meeting attendees is presented in Appendix G-138 to G-140. A meeting summary including public comments is provided in Appendix G-134 to G-137. The comments for this project focused on eliminating the left-turn from Vann Avenue onto SR 66/Lloyd Expressway; concerns about increasing traffic on residential side streets due to project, traffic lights should be eliminated not added to the corridor; and preference for design improvements that include interchanges and overpasses similar to those north of Indianapolis.

Outreach: Several outreach tools have been implemented for the project including a website (TheLloyd4U.com), Facebook and Twitter profiles, emails and text alerts, and media coverage. The draft PIP (Appendix G-5 to G-45.) describes these outreach tools in detail. This project has been covered by local media such as television stations and the *Evansville Courier & Press.* A summary of project media coverage is provided in Appendix G-158 to G-173. INDOT's public service website www.INDOT4U.com also provides a means for the public to receive information about the project and provide their comments. Public comments received through INDOT4U are provided in Appendix G-152 to G-156.

The project met the minimum requirements described in the current *INDOT Project Development Public Involvement Procedures Manual* which requires the project sponsor to offer the public an opportunity to submit comments and/or request a public hearing. Following release of the draft environmental document for public involvement, copies were posted online and placed at the McCollough Library, Lochmueller Group Evansville Office, INDOT Vincennes District Office and online at: https://thelloyd4u.com. A Legal Notice of Public Hearing (Notice) was sent along with project maps to project stakeholders, including adjacent landowners, elected officials, regulatory agencies, schools, religious institutions, and civic organizations on February 22, 2023 (Appendix G-175 to G-182). The Notice was published in the *Evansville Courier & Press* on February 21 and 28, 2023 (Appendix G-183 to G-185). As advertised, the comment period ended on March 22, 2023.

Public Hearing: A public hearing was held on March 7, 2023, at the Crescent Room at Milestones. Thirty-two people attended the public hearing, consisting of project team members and members of the community (Appendix G-186 to G-189). At the hearing, attendees were provided a welcome letter and handouts (Appendix G-190 to G-193), project display boards were presented (Appendix G-194), and project team members were available before and after the hearing to answer questions. Before the formal hearing procedures, team members discussed the project with attendees. During the hearing, the project team gave a presentation that covered an overview of the Lloyd4U program of projects, overview of the eastside improvements, INDOT's project development process, the project's purpose and need, details about the preferred alternative, maintenance of traffic, project impacts, and how to submit public comments (Appendix G-195 to G-199). There was no new information presented to the public at the hearing based on

environmental concerns. Throughout the project development process there has been no public controversy on environmental considerations.

A total of 18 comments were received during the hearing and comment period, which ended on March 22, 2023. The comments are provided in Appendix G-200 to G-232. The comments focused on the following topics: safety, Vann Avenue, bicyclist and pedestrian connectivity, traffic signals, maintenance of traffic (MOT), and the local road network. Responses to each comment are provided in Appendix G-200 to G-203.

Several comments expressed concern that the proposed displaced left turn (DLT) intersections will result in driver confusion and increased accidents. Safety is INDOT's top priority for all users of both our system and the local roadway systems in Indiana. INDOT works every day to reduce the frequency and severity of crash-related injuries and fatalities, make the roads more efficient, and reduce travel time and congestion. The preferred alternative will improve safety and traffic flow by reducing the rate of crashes and improving the level of service. The hybrid DLT at Stockwell Road will remove mainline left turns at the primary intersection and will also remove the conflict points for those movements. This will reduce both crash severity and frequency, as well as providing traffic operation benefits. INDOT has been installing innovative intersections for more than a decade with measurable safety and mobility benefits. Some of the first innovative intersections in the state were installed east of Evansville on US 231 in Spencer County several years ago, to reduce crashes involving crossing movements.

Several comments expressed concern about eliminating the left turn from Vann Avenue to WB SR66/Lloyd Expressway and the resulting potential impacts on the local roadway network. The comments recommended an overpass at Vann Avenue to provide access to WB SR66/Lloyd Expressway. The Vann Avenue intersection will be reconfigured to be a right-in and right-out intersection. The existing signals will be removed and the median will be closed along the Lloyd Expressway with permanent concrete barrier walls, eliminating the left turn lanes along SR66/Lloyd Expressway and Vann Avenue. South of SR66/Lloyd Expressway near Vann Avenue and Stockwell Avenue, the network of local streets will maintain access for residents to who live and work in the area, including the use of Weinbachn Avenue, Green River Road, and Boeke Road. Creating an overpass at the intersection of SR 66/Lloyd Expressway and Vann Avenue would not only increase costs compared to the preferred alternative, but it would likely require additional right-of-way, a relocation, and/or impacts to a park and trail; therefore, it was dismissed from further consideration.

Concern for the lack of bicycle and pedestrian facilities and lack of planning for future sidewalks or bike lanes within the project area was expressed in one of the comments. During the project development process, INDOT coordinated with local officials from the City of Evansville and Vanderburgh County about impacts to pedestrian facilities. The existing sidewalk and curb ramps along the south approach of Vann Avenue will remain in- place, as well as the curb ramp at the corner of Vann Avenue and Division Street. A pedestrian refuge is proposed for the southern splitter island. It will be reconstructed to current design standards including Americans with Disabilities Act (ADA) accessible standards. The legacy northeast and southeast curb ramps and northeast sidewalk will be removed because the existing pedestrian overpass is now utilized for this movement. There will be no impacts to the adjoining park, trails, and pedestrian overpass.

Residents commented about the installation of new traffic signals along the Lloyd Expressway. These signals will be interconnected and coordinated for maximum efficiency and traffic throughput and monitored on a regular basis by Vincennes District Traffic and Traffic Management.

Other comments addressed MOT during the anticipated two-year construction period. The proposed MOT plan includes phased construction that will allow at least two lanes of eastbound (EB) and WB traffic along the Lloyd Expressway to remain open at all times during construction.

The public involvement activities under the National Environmental Policy Act (NEPA) have been satisfied for this project.

Public Controversy on Environmental Grounds

Discuss public controversy concerning community and/or natural resource impacts, including what is being done during the project to minimize impacts.

At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: INDOT INDOT District: Vincennes

Local Name of the Facility: SR 66 / locally known as Lloyd Expressway

Funding Source (*mark all that apply*): Federal [X] State [X] Local [ ] Other* [ ]

*If other is selected, please identify the funding source:

PURPOSE AND NEED:

The need should describe the specific transportation problem or deficiency that the project will address. The purpose should describe the goal or objective of the project. The solution to the traffic problem should NOT be discussed in this section.

Need: The needs for this project stem from a high rate of crashes at the intersections of SR 66/Lloyd Expressway and Vann Avenue and Stockwell Road, as well as congestion issues at the Stockwell Road intersection. Safety is evaluated using the Road Hazard Analysis Tool (RoadHAT) software. RoadHAT provides results as an Index of Crash Frequency (ICF) and Index of Crash Cost (ICC), which illustrate how the facility is performing. Per the *Indiana Design Manual*, an ICF and ICC of zero or less represents average or below-average crash frequency. Per the 2019 INDOT Roadway Project Application for the SR 66/Lloyd Expressway/Vann Avenue intersection, for the years 2015 to 2017, the ICF and ICC were 4.87 ICF and 3.18 ICC, respectively (Appendix I-29 to I-32). Per the 2020 INDOT Roadway Project Application for the SR 66/Lloyd Expressway/Stockwell Road intersection, for the years 2014 to 2016 the ICF and ICC were 2.11 and 3.52, respectively (Appendix (I-11, and I-33 to I-34).

Between 2015 and 2017 there were 21 fatal or incapacitating crashes at the SR 66/Lloyd Expressway and Vann Avenue intersection (Appendix I-29 to I-32). The crashes were predominately rear-end with more than two times the amount in the eastbound direction. Based on safety analyses, this can be attributed, in part, to excessive queueing at the light at Vann Avenue during peak hours, and diminished sight lines caused by a crest curve at the Boeke Road interchange situated approximately 0.5 mile west of the intersection.

Traffic capacity is evaluated in terms of Levels of Service (LOS). LOS is a performance measure that represents quality of service, measured on an A – F scale, with LOS A representing a free flow of traffic and LOS F representing a breakdown in flow (e.g., start-and-stop congestion). The project area is within an urban area, therefore the minimum criteria during peak travel hours (i.e., rush hour) is LOS D. Per the 2020 INDOT Roadway Project Application, at the SR 66/Lloyd Expressway and Stockwell Road intersection the following movements are “currently failing (LOS F)” during the PM peak: EB through, EB left, WB through, WB left, southbound (SB) left, and NB left (Appendix I-12 and I-33).

Purpose: The purpose of this intersection improvement project is to reduce the rate of crashes at both intersections and improve the LOS at SR66/Lloyd Expressway and Stockwell Road to a minimum of LOS D in the design year, 2045.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Vanderburgh Municipality: City of Evansville

Limits of Proposed Work: Along SR 66/Lloyd Expressway, from approximately 1.8 miles to 2.3 miles east of US 41, and along Vann Avenue, from Sycamore Street to Division Street (Des. No.1900268).
Along SR 66/Lloyd Expressway, from 2.3 miles east of US 41 to 2.7 miles west of I-69; along Stockwell Road from John Street to approximately 100 feet north of Division Street; and along Division Street from approximately 1,110 feet west of Stockwell Road to Stockwell Road (Des No. 2000217).

Total Work Length: 0.14 Mile Total Work Area: 12.81 Acres

County Vanderburgh Route SR 66/Lloyd Expressway Des. No. 1900268 & 2000217

Is an Interstate Access Document (IAD)[1] required?	Yes[1]	No
		X
If yes, when did the FHWA provide a Determination of Engineering and Operational Acceptability?	Date:	

[1]*If an IAD is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IAD.*

Describe location of project including township, range, city, county, roads, etc. Existing conditions should include current conditions, current deficiencies, roadway description, surrounding features, etc. Preferred alternative should include the scope of work, anticipated impacts, and how the project will meet the Purpose and Need. Logical termini and independent utility also need discussed.

Location: INDOT, with funding from the Federal Highway Administration (FHWA), intends to proceed with an intersections improvement project involving a 0.9-mile section of SR 66/Lloyd Expressway in the City of Evansville, Vanderburgh County, Indiana (Appendix B-1). Specifically, the project is located approximately 1.8 miles east of US 41 and 2.7 miles west of I-69, as shown on the Evansville South and Newburgh, Indiana United States Geological Survey (USGS) topographical 7.5 minute quadrangle maps. It is located in Sections 22, 23, 26 and 27 of Township 6 South, Range 10 West (Appendix B-2).

The project setting is urban. Surrounding properties are a mixture of recreational, commercial, and institutional properties (Appendix B-3). The City of Evansville State Hospital Grounds Park abuts the project area at the southeast corner of SR 66/Lloyd Expressway and Vann Avenue, and the University of Evansville athletic fields are adjacent to the northwest of Division Street and Stockwell Road.

The project area begins along SR 66/Lloyd Expressway at Villa Drive and extends east to Congress Avenue. Project area limits also include Vann Avenue, from Sycamore Street to Division Street; Stockwell Road from John Street to approximately 100 feet north of Division Street; and Division Street from approximately 1,110 feet west of Stockwell Road to Stockwell Road.

Existing Conditions: SR 66/Lloyd Expressway is a divided highway that has three 12-foot wide through lanes in each direction, with variable auxiliary and turn lanes at the signalized intersections, and 2-foot to 4-foot wide paved shoulders. Vann Avenue is a five-lane undivided road with 11-foot wide through, left and right turn lanes, with 2-foot wide shoulders, and curb and gutter. South Stockwell Road is a six-lane undivided road with 12-foot wide through, left and right turn lanes, with 2-foot wide shoulders, and curb and gutter. North Stockwell Road is a five-lane divided road with 12-foot wide through, left and right turn lanes, with 2-foot wide shoulders, and curb and gutter. Division Street is a two-lane road with 11-foot wide travel lanes in each direction, 1-foot wide shoulders and no sidewalks. Existing conditions are shown on aerial photographs and project area photographs in Appendix B-3, and B-5 to B-7.

SR 66/Lloyd Expressway is classified as a Principal Arterial, Vann Avenue south of Lloyd Expressway, and Stockwell Road are Major Collectors, and Division Street and Vann Avenue north of SR 66/Lloyd Expressway are Local roads. As discussed in the Purpose and Need Section, there is a high rate of crashes at both intersections, as well as capacity issues at the Stockwell Road intersection.

The SR 66/Lloyd Expressway and Vann Avenue intersection is signalized. There are dedicated left-turn and right-turn lanes onto Vann Avenue in both the EB and WB directions. Vann Avenue has five lanes at the intersection, consisting of NB and SB through, left-turn, and right-turn lanes, with discontinuous sidewalk, curb, and gutter.

The SR 66/Lloyd Expressway and Stockwell Road intersection is also signalized. SR 66/Lloyd Expressway has dedicated right-turn slip lanes onto Stockwell Road in both the EB and WB directions, as well as left-turn lanes in each direction (two NB and one SB). Stockwell Road has six lanes at the intersection, consisting of two through lanes in each direction, two left-turn lanes, and a right-turn lane, with curb and gutter. There are no pedestrian facilities at the Stockwell Road intersection, including Division Street.

Existing pedestrian facilities include sidewalks along each side of Vann Avenue south of SR 66/Lloyd Expressway, a 10-foot wide asphalt path and overpass east of the intersection, and trails to the southeast abutting State Hospital Grounds Park. The sidewalks have a crosswalk with curb ramps on the south side of the intersection. The overpass is a pedestrian bridge over SR 66/Lloyd Expressway, located approximately 365 feet east of Vann Avenue. This facility includes 320-foot long retaining walls within SR 66/Lloyd Expressway ROW on both the north and south sides. The path connects to the southeastern Vann Avenue sidewalk, a curb ramp at the southeast corner of Division Street and Vann Avenue, and park trails.

There are no existing pedestrian facilities located within the project limits at Stockwell Road or Division Street. Existing overhead lighting is present throughout the project corridor. Stormwater is currently handled by a mixture of curb and gutter, subgrade sewer lines, and roadside ditches, as well as a detention basin at the southeast corner of SR 66/Lloyd Expressway and Stockwell Road.

Preferred Alternative: Right-In/Right-Out (RIRO) Intersection at Vann Avenue (Des. No.1900268) and Hybrid Displaced Left-Turn (DLT) Intersection at Stockwell Road (Des No. 2000217)

The proposed project will reconfigure both intersections to remove left turns (Appendix B-4). The preferred alternative will remove the stoplights and convert the intersection of SR 66/Lloyd Expressway and Vann Avenue to a RIRO intersection. The preferred alternative for the intersection improvement at Stockwell Road will convert the traditional signalized intersection to a hybrid DLT intersection that includes both a displaced left-turn and a boulevard left-turn. The proposed work will also realign and reconstruct Division Street, including pavement removal and full depth pavement construction. Proposed conditions are shown on the graphics in Appendix B-4 and the project plans, Appendix B-8 to B-28.

The preferred alternative for the intersection improvement work at SR 66/Lloyd Expressway and Vann Avenue will remove the existing signals, close the median along SR 66/Lloyd Expressway with permanent concrete barrier walls, eliminate the left turn lanes along SR 66/Lloyd Expressway and Vann Avenue with restriping of pavement markings, and construct new concrete splitter islands at the Vann Avenue approaches. The existing curb lines at all four quadrants of the intersection will be maintained with proposed concrete splitter islands. Access to all properties will remain, see the Community Impacts section for further discussion.

The existing sidewalk and curb ramps along the south approach of Vann Avenue will remain in-place and undisturbed, as well as the curb ramp at the southeast corner of Vann Avenue and Division Street. A pedestrian refuge is proposed for the southern splitter island. The legacy northeast and southeast curb ramps and northeast sidewalk will be removed, because the existing pedestrian overpass is now utilized for this movement. No impacts to the adjoining park, trails, or pedestrian overpass are expected. The existing sidewalk on both sides of Vann Avenue will remain in place along with the curb ramps associated with the east-west pedestrian movement across Vann Avenue. See the Community Impacts section for further discussion.

The preferred alternative for the intersection improvement work at SR 66/Lloyd Expressway and Stockwell Road will include a crossover in advance of the intersection in the EB direction to displace the left-turn lanes along SR 66/Lloyd Expressway to be on the opposite side of the through traffic, a bypass right-turn lane for movements from SB Stockwell Road to WB SR 66/Lloyd Expressway, two proposed signals at the crossover to control the left-turn movements and the bypass right-turn lane, a boulevard left-turn in the WB direction, one proposed signal and a bump-out for turning movements (also known as a “truck loon”) at the boulevard left-turn, modification of the existing signals to accommodate updated traffic movements, and proposed concrete splitter islands to separate opposing directions of traffic. Partial pavement replacement will be done as needed in order to construct the proposed concrete splitter islands and the right slip lane in addition to pavement replacement where the existing concrete median barrier will be demolished. The WB SR 66/Lloyd Expressway left-turn onto SB Stockwell Road will be eliminated and replaced with the proposed boulevard left-turn west of the intersection. The entrance and exit to the private drive for the Boy Scouts of America and American Red Cross properties will be widened, and full depth replacement of Stockwell Road pavement within the project limits is proposed. Existing drives to the athletic fields owned by University of Evansville will be maintained.

There are no existing pedestrian facilities located within the project limits at Stockwell Road, therefore no pedestrian facilities are proposed.

At both intersections, existing guardrail will be upgraded. In addition to the proposed added signals and changes to signal heads, existing streetlights will be moved and/or upgraded. The existing stormwater drainage system at the Vann Avenue intersection will remain in place and be extended. Under the SR 66/Lloyd Expressway median barrier, inlets and pipe extensions will be located under the EB and WB lanes. The stormwater drainage system at the Stockwell Road intersection will include a closed storm sewer system, curb turnouts, approach culverts, roadside ditches, and detention. There will also be new underdrains installed throughout the project limits.

This project will mostly occur within existing, previously-developed ROW. This project will require a total of 0.6 acre of additional permanent ROW, consisting of strips from previously developed commercial properties. No temporary ROW is proposed (see the Right of Way Section for further discussion).

The preferred alternative will permanently alter the traffic patterns at the SR 66/Lloyd Expressway and Vann Avenue intersection by removing the stoplight and left turns; and installing a median along SR 66/Lloyd Expressway through the intersection. The preferred alternative will impact approximately 2.57 acres of terrestrial habitat, including up to 0.5 acre of tree clearing, and 0.080 acre of wetlands. The project will not impact historical or other cultural resources.

The preferred alternative will meet the purpose and need of the project by reducing the rate of crashes at both intersections and improving the LOS at SR66/Lloyd Expressway and Stockwell Road to a minimum of LOS D in the design year, 2045.

Logical Termini/Independent Utility: TheLloyd4U initiative stems from the 2018 *Corridor Study*, which was conducted by INDOT and the Evansville Metropolitan Planning Organization (MPO) (Appendix I-1 to I-12). The *Corridor Study* recommended that the Vann Avenue and Stockwell Road intersections be grouped together and the Burkhardt Road and Cross Pointe Boulevard intersections be grouped together for implementation for the following reasons: their close geographic proximity; to maximize the

traffic flow benefit from the new traffic movements; and coordinating maintenance of traffic (MOT) during construction. The independent utility and logical termini of the two projects was evaluated in a memorandum dated June 27, 2022, which is provided in Appendix A-2 to A-3. The memorandum concluded that since the Lloyd Expressway Intersections Improvement Projects at Vann Avenue/Stockwell Road and Burkhardt Road/Cross Pointe Boulevard are separated by 1.5 miles of the Lloyd Expressway, they have their own logical termini and will not restrict the consideration of alternatives for other reasonably foreseeable transportation improvements, either connecting or nearby.

A traffic analysis was conducted on the two Lloyd Expressway Intersections Improvement Projects to assess their independent utility in regard to traffic operations. The traffic analysis evaluated the future 2040 Build and No Build scenarios for each project in the AM and PM peak periods. To assess the independent utility of the two intersection projects, the analysis evaluated the combined 2040 Build scenario of each project (i.e., Vann Avenue/Stockwell Road) and the 2040 No Build scenario of the other project (i.e., Burkhardt Road/ Cross Pointe Boulevard). The following summarizes the results:

- Burkhardt Road WB average number of vehicles exiting the traffic model towards Vann Avenue and Stockwell Road:
  - AM Peak Period: 2040 No Build=3,022 vehicles; 2040 Build=3,024 vehicles; Total difference=2 vehicles
  - PM Peak Period: 2040 No Build=2,364 vehicles; 2040 Build=2,431 vehicles; Total difference=67 vehicles
  - Summary: *More* vehicles depart Burkhardt Road and travel WB towards Stockwell Road in the Build scenario than in the No Build scenario.
- Stockwell Road EB average number of vehicles exiting the traffic model towards Burkhardt Road and Cross Pointe Boulevard:
  - AM Peak Period: 2040 No Build=2,512 vehicles; 2040 Build=2,470 vehicles; Total difference=42 vehicles
  - PM Peak Period: 2040 No Build=3,347 vehicles; 2040 Build=3,316 vehicles; Total difference=31 vehicles
  - Summary: *Less* vehicles depart Stockwell Road and travel EB towards Burkhardt Road in the Build scenario than in the No Build scenario.

The traffic analysis determined that if the Burkhardt Road/Cross Pointe Boulevard Intersections Improvement Project is constructed and no improvements are made are to Vann Avenue and Stockwell Road, there would be a maximum of 67 additional vehicles traveling WB towards Stockwell Road. It is anticipated that some of these additional vehicles would disperse to one of the four access points along the 1.5 miles between the two Intersections Improvement Projects. If all additional 67 vehicles were added to the traffic at Stockwell Road in the PM peak period, they would be insignificant to the traffic operations and would not exacerbate any current operational issues at either Stockwell Road or Vann Avenue. If the Vann Avenue/Stockwell Road Intersections Improvements Project is constructed and no improvements are made to Burkhardt Road and Cross Pointe Boulevard, there would be a decrease in the number of vehicles traveling EB towards Burkhardt Road. These vehicles would be insignificant to the traffic operations and would not exacerbate any current operational issues at either Burkhardt Road or Cross Pointe Avenue. Therefore, the two Intersections Improvement Projects have independent utility and are not dependent on each other for efficient traffic operations.

The study area for this project is located along a 0.8 mile section of Lloyd Expressway between Villa Drive and Congress Avenue. These are rational endpoints that are of sufficient length to address broad environmental concerns associated with the design and construction of the project. The proposed improvements will connect to the existing network of streets and will be constructed within existing ROW except for 0.6 acre of additional permanent new ROW from previously developed areas of commercial properties.

The Corridor Study evaluated conceptual alternatives for the Vann Avenue and Stockwell Road intersections and proposed feasible and reasonable solutions. The proposed improvements will meet the purpose and need of the project by reducing the rate of crashes and improving the levels of service at both intersections. Therefore, the intersection improvements have independent utility and are not dependent on any additional transportation improvements along the corridor. The Lloyd Expressway Intersections Improvement Project at Vann Avenue and Stockwell Road is independent even if no additional transportation improvements in the area are made. This project will not restrict consideration of alternatives for other reasonably foreseeable local and state transportation improvements since it is a reconfiguration of existing intersections within INDOT ROW.

OTHER ALTERNATIVES CONSIDERED:

Provide a header for each alternative. Describe all discarded alternatives, including the No Build Alternative. Explain why each discarded alternative was not selected. Make sure to state how each alternative meets or does not meet the Purpose and Need and why.

Alternatives considered for the project were initially evaluated in the Corridor Study (Appendix I-1 to I-12). This Corridor Study evaluated conceptual alternatives for the Vann Avenue and Stockwell Road intersections and proposed feasible and reasonable solutions. The *Draft Engineer's Report SR 66 Lloyd Expressway Intersection Improvements @ Vann Avenue, Burkhardt Road, Cross Pointe Boulevard, and Stockwell Road*, October 11, 2021, prepared by Parsons continued the evaluation of conceptual alternatives from the Corridor Study and recommended preferred alternatives for the Vann Avenue and Stockwell Road intersections. These alternatives are summarized below and further details are provided in Appendix I-1 to I-12.

No Build Alternative

Des. 1900268 and 2000217: This alternative would leave the Vann Avenue and Stockwell Road intersections in their current condition. This alternative would incur no costs, and it would not impact any environmental resources. However, the rate of crashes at both intersections would not be reduced and the LOS at the Stockwell Road intersection would not improve. The safety and capacity issues would remain and potentially increase. Since this alternative does not meet the purpose and need of the project, it was dismissed from further consideration.

Displaced Left Turn Alternative

Des. 1900268: This alternative, also known as a continuous flow intersection, would provide a dedicated left turn lane on the other side of Vann Avenue with a signal. The left turn traffic would move with the flow of traffic on the SR 66/Lloyd Expressway. Displaced left-turn lanes would essentially eliminate the left turn movement from the SR 66 traffic signal phasing as well as increase storage, which would only reduce the crash rate by 33 percent. Although this alternative meets the purpose and need of the project, the engineering analysis determined that it was a less effective alternative to reduce the crash rate at this intersection compared to the preferred alternative. Therefore, this alternative was eliminated from further consideration at Vann Avenue.

Des. 200021: The displaced left turn alternative was initially identified as the recommended design at the Stockwell Road intersection. Further engineering analysis determined that this alternative would require a larger footprint compared to the preferred alternative, and it would create a two-sided weave conflict between the Green River Road WB entrance ramp and the WB diverted left movement. Therefore, this alternative was eliminated from further consideration.

Bow-Tie Intersection Alternative

Des. 1900268 and 2000217: This alternative would require motorists to make a right turn and pass through a roundabout at both intersections, instead of making left turns from the mainline on to Vann Avenue or Stockwell Road. In order to construct the double roundabouts, this alternative would require more ROW compared to the other alternatives. Although this alternative meets the purpose and need of the project, it was dismissed from further consideration because it would require more ROW than the preferred alternative.

The No Build Alternative is not feasible, prudent or practicable because (*Mark all that apply*):

It would not correct existing capacity deficiencies;	X
It would not correct existing safety hazards;	X
It would not correct the existing roadway geometric deficiencies;	
It would not correct existing deteriorated conditions and maintenance problems; or	
It would result in serious impacts to the motoring public and general welfare of the economy.	
Other (Describe):	

ROADWAY CHARACTER:

If the proposed action includes multiple roadways, complete and duplicate for each roadway.

Name of Roadway: SR 66/Lloyd Expressway at Vann Avenue
Functional Classification: Principal Arterial (Other)
Current ADT: 60,390 VPD (2023) Design Year ADT: 71,893 VPD (2043)
Design Hour Volume (DHV): 6,027 Truck Percentage (%): 3
Designed Speed (mph): 50 Legal Speed (mph): 50

	Existing		Proposed	
Number of Lanes:	9		8	
Type of Lanes:	Through, Auxiliary, Merging, Right-turn, and Left-turn		Through, Auxiliary, Merging, and Right-turn	
Pavement Width:	90-113	ft.	90-118	ft.
Shoulder Width:	2-4	ft.	2-4	ft.
Median Width:	2	ft.	2	ft.
Sidewalk Width:	N/A	ft.	N/A	ft.

Setting:	X	Urban		Suburban		Rural
Topography:	X	Level		Rolling		Hilly

Name of Roadway: SR 66/Lloyd Expressway at Stockwell Road
Functional Classification: Principal Arterial (Other)
Current ADT: 54,348 VPD (2023) Design Year ADT: 64,700 VPD (2043)
Design Hour Volume (DHV): 5,890 Truck Percentage (%): 3
Designed Speed (mph): 50 Legal Speed (mph): 50

	Existing		Proposed	
Number of Lanes:	7-9		7-11	
Type of Lanes:	Through, Right-turn, and Left-turn		Through, Right-turn, J-turn, Slip, Merging and Crossover	
Pavement Width:	112-161	ft.	112-168	ft.
Shoulder Width:	2	ft.	2	ft.
Median Width:	2	ft.	2-30	ft.
Sidewalk Width:	N/A	ft.	N/A	ft.

Setting:	X	Urban		Suburban		Rural
Topography:	X	Level		Rolling		Hilly

Name of Roadway: Vann Avenue
Functional Classification: Local Road
Current ADT: 10,361 VPD (2023) Design Year ADT: 12,353 VPD (2043)
Design Hour Volume (DHV): N/A* Truck Percentage (%): N/A*
Designed Speed (mph): 30 Legal Speed (mph): 30
**Not available for this local roadway*

County Vanderburgh Route SR 66/Lloyd Expressway Des. No. 1900268 & 2000217

	Existing		Proposed	
Number of Lanes:	5		3-4	
Type of Lanes:	Through, Right-turn, and Left- turn		Through and Right-turn	
Pavement Width:	54	ft.	54	ft.
Shoulder Width:	2	ft.	2	ft.
Median Width:	8	ft.	8-20	ft.
Sidewalk Width	6	ft.	0*-6	ft.

**Legacy sidewalk to be removed, see Project Description (Preferred Alternative) and Community Impacts sections.*

Setting:	X	Urban		Suburban		Rural
Topography:	X	Level		Rolling		Hilly

Name of Roadway: Stockwell Road
Functional Classification: Major Collector
Current ADT: 12,205 VPD (2023) Design Year ADT: 14,529 VPD (2043)
Design Hour Volume (DHV): 1,414 Truck Percentage (%): 3
Designed Speed (mph): 35 Legal Speed (mph): 35

	Existing		Proposed	
Number of Lanes:	5-6		5-6	
Type of Lanes:	Through, Right-turn, and Left-turn		Through, Right-turn, and Left-turn	
Pavement Width:	83-85	ft.	83-88	ft.
Shoulder Width:	2	ft.	2	ft.
Median Width:	7.5	ft.	7.5	ft.
Sidewalk Width:	N/A	ft.	N/A	ft.

Setting:	X	Urban		Suburban		Rural
Topography:	X	Level		Rolling		Hilly

Name of Roadway: Division Street
Functional Classification: Local Road
Current ADT: 12,205 VPD (2023) Design Year ADT: 14,529 VPD (2043)
Design Hour Volume (DHV): 1,414 Truck Percentage (%): 3
Designed Speed (mph): 30 Legal Speed (mph): 30

	Existing		Proposed	
Number of Lanes:	2		2	
Type of Lanes:	Through		Through	
Pavement Width:	24	ft.	24	ft.
Shoulder Width:	1	ft.	1	ft.
Median Width:	N/A	ft.	N/A	ft.
Sidewalk Width:	N/A	ft.	N/A	ft.

Setting:	X	Urban		Suburban		Rural
Topography:	X	Level		Rolling		Hilly

BRIDGES AND/OR SMALL STRUCTURE(S):

If the proposed action includes multiple structures, complete and duplicate for each bridge and/or small structure. Include both existing and proposed bridge(s) and/or small structure(s) in this section.

Structure/NBI Number(s): N/A Sufficiency Rating: N/A
(Rating, Source of Information)

	Existing		Proposed	
Bridge/Structure Type:				
Number of Spans:				
Weight Restrictions:		ton		ton
Height Restrictions:		ft.		ft.
Curb to Curb Width:		ft.		ft.
Outside to Outside Width:		ft.		ft.
Shoulder Width:		ft.		ft.

Describe impacts and work involving bridge(s), culvert(s), pipe(s), and small structure(s). Provide details for small structure(s): structure number, type, size (length and dia.), location and impacts to water. Use a table if the number of small structures becomes large. If the table exceeds a complete page, put it in the appendix and summarize the information below with a citation to the table.

There are no bridges or structures/culverts over 36-inches in diameter within the project area. Drainage work is limited to regrading and replacement of existing storm sewer structures where needed, including several existing drainage inlets that are 30 inches or less in diameter. None of the structures are historic, and they do not have numbers from INDOT's Bridge Inspection Assessment System (BIAS). Tables of structures for the preferred alternative are provided in Appendix B-16, and B-27 to B-28.

The drainage work will connect the existing system of roadside ditches and subgrade storm sewers. No streams will be impacted. A total of approximately 0.080 acre of wetlands impacts is needed to accommodate the wider intersections/added turn lanes; see the Ecological Section for further discussion.

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?		X
Is a temporary roadway proposed?		X
Will the project involve the use of a detour or require a ramp closure? (describe below)		X
Provisions will be made for access by local traffic and so posted.	X	
Provisions will be made for through-traffic dependent businesses.	X	
Provisions will be made to accommodate any local special events or festivals.	X	
Will the proposed MOT substantially change the environmental consequences of the action?		X
Is there substantial controversy associated with the proposed method for MOT?		X
Will the project require a sidewalk, curb ramp, and/or bicycle lane closure? (describe below)	X	
Provisions will be made for access by pedestrians and/or bicyclist and so posted (describe below).	X	

Discuss closures, detours, and/or facilities (if any) that will be provided for maintenance of traffic. Any known impacts from these temporary measures should be quantified to the extent possible, particularly with respect to properties such as Section 4(f) resources and wetlands. Discuss any pedestrian/bicycle closures. Any local concerns about access and traffic flow should be detailed as well.

Improvements to Vann Avenue and Stockwell Road will be constructed together due to the proximity of the intersections. The proposed MOT plan includes phased construction that will allow at least two lanes of EB and WB traffic along SR 66/Lloyd Expressway to remain open at all times. The first phase of MOT will close the outside through lane in each EB and WB direction on SR 66/Lloyd Expressway and shift traffic to the inside lanes. There will be two inside through lanes in the EB and WB directions with one dedicated right-turn lane for each direction on SR 66/Lloyd Expressway. The left turn lanes will be closed. The second phase of MOT will close the inside through lanes in the EB and WB directions on SR 66/Lloyd Expressway and shift traffic to the outside lanes. MOT at the intersections will limit turning movements to Vann Avenue and Stockwell Road from SR 66/Lloyd Expressway and

to SR 66/Lloyd Expressway from Vann Avenue and Stockwell Road during different phases.

Construction zone design speeds will be reduced 10 mph from the posted speed limits. Access for all residences and businesses will be maintained throughout construction. Pedestrian facilities will remain open to users, including one detour to accommodate the removal of the legacy curb ramps at the Vann Avenue intersection. Pedestrian users who need to cross Vann Avenue south of SR 66/Lloyd Expressway will be directed to the ADA-compliant intersection of Sycamore Street. The MOT plan will include input obtained from meetings with TMP stakeholders to ensure impacts to the public transit, schools, and community events are minimized.

The Metropolitan Evansville Transit System operates their Walnut Route within the study area (Appendix I-28). There are no transit stops along SR 66/Lloyd Expressway or at the Vann Avenue and Stockwell Road intersections. The Walnut Route uses SR 66/Lloyd Expressway as a connection to the stops on East Walnut Street and East Virginia Street. There will be ongoing coordination with the City of Evansville and the Metropolitan Evansville Transit System via the TMP process to minimize any disruption to transit service. This is included in the Environmental Commitments section.

Early coordination letters were sent to stakeholders on March 2, 2022 (see the Early Coordination section for a list) (Appendix C-1 to C-5). No responses regarding the proposed MOT were received. A summary of the TMP meeting conducted to-date was provided in the Public Involvement section and the records are provided in Appendix G-69 to G-70.

The lane restrictions and local road closures will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences and delays will cease upon project completion.

ESTIMATED PROJECT COST AND SCHEDULE:

	$ 5,131,806	(2022)	Right-of-Way:	$ 612,000	(2023)	Construction:	$ 63,407,586	(2024)
Engineering:	$ 2,572,685	(2023)		$ 500,000	(2024)		$ 46,172,190	(2025)

Anticipated Start Date of Construction: March 2024

The project is part of the Fiscal Year (FY) 2022-2026 EMPO Transportation Improvement Program (TIP), which has been directly incorporated into the FY 2022-2026 Statewide Transportation Improvement Program (STIP). The lead DES number for this contract is 1900308 and includes DES numbers 1900268 and 2000217 by reference with the contract number 42287 (Appendix H-1 and H-2). These estimated costs for engineering, ROW, and construction include the entire bundled contract R-42287.

RIGHT OF WAY:

	Amount (acres)	
Land Use Impacts	Permanent	Temporary
Residential	0.0	0.0
Commercial	0.6	0.0
Agricultural	0.0	0.0
Forest	0.0	0.0
Wetlands	0.0	0.0
Other:	0.0	0.0
Other:	0.0	0.0
TOTAL	0.6	0.0

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition, reacquisition or easements, either known or suspected, and their impacts on the environmental analysis should be discussed.

The existing ROW consists of paved areas, sidewalks, maintained grass areas, wetlands, and ditches. In the vicinity of the SR 66/Lloyd Expressway and Vann Avenue intersection, the existing ROW widths along SR 66/Lloyd Expressway vary from approximately 210 to 295 feet north of the median centerline and from approximately 80 to 110 feet south of the median centerline. The existing ROW width along North Vann Avenue is approximately 250 feet from the road center line. The existing ROW width along South Vann Avenue is approximately 45 feet from the road centerline.

In the vicinity of the SR 66/Lloyd Expressway and Stockwell Road intersection, the existing ROW widths along SR 66/Lloyd Expressway vary from approximately 110 to 315 feet north of the median centerline and from approximately 90 to 185 feet south of the median centerline. The existing ROW width along North Stockwell Road is approximately 260 feet from the road center line. The existing ROW width along South Stockwell Road is approximately 50 feet from the road centerline.

The ROW limits will remain mostly the same with an exception of an area west of Stockwell Road along the south side of SR 66/Lloyd Expressway where the ROW limits will extend southward approximately 25 feet. The project requires approximately 0.6 acre of permanent ROW for grading purposes. No temporary ROW is required. The impacts are limited to strips of ROW from commercial properties owned by the Boy Scouts of America and the American National Red Cross (Appendices B-21 to B-22, and I-27).

If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A - EARLY COORDINATION:

List the date(s) coordination was sent and all resource agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.

Early coordination letters (ECL) were sent on March 2, 2022, (Appendix C-1 to C-5). The listed agencies are summarized below.

Agency	Date Sent	Date Response Received	Appendix
FHWA	3/2/2022	No response received	N/A
Indiana Department of Natural Resources, Division of Fish and Wildlife (IDNR-DFW)	3/2/2022	3/31/2022	C-6 to C-7
Indiana Geological and Water Survey (IGWS)	3/2/2022	4/21/2022	C-15 to C-17
National Park Service	3/2/2022	No response received	N/A
US Department of Housing and Urban Development (HUD)	3/2/2022	No response received	N/A
US Army Corps of Engineers (USACE)	3/2/2022	No response received	N/A
INDOT Vincennes District Office	3/2/2022	No response received	N/A
INDOT Office of Aviation	3/2/2022	3/8/2022	C-14
INDOT Utilities and Rail Office	3/2/2022	No response received	N/A
EMPO	3/2/2022	No response received	N/A
Metropolitan Evansville Transit System	3/2/2022	No response received	N/A
Vanderburgh County Commission President	3/2/2022	No response received	N/A
Vanderburgh County Council President	3/2/2022	No response received	N/A
Vanderburgh County Council Personnel Chair	3/2/2022	No response received	N/A
Vanderburgh County Health Department	3/2/2022	No response received	N/A
Evansville Vanderburgh School Corporation, Superintendent	3/2/2022	No response received	N/A
Evansville Vanderburgh School Corporation Bus Transportation	3/2/2022	No response received	N/A
Evansville Fire Department Administration	3/2/2022	No response received	N/A
Evansville Police Department	3/2/2022	No response received	N/A
City of Evansville Mayor	3/2/2022	No response received	N/A
Vanderburgh County Surveyor	3/2/2022	3/7/2022	C-8 to C-11
Vanderburgh County Highway Superintendent	3/2/2022	No response received	N/A
Vanderburgh County Building Commissioner, Local Floodplain Administrator	3/2/2022	No response received	N/A
City of Evansville Stormwater Coordinator/MS4	3/2/2022	No response received	N/A
City of Evansville Engineer	3/2/2022	No response received	N/A
City of Evansville Parks and Recreation	3/2/2022	No response received	N/A
City of Evansville Transportation Executive Director	3/2/2022	No response received	N/A
City of Evansville City Councilor, Ward 1	3/2/2022	No response received	N/A
City of Evansville City Councilor, Ward 3	3/2/2022	No response received	N/A
Evansville Convention and Visitors Bureau Commission	3/2/2022	3/25/2022	C-12 to C-13
Evansville State Hospital	3/2/2022	No response received	N/A
Harper Elementary School	3/2/2022	No response received	N/A
Harrison High School	3/2/2022	No response received	N/A
University of Evansville	3/2/2022	No response received	N/A
Ascension St. Vincent Evansville	3/2/2022	No response received	N/A
Deaconess Gateway Hospital	3/2/2022	No response received	N/A
Catholic Diocese of Evansville	3/2/2022	No response received	N/A

All applicable recommendations are included in the Environmental Commitments section of this CE document.

SECTION B – ECOLOGICAL RESOURCES:

	Presence	Impacts	
		Yes	No
Streams, Rivers, Watercourses & Other Jurisdictional Features			
Federal Wild and Scenic Rivers			
State Natural, Scenic or Recreational Rivers			
Nationwide Rivers Inventory (NRI) listed			
Outstanding Rivers List for Indiana			
Navigable Waterways			

Total stream(s) in project area: 0.0 Linear feet Total impacted stream(s): 0.0 Linear feet

Stream Name	Classification	Total Size in Project Area (linear feet)	Impacted linear feet	Comments (i.e. location, flow direction, likely Water of the US, appendix reference)

Describe all streams, rivers, watercourses and other jurisdictional features adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if the streams or rivers are listed on any federal or state lists for Indiana. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area (Appendix B-3), and the Red Flag Investigation (RFI) report (Appendix E-1 to E-10), there are two streams, rivers, watercourse, or other jurisdictional features within the 0.5-mile search radius. There are no streams, rivers, watercourses, or other jurisdictional features within or adjacent to the project area, which was confirmed by the site visits on June 15 to 18, 2021 by Parsons. Therefore, no impacts are expected.

A Waters of the US (WOTUS) Report was approved by INDOT Ecology and Waterway Permitting Office (EWPO) on August 1, 2022. Please refer to Appendix F-3 to F-30 for the WOTUS Report. It was determined there are no streams, rivers, watercourses, or other jurisdictional features within or adjacent to the project area. The USACE makes all final determinations regarding jurisdiction.

Non-Jurisdictional Features: There are three roadside ditches (RSD) totaling approximately 2,179 linear feet within the project area (Appendix F-23, F-24, and F-26). They do not have either an ordinary highwater mark (OHWM) or wetland characteristics therefore, they are considered to be non-jurisdictional features.

	Presence	Impacts	
		Yes	No
Open Water Feature(s)			
Reservoirs			
Lakes			
Farm Ponds			
Retention/Detention Basin			
Storm Water Management Facilities			
Other:			

Describe all open water feature(s) identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area (Appendix B-3), and the RFI report (Appendix E-1 to E-10), there are three lakes within the 0.5-mile search radius. There are no open water features within or adjacent to the project area, which was confirmed by the site visits on June 15 to 18, 2021 by Parsons.

A WOTUS Report was approved by INDOT Ecology and Waterway Permitting Office (EWPO) on August 1, 2022. Please refer to Appendix F-3 to F-30 for the WOTUS Report. It was determined that there are no open water features within or adjacent to the project area. The detention basin at the southeast corner of SR 66/Lloyd Expressway and Stockwell Road is an emergent wetland, identified as Wetland 7, in the WOTUS Report. The USACE makes all final determinations regarding jurisdiction. See the Wetlands

section for further discussion.

Responses to early coordination did not include any recommendations regarding open water features.

	Presence	Impacts	
		Yes	No
Wetlands	X	X	

Total wetland area: 0.405 Acre(s) Total wetland area impacted: 0.080 Acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments (i.e. location, likely Water of the US, appendix reference)
Wetland 1	Emergent	0.099	0.005	Located within the roadside ditch along the northside of the SR 66/Lloyd Expressway and approximately 650 feet west of Stockwell Road. Likely a water of the State. (Appendix F-23)
Wetland 2	Emergent	0.006	0.000	Located within the roadside ditch along the northside of the Division Street and approximately 650 feet west of Stockwell Road. Likely a water of the State. (Appendix F-23 and F-24)
Wetland 3	Emergent	0.026	0.026	Located within the roadside ditch along the southside of the SR 66/Lloyd Expressway and approximately 1,060 feet west of Stockwell Road. Likely a water of the State. (Appendix F-23)
Wetland 4	Emergent	0.097	0.049	Located within the roadside ditch along the southside of the SR 66/Lloyd Expressway and approximately 80 feet west of Stockwell Road. Likely a water of the State. (Appendix F-23 and F-25)
Wetland 5	Emergent	0.010	0.000	Located within the roadside ditch along the northside of the Division Street and approximately 600 feet west of Stockwell Road. Likely a water of the State. (Appendix F-24)
Wetland 6	Emergent	0.012	0.000	Located within the roadside ditch along the northside of the Division Street and approximately 55 feet east of Stockwell Road. Likely a water of the State. (Appendix F-24)
Wetland 7	Emergent	0.155	0.000	Located within the roadside ditch along the southside of the SR 66/Lloyd Expressway and approximately 35 feet east of Stockwell Road. Likely a water of the State. (Appendix F-25 and F-26)

Wetlands (*Mark all that apply*)	Documentation	ESD Approval Dates
Wetland Determination	X	August 1, 2022
Wetland Delineation	X	August 1, 2022
USACE Isolated Waters Determination		

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

Substantial adverse impacts to adjacent homes, business or other improved properties;	
Substantially increased project costs;	X
Unique engineering, traffic, maintenance, or safety problems;	X
Substantial adverse social, economic, or environmental impacts, or	
The project not meeting the identified needs.	

Describe all wetlands identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area (Appendix B-3), the USFWS National Wetland Inventory (NWI) (Appendix F-1), and the RFI report (Appendix E-1 to E-10) there are eight wetlands within the 0.5-mile search radius. There is one mapped wetland within or adjacent to the project area. That number was updated to seven during the site visits on June 15 to 18, 2021 by Parsons.

A WOTUS Report was approved by INDOT EWPO on August 1, 2022. Please refer to Appendix F-3 to F-30 for the WOTUS Report. It was determined that seven wetlands are within or adjacent to the project area. The USACE makes all final determinations regarding jurisdiction. The wetlands are shown on the Field Identified Resources maps in Appendix F-23 to F-26.

Wetland 1: Wetland 1 is an emergent wetland that is approximately 0.099 acre in size. It is located entirely within the roadside ditch along the northside of the SR 66/Lloyd Expressway and approximately 650 feet west of Stockwell Road. Wetland 1 has low species diversity and is located within INDOT's maintained right-of-way. It was classified as a poor-quality wetland. Wetland 1 is likely a water of the State. There will be approximately 0.005-acre of permanent impact to Wetland 1 due to the installation of a right-turn lane. There will be no temporary impacts to Wetland 1.

Wetland 2: Wetland 2 is an emergent wetland that is approximately 0.006 acre in size. It is located entirely within the roadside ditch along the northside of the Division Street and approximately 650 feet west of Stockwell Road. Wetland 2 has low species diversity and is located within the City of Evansville's maintained right-of-way. It was classified as a poor-quality wetland. Wetland 2 is likely a water of the State. There will be no temporary or permanent impacts to Wetland 2 because it is located outside of the construction limits for the project. Therefore, no impacts are expected.

Wetland 3: Wetland 3 is an emergent wetland that is approximately 0.026 acre in size. It is located entirely within the roadside ditch along the southside of the SR 66/Lloyd Expressway and approximately 1,060 feet west of Stockwell Road. Wetland 3 has low species diversity and is located within INDOT's maintained right-of-way. It was classified as a poor-quality wetland. Wetland 3 is likely a water of the State. There will be permanent impacts to all 0.026-acre of Wetland 3 due to the installation of a right-turn lane.

Wetland 4: Wetland 4 is an emergent wetland that is approximately 0.097 acre in size. It is located entirely within the roadside ditch along the southside of the SR 66/Lloyd Expressway and approximately 80 feet west of Stockwell Road. Wetland 4 has low species diversity and is located within INDOT's maintained right-of-way. It was classified as a poor-quality wetland. Wetland 4 is likely a water of the State. There will be approximately 0.049-acre of permanent impact to Wetland 4 due to the installation of a right-turn lane and U-turn area on EB SR66/Lloyd Expressway. There will be no temporary impacts to Wetland 4.

Wetland 5: Wetland 5 is an emergent wetland that is approximately 0.010 acre in size. It is located entirely within the roadside ditch along the northside of the Division Street and approximately 600 feet west of Stockwell Road. Wetland 5 has low species diversity and is located within the City of Evansville's maintained right-of-way. It was classified as a poor-quality wetland. Wetland 5 is likely a water of the State. There will be no temporary or permanent impacts to Wetland 5 because it is located outside of the construction limits for the project. Therefore, no impacts are expected.

Wetland 6: Wetland 6 is an emergent wetland that is approximately 0.012 acre in size. It is located entirely within the roadside ditch along the northside of the Division Street and approximately 55 feet east of Stockwell Road. Wetland 6 has low species diversity and is located within the City of Evansville's maintained right-of-way. It was classified as a poor-quality wetland. Wetland 6 is likely a water of the State. There will be no temporary or permanent impacts to Wetland 6 because it is located outside of the construction limits for the project. Therefore, no impacts are expected.

Wetland 7: Wetland 7 is an emergent wetland that is approximately 0.155 acre in size. It is located entirely within the roadside ditch along the southside of the SR 66/Lloyd Expressway and approximately 35 feet east of Stockwell Road. Wetland 7 has low species diversity and is located within INDOT's maintained right-of-way. It was classified as a poor-quality wetland. Wetland 7 is likely a water of the State. There will be no temporary or permanent impacts to Wetland 7 because it is located outside of the construction limits for the project. Therefore, no impacts are expected.

Impacts to Wetlands 1, 3, and 4 cannot be avoided because they are present within areas of the existing SR 66/Lloyd Expressway ROW where turn lanes and a U-turn area will be added. The project will require a USACE Section 404 permit and an IDEM Section 401 Water Quality Certification before impacting these resources. Mitigation for wetlands impacts is not anticipated. There is no practicable alternative to the proposed new construction in wetlands and the proposed action includes all practicable measures to minimize harm to wetlands which may result from such use. FHWA approval of this document will constitute approval of the adverse impacts to wetlands.

Responses to early coordination did not include applicable recommendations regarding wetlands.

	Presence	Impacts	
		Yes	NO
Terrestrial Habitat	X	X	

Total terrestrial habitat in project area: 2.57 Acres Total tree clearing: < 0.5 Acre

Describe types of terrestrial habitat (i.e. forested, grassland, farmland, lawn, etc) adjacent or within the project area. Include whether or not impacts will occur to habitat identified. Include total terrestrial habitat impacted and total tree clearing that will occur. Discuss measure to avoid, minimize, and mitigate if impacts will occur.

Based on a desktop review, site visits on June 15-18, 2021, by Parsons, and the aerial map of the project area (Appendix B-3), habitats within the project area mainly consist of maintained grassy roadsides and clusters of coniferous and deciduous trees. The maintained grassy roadsides are dominated by tall false rye grass (*Schedonorus asundinaceus*), English plantain (*Plantago lanceolata*), red clover (*Trifolium pretense*), yellow sweet clover (*Melilotus officinalis*), and Kentucky blue grass (*Poa pratensis*). The clusters of trees are dominated by silver maple (*Acer saccharinum*), red maple (*Acer rubrum*), green ash (*Fraxinus pennsylvanica*), red mulberry (*Morus rubra*), and crabapple (*Malus sp.*).

Approximately 2.57 acres of terrestrial habitat will be disturbed by this project. A total of 25 trees will be removed and all tree clearing/trimming will take place within 100 feet of paved surfaces. Avoiding impacts to terrestrial habitat is not feasible because it is present within the construction limits of the project, and INDOT needs to improve the intersections (see Purpose and Need section). All tree trimming and clearing activities will be done in the bats' inactive season. The City of Evansville requested that 50 trees be planted to mitigate for the 25 trees removed. INDOT will plant as many trees as practicable within the project area to mitigate for the loss of trees.

IDNR-DFW responded on March 31, 2022, with recommendations to revegetate all bare areas with a mixture of grasses and legumes as soon as possible; and implement erosion and sediment control measures (Appendix C-6 to C-7). All applicable recommendations are included in the Environmental Commitments section of this CE document.

Protected Species

Federally Listed Bats	Yes	No
Information for Planning and Consultation (IPaC) determination key completed	X	
Section 7 informal consultation completed (IPaC cannot be completed)		
Section 7 formal consultation Biological Assessment (BA) required		

Determination Received for Listed Bats from USFWS: NE [ ] NLAA [X] LAA [ ]

Other Species not included in IPaC	Yes	No
Additional federal species found in project area (based on IPaC species list)		X
State species (not bird) found in project area (based upon consultation with IDNR)		X

Migratory Birds	Yes	No
Known usage or presence of birds (i.e. nests)		X
State bird species based upon coordination with IDNR		X

Discuss IDNR coordination and species identified. Describe USFWS Section 7 consultation and determination received for Indiana bat and northern long-eared bat impacts. Discuss if other federally listed species were identified. If so, include consultation that has occurred and the determination that was received. Discuss if migratory birds have been observed and any impacts.

Based on a desktop review and the RFI report (Appendix E-1 to E-10), completed by Parsons on June 13, 2022, the IDNR Vanderburgh County Endangered, Threatened and Rare (ETR) Species List has been checked and is and is available at https://www.in.gov/dnr/nature-preserves/files/np_vanderburgh.pdf. According to the IDNR-DFW early coordination response letter dated March 31, 2022 (Appendix C-6 and C-7), the Natural Heritage Program's Database has been checked, and there are three managed lands, one natural community, two state endangered plants, and one state threatened plant within 0.5-mile of the project area. The Division of Natural Resources does not anticipate any impacts to the natural community or endangered/threatened plants as a result of this project. An INDOT 0.5-mile bat review occurred on December 3, 2021, which did not indicate the presence of endangered bat species (Appendix C-21).

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C-19 to C-33). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally endangered northern long-eared bat (NLEB) (*Myotis septentrionalis*). The Monarch butterfly (*Danaus plexippus*) was listed in IPaC as a candidate species and at this time there is no guidance. The project is not anticipated to significantly impact the Monarch butterfly or its habitat. No additional species were generated in the IPaC species list other than the Indiana bat and NLEB.

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB)*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on April 9, 2022, and based on the responses provided, the project was found to "May Effect, but is Not Likely to Adversely Affect" the Indiana bat and/or the NLEB (Appendix C-34 to C-44). INDOT reviewed and verified the effect finding on April 22, 2022 and requested USFWS's review of the finding. No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. Avoidance and Mitigation Measures (AMMs) to be included on this project are General AMM 1, Lighting AMM 1, and Lighting AMM 2. These commitments are included as firm commitments in the Environmental Commitments section of this document.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

Geological and Mineral Resources	**Yes**	**No**
Project located within the Indiana Karst Region		X
Karst features identified within or adjacent to the project area		X
Oil/gas or exploration/abandoned wells identified in the project area		X

Date Karst Evaluation reviewed by INDOT EWPO (if applicable): _____

Discuss if project is located in the Indiana Karst Region and if any karst features have been identified in the project area (from RFI). Discuss response received from IGWS coordination. Discuss if any mines, oil/gas, or exploration/abandoned wells were identified and if impacts will occur. Include discussion of karst study/report was completed and results. (Karst investigation must comply with the current Protection of Karst Features during Planning and Construction guidance and coordinated and reviewed by INDOT EWPO)

Based on a desktop review of the Physiographic divisions of southern Indiana map, the project is located within the Booneville Hills region, which is outside the designated Indiana Karst Region as outlined in INDOT's *Protection of Karst Features during Project Development and Construction* manual. According to the topographic map of the project area (Appendix B-2), the RFI report (Appendix E-1 to E-10), and the *IndianaMap (http://www.indianamap.org/),* there are no karst features identified within or adjacent to the project area. In the early coordination response dated April 21, 2022, IGWS did not indicate that karst features exist in the project area. Their response noted that the project area has a high liquefaction potential, a high potential for bedrock resources, a high potential for sand and gravel resources, a 1% annual chance flood hazard, and that there are no documented active mineral resources extraction sites in the area (Appendix C-15 to C-17). Response from IGWS has been communicated with the designer on May 5, 2022. No impacts are expected.

SECTION C – OTHER RESOURCES

Drinking Water Resources	Presence	Impacts Yes	Impacts No
Wellhead Protection Area(s)			
Source Water Protection Area(s)			
Water Well(s)			
Urbanized Area Boundary	X		X
Public Water System(s)	X		X

	Yes	No
Is the project located in the St. Joseph Sole Source Aquifer (SSA):		X
If Yes, is the FHWA/EPA SSA MOU Applicable?		
If Yes, is a Groundwater Assessment Required?		

Check the appropriate boxes and discuss each topic below. Provide details about impacts and summarize resource-specific coordination responses and any mitigation commitments. Reference responses in the Appendix.

The project is located in Vanderburgh County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA/INDOT Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project, a detailed groundwater assessment is not needed, and no impacts are expected.

IDEM's Wellhead Proximity Determinator website (http://www.in.gov/idem/cleanwater/pages/wellhead/) was accessed on March 12, 2022, by Parsons. This project is not located within a Wellhead Protection Area or Source Water Area. No impacts are expected.

The IDNR Water Well Record Database website (https://www.in.gov/dnr/water/3595.htm) was accessed on March 12, 2022, by Parsons. There are two water wells within approximately 0.5 mile of the project area. The features will not be affected because they are located outside of the construction limits. Therefore, no impacts are expected. Should it be determined during the ROW phase that these wells will be affected, a cost to cure will likely be included in the appraisal to restore the wells.

Based on a desktop review of the INDOT Municipal Separate Storm Sewer Systems (MS4) website (https://entapps.indot.in.gov/MS4/) by Parsons on March 12, 2022, this project is located in an Urban Area Boundary (UAB). An ECL was sent on March 2, 2022, to the City of Evansville Stormwater Coordinator/MS4. The MS4 coordinator did not respond within the 30-day time frame. No impacts are expected.

Based on a desktop review, site visits on June 15 to 18, 2021 by Parsons, the aerial map of the project area (Appendix B-3), and coordination with Evansville Water and Sewer Utility (EWSU), this project is located where there is a public water system. Utility coordination is ongoing (Appendix I-14 to I-17) and there will be no disruption to service. An ECL was sent on March 2, 2022 to the City of Evansville Engineer (Appendix C-1 to C-5). No response was received within the 30-day time frame. No impacts are expected.

Floodplains	Presence	Impacts Yes	Impacts No
Project located within a regulated floodplain			
Longitudinal encroachment			
Transverse encroachment			
Homes located in floodplain within 1000' up/downstream from project			

If applicable, indicate the Floodplain Level?

Level 1 ☐ Level 2 ☐ Level 3 ☐ Level 4 ☐ Level 5 ☐

Use the IDNR Floodway Information Portal to help determine potential impacts. Include floodplain map in appendix. Discuss impacts according to the classification system. If encroachment on a flood plain will occur, coordinate with the Local Flood Plain Administrator during design to insure consistency with the local flood plain planning.

The IDNR Indiana Floodway Information Portal website (http://dnrmaps.dnr.in.gov/appsphp/fdms/) was accessed on March 12, 2022 by Parsons. This project is not located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F-2). Therefore, it does not fall within the guidelines for the implementation of 23 CFR 650, 23 CFR 771, and 44 CFR. No impacts are expected.

	Presence	Impacts	
Farmland		**Yes**	**No**
Agricultural Lands			
Prime Farmland (per NRCS)			

Total Points (from Section VII of CPA-106/AD-1006*) _____
**If 160 or greater, see CE Manual for guidance.*

Discuss existing farmland resources in the project area, impacts that will occur to farmland, and mitigation and minimization measures considered.

Based on a desktop review, site visits on June 15-18, 2021 by Parsons, and the aerial map of the project area (Appendix B-3), there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected.

SECTION D – CULTURAL RESOURCES

	Category(ies) and Type(s)	INDOT Approval Date(s)	N/A
Minor Projects PA	Category B, Types 1, 2, and 3	June 28, 2022	

Full 106 Effect Finding
No Historic Properties Affected ☐ No Adverse Effect ☐ Adverse Effect ☐

Eligible and/or Listed Resources Present
NRHP Building/Site/District(s) ☐ Archaeology ☐ NRHP Bridge(s) ☐

Documentation Prepared (mark all that apply)		ESD Approval Date(s)	SHPO Approval Date(s)
APE, Eligibility and Effect Determination			
800.11 Documentation			
Historic Properties Report or Short Report			
Archaeological Records Check and Assessment			
Archaeological Phase Ia Survey Report	X	June 28, 2022	N/A
Archaeological Phase Ic Survey Report			
Other:			

		MOA Signature Dates (List all signatories)
Memorandum of Agreement (MOA)		

If the project falls under the MPPA, describe the category(ies) that the project falls under and any approval dates. If the project requires full Section 106, use the headings provided. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of the paper(s) and the comment period deadline. Include any further Section 106 work which must be completed at a later date, such as mitigation from a MOA or avoidance commitments.

On June 28, 2022, the INDOT Cultural Resource Office (CRO) determined that this project falls within the guidelines of Category B, Types 1, 2, and 3 under the Minor Projects Programmatic Agreement (Appendix D-1 to D-8):

- Category B-1 is the replacement, repair, or installation of curbs, curb ramps, or sidewalks, including when such projects are associated with roadway work such as surface replacement, reconstruction, rehabilitation, or resurfacing projects, including overlays, shoulder treatments, pavement repair, seal coating, pavement grinding, and pavement marking, under the specified conditions.
- Category B-2 is the installation of new lighting, signals, signage and other traffic control devices under the specified conditions.
- Category B-3 is the construction of added travel, turning, or auxiliary lanes, and shoulder widening under the specified conditions.

An archaeological Phase Ia records check and reconnaissance survey of the project area was conducted by Cultural Resource Analysts, Inc. (Appendix D-9 to D-10), which was approved by INDOT CRO on June 28, 2022 (Appendix D-1 to D-8). Three previously recorded sites were identified within or adjacent to the project area (Appendix D-9 to D-10). Based on the scope of the preferred alternative, it was determined the archaeological sites would either not be disturbed, and/or the previously recorded sites that were investigated warrant no additional investigation, and the project should be allowed to proceed as planned.

No further consultation is required. This completes the Section 106 process and the responsibilities of the FHWA under Section 106 have been fulfilled. If any archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, construction in the immediate area of the find will be stopped, and the INDOT CRO and the IDNR Division of Historic Preservation and Archaeology will be notified immediately.

SECTION E – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

	Presence	Use	
Parks and Other Recreational Land		**Yes**	**No**
Publicly owned park	**X**		**X**
Publicly owned recreation area			
Other (school, state/national forest, bikeway, etc.)			
Wildlife and Waterfowl Refuges			
National Wildlife Refuge			
National Natural Landmark			
State Wildlife Area			
State Nature Preserve			
Historic Properties			
Site eligible and/or listed on the NRHP			

	Evaluations Prepared
Programmatic Section 4(f)	
"De minimis" Impact	
Individual Section 4(f)	
Any exception included in 23 CFR 774.13	

Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the discussion below. Individual Section 4(f) documentation must be included in the appendix and summarized below. Discuss proposed alternatives that satisfy the requirements of Section 4(f). FHWA has identified various exceptions to the requirement for Section 4(f) approval. Refer to 23 CFR § 774.13 - Exceptions.

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and National Register of Historic Places (NRHP) eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, the aerial map of the project area (Appendix B-3), Section 106 documents (Appendix D-1 to D-10), and the RFI report (Appendix E-1 to E-10), there are twenty-six potential Section 4(f) resources located within the 0.5-mile search radius. According to additional research and by site visits on June 15 to 18, 2021, by Parsons, there is one Section 4(f) resource located within or adjacent to the project area: State Hospital Grounds Park. This facility is a park owned and operated by the City of Evansville, therefore, it is a Section 4(f) resource.

The State Hospital Grounds Park abuts the project area at the southeast corner of the Vann Avenue and SR 66/Lloyd Expressway intersection (Appendix B-3). Work for the preferred alternative in this area is limited to existing ROW. A segment of a 1.3-mile loop trail runs along the project area near this intersection and there are nearby amenities including a bench with landscape features. These features are labeled Do Not Disturb on project plans (Appendix B-14). The preferred alternative will not alter access, and the park and its features, including the abutting trail, will remain open during construction. Applicable commitments are included in the Environmental Commitments section of this document. Therefore, no impacts to the State Hospital Grounds Park are expected. The project will not use this resource by taking permanent ROW and will not indirectly use the resource in such a way that the protected activities, features, or attributes that qualify a resource for protection under Section 4(f) are substantially impaired. Therefore, no Section 4(f) use is expected.

Other adjacent and nearby resources include the pedestrian overpass bridge, the University of Evansville Sports Complex, and Roberts Park. The pedestrian overpass bridge crosses the project area east of Vann Avenue, and it connects to ramps and sidewalk abutting the project area (Appendix B-3). However, this facility is not part of a designated greenway trail (source: EMPO Bicycle and Pedestrian Connectivity Master Plan, https://www.walkbikeevv.org/s/EvansvilleMPO_BPCMP_Final_Plan.pdf). Based on their transportation use, the pedestrian overpass and associated ramps and sidewalk are not Section 4(f) resources (see Community Impacts for further discussion of pedestrian facilities). The sports complex located adjacent to the north of the project area is owned by a private university; therefore, it is not a Section 4(f) resource. Additionally, Roberts Park, a public park, is adjacent to the northwest of the initial study area (see Appendix B-3). However, this resource is not located within or adjacent to the preferred alternative's project area; therefore, no use is expected.

ECLs were sent to City of Evansville and Evansville Department of Parks and Recreation on March 2, 2022 (Appendix C-1 to C-5). No responses were received.

Section 6(f) Involvement	**Presence**	**Use**	
		Yes	**No**
Section 6(f) Property	☐	☐	☐

Discuss Section 6(f) resources present or not present. Discuss if any conversion would occur as a result of this project. If conversion will occur, discuss the conversion approval.

The US Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of Section 6(f) properties on the INDOT ESD website revealed a total of 16 projects (at 13 properties) in Vanderburgh County (Appendix I-13). Wesselman Park and Par 3 Golf Course are located 0.25 mile north of the project area (see Appendix B-3). Access will be maintained throughout construction. None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to Section 6(f) resources.

SECTION F – Air Quality

STIP/TIP and Conformity Status of the Project	Yes	No
Is the project in the most current STIP/TIP?	X	
Is the project located in an MPO Area?	X	
Is the project in an air quality non-attainment or maintenance area?	X	
If Yes, then:		
Is the project in the most current MPO TIP?	X	
Is the project exempt from conformity?	X	
If No, then:		
Is the project in the Transportation Plan (TP)?		
Is a hot spot analysis required (CO/PM)?		

Location in STIP: Initial FY 2022-2026

Name of MPO (if applicable): EMPO

Location in TIP (if applicable): 2022-2026 TIP Program of Projects Page 35

Level of MSAT Analysis required?

Level 1a [X] Level 1b [ ] Level 2 [ ] Level 3 [ ] Level 4 [ ] Level 5 [ ]

Describe if the project is listed in the STIP and if it is in a TIP. Describe the attainment status of the county(ies) where the project is located. Indicate whether the project is exempt from a conformity determination. If the project is not exempt, include information about the TP and TIP. Describe if a hot spot analysis is required and the MSAT Level.

The project is part of the FY 2022-2026 EMPO TIP, which has been directly incorporated into the FY 2022-2026 STIP (Appendix H-1 and H-2). The lead DES number for this contract is 1900308 and includes DES numbers 1900268 and 2000217 by reference with the contract number R-42287.

This project is located in Vanderburgh County, which is currently a maintenance area for the 1997 Ozone 8-hour standard, according to IDEM (https://www.in.gov/idem/sips/nonattainment-status-of-counties/). The 1997 Ozone 8-hour standard was revoked in 2015 but is being evaluated for conformity due to the February 16, 2018, *South Coast Air Quality Management District V. Environmental Protection Agency, Et. Al. Decision*. This project has been identified as being exempt from air quality analysis in accordance with 40 CFR Part 93.126 and this project is not a project of air quality concern (40 CFR Part 93.123). Therefore, the project will have no significant impact on air quality.

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

SECTION G - NOISE

Noise	Yes	No
Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?		X

Date Noise Analysis was approved/technically sufficient by INDOT ESD: _____

Describe if the project is a Type I or Type III project. If it is a Type I project, describe the studies completed to date and if noise impacts were identified. If noise impacts were identified, describe if abatement is feasible and reasonable and include a statement of likelihood.

This project is a Type III project. In accordance with 23 CFR 772 and the current *Indiana Department of Transportation Traffic Noise Analysis Procedure*, this action does not require a formal noise analysis.

SECTION H – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors	Yes	No
Will the proposed action comply with the local/regional development patterns for the area?	X	
Will the proposed action result in substantial impacts to community cohesion?		X
Will the proposed action result in substantial impacts to local tax base or property values?		X
Will construction activities impact community events (festivals, fairs, etc.)?		X
Does the community have an approved transition plan?	X	
If No, are steps being made to advance the community's transition plan?		
Does the project comply with the transition plan? (explain in the discussion below)	X	

Discuss how the project complies with the area's local/regional development patterns; whether the project will impact community cohesion; and impact community events. Discuss how the project conforms with the ADA Transition Plan.

This project complies with local and regional development plans including the *City of Evansville-Vanderburgh County Comprehensive Plan 2015-2035* (https://dev.evansvilleapc.com/assets/docs/Planning/comp-plan/Evansville-Vanderburgh%20County%20Comprehensive%20Plan%202015-2035.pdf), the EMPO *Bicycle and Pedestrian Connectivity Master Plan* (https://www.walkbikeevv.org/s/EvansvilleMPO_BPCMP_Final_Plan.pdf), and the EMPO 2022-2026 TIP (Appendix H-1).

The preferred alternative will permanently alter the traffic patterns at the SR 66/Lloyd Expressway and Vann Avenue intersection. As detailed in the Purpose and Need section, there is a high rate of fatal/incapacitating crashes at this intersection, which can be attributed in part to excessive queueing and sight line issues. In order to improve these conditions, the stoplight and left turns will be removed; and a median along SR 66/Lloyd Expressway through the intersection will be installed. This will permanently alter the traffic patterns for motorists to access the properties near SR 66/Lloyd Expressway and Vann Avenue. However, the proposed changes will not result in the inaccessibility of these properties. All roadways and surrounding properties will continue to be accessible via the existing grid of city streets, as summarized below:

- Motorists traveling EB SR 66/Lloyd Expressway will access NB Vann Avenue by continuing to the Green River Road interchange, located approximately one mile east, where they could exit and return on WB SR 66/Lloyd Expressway to turn right onto NB Vann Avenue.
- Motorists traveling on WB SR 66/Lloyd Expressway will access SB Vann Avenue by continuing to the Boeke Road interchange, located approximately 0.5 mile west, where they could exit and return on EB SR 66/Lloyd Expressway to turn right onto SB Vann Avenue.
- Motorists traveling from SB Vann Avenue/Division Street to EB SR 66/Lloyd Expressway will take EB Division Street to the Stockwell Road intersection, located approximately 0.4 mile east, and then turn left at the stoplight onto EB SR 66/Lloyd Expressway.
- Motorists traveling from NB Vann Avenue to WB SR 66/Lloyd Expressway will travel along WB local streets, such as Walnut Street or Sycamore Street, approximately 0.5 mile west to Boeke Road where they can use the existing interchange.

This project will not result in substantial impacts to community cohesion because it involves the reconstruction of existing intersections and roads, primarily within the existing ROW, and there will be no change in access to surrounding properties. According to the Corridor Study, traffic analysis modeling for the preferred alternative indicates the local streets will not become congested (Appendix I-10). Furthermore, the above-listed alternate routes either lack pedestrian facilities (e.g., Division Street), or they are relatively newly-redeveloped areas with pedestrian facilities that likely meet current standards including ADA (e.g., Boeke Road, Green River Road, and Walnut Street). Therefore, the rerouted traffic is not expected to create congestion or safety issues.

This project is necessary to address the safety and capacity issues at the SR 66/Lloyd Expressway intersections with Vann Avenue and Stockwell Road (see the Purpose and Need section for further discussion). Public involvement activities have not identified substantial community concerns regarding the proposed changes in access. Public comments received during and after the public hearing held on March 7, 2023, focused on; safety, Vann Avenue, bicyclist and pedestrian connectivity, traffic signals, MOT, and the local road network.

The proposed MOT plan includes phased construction that would allow at least two lanes of EB and WB traffic along SR 66/Lloyd Expressway to remain open at all times. Detours may be needed for portions of Vann Avenue and Stockwell Road, as well as other local roads. Access for all residences and businesses will be maintained throughout construction. The TMP will include input obtained from meetings with stakeholders to ensure impacts to the public transit, schools, and community events are minimized.

The project will comply with the City of Evansville's *Americans with Disabilities Act Transition Plan for Local Sidewalk Accessibility* (https://www.evansvillegov.org/egov/apps/document/center.egov?view=item&id=98) . The existing sidewalk and curb ramps along the south approach of Vann Avenue will remain in- place and undisturbed, as well as the curb ramp at the southeast corner of Vann Avenue and Division Street. A pedestrian refuge is proposed for the southern splitter island. It will be reconstructed to current design standards including ADA accessible standards. The legacy northeast and southeast curb ramps and northeast sidewalk will be removed because the existing pedestrian overpass is now utilized for this movement. No impacts to the adjoining park, trails, and pedestrian overpass are expected. The pedestrian overpass and its ramps are outside the construction area and it will remain open to users during construction. The pedestrian accommodations were coordinated with the City of Evansville (Appendix I-38) and applicable commitments are included in the Environmental Commitments section. The proposed project will not impact pedestrian access.

The SR 66/Lloyd Expressway Intersections Improvements at Vann Avenue and Stockwell Road Project involve minimal strips of ROW and no relocations; therefore, it should not impact the local tax base. Based on the discussion above, no significant economic or community impacts are expected to develop as a result of the project.

Public Facilities and Services

Discuss what public facilities and services are present in the project area and impacts (such as MOT) that will occur to them. Include how the impacts have been minimized and what coordination has occurred. Some examples of public facilities and services include health facilities, educational facilities, public and private utilities, emergency services, religious institutions, airports, transportation or public pedestrian and bicycle facilities.

Based on a desktop review, the aerial map of the project area (Appendix B-3), and the RFI report (Appendix E-1 to E-10), there is one church, five cemeteries, one hospital, one school, six recreational facilities, three pipeline segments, one railroad, 17 trail segments, and three managed lands located within 0.5-mile of the project. There are 10 public facilities mapped as within or adjacent to the project area. That number was updated to seven by the desktop review and site visits on June 15-18, 2021 by Parsons.

Four cemeteries associated with the Evansville State Hospital are mapped as adjacent to the project area. An ECL was sent to the Evansville State Hospital on March 2, 2022, and no response was received. Based on coordination with INDOT CRO and the Section 106 documentation, a Cemetery Development Plan is not required and no impacts are expected (Appendix D-1 to D-8).

The University of Evansville Sports Complex is adjacent to the north side of project area (Appendix B-3). Driveway entrances to this facility are located within existing ROW will be reconstructed during this project to tie into the new grades (Appendix B-38 and B-39). There will be no changes in ownership or access, and the contractor must maintain access during construction. Therefore, no impacts are expected.

One trail segment is located in the project area at the southeast corner of the Vann Avenue and SR 66/ Lloyd Expressway intersection in the State Hospital Grounds Park (Appendix B-3). As previously discussed, no impacts are expected; see the Section 4(f) section for further discussion of State Hospital Ground Park.

As previously discussed, the pedestrian overpass bridge located east of Vann Avenue is outside the construction area and will remain open to users; therefore, no impacts are expected.

ECLs were sent to the Harrison High School, University of Evansville, Evansville Convention and Visitors Bureau, City of Evansville, and Evansville Department of Parks and Recreation on March 2, 2022. No comments were received regarding the recreation facilities, parks or trails in the project area.

The Metropolitan Evansville Transit System operates the Walnut Route within the project area (Appendix I-28). There are no transit stops along SR 66/Lloyd Expressway or at the Vann Avenue and Stockwell Road intersections. The Walnut Route uses SR 66/Lloyd Expressway as a connection to the stops on East Walnut Street and East Virginia Street. ECLs were sent to the Metropolitan Evansville Transit System on March 2, 2022, and no response was received. There will be ongoing coordination with the Metropolitan Evansville Transit System throughout the project development process to minimize any disruption to transit service. The proposed MOT plan includes phased construction that will allow at least two lanes of EB and WB traffic along SR 66/Lloyd Expressway to remain open at all times. Therefore, the proposed project is not anticipated to impact transit service.

One pipeline segment, owned by Southern Indiana Gas & Electric Co., is adjacent to the project area and there are multiple utilities within the project area. There are no railroads within the project area. The project team is conducting ongoing utility coordination for this project. A copy of the Utilities Coordination Log is provided in Appendix I-14 to I-17. There will be no disruption in service; therefore, no impacts are expected. ECLs were sent to City of Evansville and Vanderburgh County Engineer (Appendix C-1 to C-5), and no responses regarding utilities were received. Refer to the Drinking Water section for further discussion of public water supplies.

Evansville Regional Airport is located approximately 3.7 miles northwest of the project area. An ECL was sent to the INDOT Office of Aviation on March 2, 2022. The Office of Aviation responded on March 8, 2022 and stated that the project meets the required glideslope criteria from the nearest public-use facility according to 14 CFR Part 77 – Safe, efficient use, and preservation of the navigable airspace (Appendix C-14). If any object will exceed 200 feet in height regardless of location, the object will need to be airspaced with the Federal Aviation Administration (FAA) 45 days prior to construction.

Access for all residences and businesses will be maintained throughout construction. The TMP will include input obtained from meetings with stakeholders to ensure impacts to the public transit, schools, and community events are minimized. Therefore, no impacts are expected.

The Vanderburgh County Surveyor responded to the ECL on March 7, 2022 (Appendix C-8 to C-11). The Surveyor stated the project extents appear to include multiple section corners that have been perpetuated by the Vanderburgh County Surveyor's Office. If these points are disturbed by the project, the Vanderburgh County Surveyor's Office requests that they be replaced by a Licensed Surveyor and updated coordinate data be provided.

All applicable recommendations are included in the Environmental Commitments section of this CE.

Environmental Justice (EJ) (Presidential EO 12898)	**Yes**	**No**
During the development of the project were EJ issues identified?	X	
Does the project require an EJ analysis?	X	
If YES, then:		
Are any EJ populations located within the project area?	X	
Will the project result in adversely high and disproportionate impacts to EJ populations?		X

Indicate if EJ issues were identified during project development. If an EJ analysis was not required, discuss why. If an EJ analysis was required, describe how the EJ population was identified. Include if the project has a disproportionately high or adverse effect on EJ populations and explain your reasoning. If yes, describe actions to avoid, minimize and mitigate these effects.

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT *Categorical Exclusion Manual*, an Environmental Justice (EJ) Analysis is required for any project that has two or more relocations or 0.5 acre of additional permanent ROW. The project will require 0.6 acre of additional permanent new ROW therefore, an EJ Analysis is required. The EJ Analysis is provided in Appendix I-18 to I-28 and INDOT's concurrence is in Appendix I-39.

Identification of Populations: Potential EJ impacts are detected by locating minority and low-income populations relative to a reference population to determine if populations of EJ concern exist, and whether there could be disproportionately high and adverse impacts to them. The reference population may be a county, city or town and is called the community of comparison (COC). In this project, the COC is Vanderburgh County (Appendix I-22). The community that overlaps the project area is called the affected community (AC). In this project, the ACs are the following Census Track (CT) Block Groups: Block 1, CT 2.02 (AC-A), Block 1, CT 4 (AC-B), and Block 1, CT 37.01 (AC-C) (Appendix I-23).

An AC has a population of concern for EJ if the population is more than 50% minority or low-income or if the low-income or minority population is 125% of the COC. Data from the Census.gov 2019 American Community Survey (ACS) 5-year Estimates was obtained from the census.gov website on February 24, 2022. The data collected for minority and low-income populations within the ACs are summarized in the following table.

Minority and Low-Income Data (2019 ACS 5-Year Estimates)				
	COC Vanderburgh County	AC-A	AC-B	AC-C
Percent Minority	15.0	2.1	5.9	24.4
125% of COC	18.7	AC < 125% COC	AC < 125% COC	AC > 125% COC
EJ Population of Concern?		No	No	**Yes**
Percent Low-Income	16.7	7.6	15.1	13.6
125% of COC	20.8	AC < 125% COC	AC < 125% COC	AC < 125% COC
EJ Population of Concern?		No	No	No

Based on the data presented in the table, AC-C contains a population of EJ concern. The census data sheets, map, and calculations are provided in Appendix I-24 to I-26.

AC-A has a percent minority of 2.1, which is below 50% and below the 125% COC threshold. Therefore, AC-A does not contain a minority population of EJ concern. AC-A has a percent low-income of 7.6, which is below 50% and below the 125% COC threshold. Therefore, AC-A does not contain a low-income population of EJ concern.

AC-B has a percent minority of 5.9, which is below 50% and below the 125% COC threshold. Therefore, AC-B does not contain a minority population of EJ concern. AC-B has a percent low-income of 15.1, which is below 50% and below the 125% COC threshold. Therefore, AC-B does not contain a low-income population of EJ concern.

AC-C has a percent minority of 24.4, which is below 50%, but is above the 125% COC threshold. Therefore, AC-C does contain a minority population of EJ concern. AC-C has a percent low-income of 13.6, which is below 50% and below the 125% COC threshold. Therefore, AC-C does not contain a low-income population of EJ concern.

The HUD Resource Locator (https://resources.hud.gov/) was researched to identify potential EJ resources and/or populations. No HUD resources were identified within 0.5-mile of the project area.

Analysis:

ROW and Relocations: The project will require 0.6-acre of additional permanent ROW. The impacts are all within AC-C and are limited to strips of ROW from commercial properties (Appendix I-27). There are no relocations resulting from the project.

Permanent Traffic Pattern Alteration at Vann Avenue: As detailed in the Purpose and Need section, there is a high rate of fatal/incapacitating crashes at this intersection, which can be attributed in part to excessive queueing and sight line issues. In order to improve these conditions, the preferred alternative will permanently remove the stoplight and left turns at the SR 66/Lloyd Expressway and Vann Avenue intersection. This will permanently alter the traffic patterns for motorists to access the properties near SR 66/Lloyd Expressway and Vann Avenue. However, the proposed changes will not result in the inaccessibility of these properties. All roadways and surrounding properties will continue to be accessible via the existing grid of city streets (see the Community Impacts section for details). The proposed traffic pattern alternations will impact all three of the project area's ACs, and the surrounding roadways and properties will continue to be accessible.

According to the Corridor Study, traffic analysis modeling for the preferred alternative indicates the local streets will not become congested (Appendix I-8). Furthermore, the alternate routes for the altered traffic patterns, listed in the Community Impacts section, either lack pedestrian facilities (e.g., Division Street), or they are relatively newly-redeveloped areas with pedestrian facilities that likely meet current standards including ADA (e.g., Boeke Road, Green River Road, and Walnut Street). Therefore the rerouted traffic is not expected to create congestion or safety issues.

Pedestrian Facilities: As previously discussed in the Community Impacts and Public Facilities and Services sections, the proposed project will not impact pedestrian access. At Vann Avenue, the legacy northeast and southeast curb ramps and northeast sidewalk will be removed because the existing pedestrian overpass is now utilized for this movement. The pedestrian overpass was constructed within the last 10 years and appears to meet current standards, including ADA.

Transit Service: The Metropolitan Evansville Transit System, Walnut Route operates within the study area (Appendix I-28). There are no transit stops along Lloyd Expressway or at the Vann Avenue and Stockwell Road intersections. The Walnut Route uses Lloyd Expressway as a connection to the stops on East Walnut Street and East Virginia Street. There will be ongoing coordination with the City of Evansville and the Metropolitan Evansville Transit System throughout the TMP process to minimize any disruption to transit service. Therefore, the proposed project is not anticipated to impact transit service.

Maintenance of Traffic: The proposed MOT includes phased construction that would allow at least two lanes of EB and WB traffic along Lloyd Expressway to remain open at all times. Detours may be needed for portions of Vann Avenue and Stockwell Road, as well as other local roads. Access to all properties will be maintained. Design of the MOT is ongoing. Access for all residences and businesses will be maintained throughout construction. The TMP will include input obtained from meetings with stakeholders to ensure impacts to the public transit, schools, and community events are minimized. Therefore, the proposed MOT is not anticipated to disproportionately impact EJ populations.

Outreach: Prior to the public hearing for the Lloyd4U east side projects, outreach efforts were targeted at informing apartment building residents in and adjacent to the project area of the upcoming hearing and comment period. Some of these facilities were identified during EJ evaluations at the beginning of the project, others were targeted to inform and include property dwellers who might not have received the legal notice due to lack of ownership of their property. The public outreach coordinator communicated with several apartment managers to provide maps and other project materials for use in electronic communications with residents such as newsletters or social media pages. Printed materials and copies of the press release/public notice were also provided to hang in public spaces like on bulletin boards or in laundry rooms. The apartment complexes included in the outreach were Ashley

Court, Ashley Pointe, Fairmont, Fielding Court, Kimber Green, Pavilion Lakes, Regency Club, and Shannon Glenn.

The project's public hearing was held at a nearby accessible location, the Crescent Room at Milestones, and stakeholders who represent EJ populations such as elected officials, transit, local housing authorities, adjoining landowners, public schools, religious institutions, and civic organizations were invited (Appendix G-175 to G-180). Further details about the hearing are provided in the Public Involvement section.

Conclusions: The project area contains EJ populations of concern. The purpose of this intersection improvement project is to reduce the rate of crashes at both intersections and improve the LOS at SR66/Lloyd Expressway and Stockwell Road to a minimum of LOS D in the design year, 2045. Therefore, the project should provide benefits to the community. The proposed ROW impacts are limited to acquisitions from commercial properties (Appendix I-27). Potential impacts to public transit during construction will be minimized through coordination with transit authorities and local governmental officials (firm commitment). Based on this analysis, there does not appear to be disproportionately high and adverse impacts to EJ populations in or near the project area.

On June 28, 2022, INDOT ESD stated, "INDOT-Environmental Services Division (ESD) has reviewed the project information along with the Environmental Justice (EJ) Analysis for the above referenced project. With the information provided, the project may require minimal right-of-way, require no relocations, and would not disrupt community cohesion or create a physical barrier. With the information provided, INDOT-ESD would not consider the impacts associated with this project as causing a disproportionately high and adverse effect on minority and/or low-income populations of EJ concern relative to non-EJ populations in accordance with the provisions of Executive Order 12898 and FHWA Order 6640.23a. No further EJ Analysis is required." (Appendix I-39).

Relocation of People, Businesses or Farms	**Yes**	**No**
Will the proposed action result in the relocation of people, businesses or farms?		X
Is a BIS or CSRS required?		X

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: 0

Discuss any relocations that will occur due to the project. If a BIS or CSRS is required, discuss the results in the discussion below.

No relocations of people, businesses, or farms will take place as a result of this project.

SECTION I – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Hazardous Materials & Regulated Substances (Mark all that apply)	**Documentation**
Red Flag Investigation (RFI)	X
Phase I Environmental Site Assessment (Phase I ESA)	
Phase II Environmental Site Assessment (Phase II ESA)	
Design/Specifications for Remediation required?	

Date RFI concurrence by INDOT SAM (if applicable): June 29, 2022

Include a summary of the potential hazardous material concerns found during review. Discuss in depth sites found within, directly adjacent to, or ones that could impact the project area. Refer to current INDOT SAM guidance. If additional documentation (special provisions, pay quantities, etc.) will be needed, include in discussion. Include applicable commitments.

Based on a review of GIS and available public records, the RFI was completed on June 13, 2022 by Parsons and INDOT Site Assessment and Management (SAM) provided their concurrence on June 29, 2022 (Appendix E-1 to E-10). Three Resource Conservation and Recovery Act (RCRA) Generator/Treatment, Storage, and Disposal (TSD) sites, one State Cleanup site, twelve underground storage tanks (UST) sites, one solid waste landfill site, fifteen leaking underground storage tank (LUST) sites, one brownfield site, two institutional control sites, and three National Pollutant Discharge Elimination System (NPDES) facilities are located within the 0.5-mile search radius. None of the hazmat sites identified resulted in recommendations except for one site, discussed below.

Robert Stadium, 2600 East Division Street, AID 41535, is located adjacent to the north of the project area near the Vann Avenue and East Division Street intersection. According to the October 22, 1992, Notification for Underground Storage Tanks, two USTs were removed. No additional information was found in the VFC. If excavation occurs in this area, it is possible that petroleum may be

encountered. Before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.

Applicable recommendations are included in the Environmental Commitments section.

Part IV – Permits and Commitments

PERMITS CHECKLIST

Permits (mark all that apply)	**Likely Required**
Army Corps of Engineers (404/Section10 Permit)	
Nationwide Permit (NWP)	X
Regional General Permit (RGP)	
Individual Permit (IP)	
Other	
IN Department of Environmental Management (401/Rule 5)	
Nationwide Permit (NWP)	X
Regional General Permit (RGP)	
Individual Permit (IP)	
Isolated Wetlands	
Rule 5	X
Other	
IN Department of Natural Resources	
Construction in a Floodway	
Navigable Waterway Permit	
Other	
Mitigation Required	
US Coast Guard Section 9 Bridge Permit	
Others (Please discuss in the discussion below)	

List the permits likely required for the project and summarize why the permits are needed, including permits designated as "Other."

More than 1-acre of land will be disturbed; therefore, an IDEM Construction Stormwater General Permit is anticipated.

A USACE Section 404 Nationwide General Permit and an IDEM Section 401 Water Quality Certification are required. Mitigation is not anticipated.

If any object will exceed 200 feet in height regardless of location, the object will need to be airspaced with the FAA 45 days prior to construction and further coordination will be required with INDOT Aviation (Appendix C-14).

Applicable recommendations provided by resource agencies are included in the Environmental Commitments section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

ENVIRONMENTAL COMMITMENTS

List all commitments and include the name of agency/organization requesting/requiring the commitment(s). Listed commitments should be numbered.

Firm:

1) If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT District)
2) It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
3) General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
4) Lighting AMM 1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)
5) Lighting AMM 2: When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable. (USFWS)
6) The contractor will coordinate the TMP with local stakeholders including but not limited to the City of Evansville, Evansville Vanderburgh Schools, Deaconess Gateway Hospital, University of Evansville, and the Metropolitan Evansville Transit System. (INDOT ESD)
7) No permanent or temporary ROW is proposed from the City of Evansville's State Hospital Grounds Park located at the southeast corner of SR 66/Lloyd Expressway and Vann Avenue. This park is located outside construction limits and its features and amenities will not be impacted by this project. The park's trail, bench, and nearby landscaping features are labeled Do Not Disturb on project plans. The park, including the trail, will remain open to users during construction and access will not be affected. If conditions change INDOT ESD will be contacted immediately. (INDOT ESD)
8) Robert Stadium, 2600 East Division Street, AID 41535, is located adjacent to the north of the project area near the Vann Avenue and East Division Street intersection. According to the October 22, 1992, Notification for Underground Storage Tanks, two USTs were removed. No additional information was found in the VFC. If excavation occurs in this area, it is possible that petroleum may be encountered. Before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination. (INDOT SAM)
9) If any object will exceed 200 feet in height regardless of location, the object will need to be airspaced with the Federal Aviation Administration (FAA) 45 days prior to construction. (INDOT Aviation)
10) The pedestrian overpass bridge located east of Vann Avenue is outside the project area and will remain open to users during construction. If pedestrian facilities require closure during construction, an ADA-accessible detour will be provided. (INDOT ESD)
11) If section corners including but not limited to Point 2856 located in 22-6-10, Point 2173 in 27-6-10 and Point 1838 in Section 26-6-10 are disturbed by the project, they will be replaced by a Licensed Surveyor and updated coordinate data will be provided to the Vanderburgh County Surveyor's Office. (Vanderburgh County Surveyor's Office)
12) If any archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, construction in the immediate area of the find will be stopped, and the INDOT CRO and the Division of Historic Preservation and Archaeology will be notified immediately. (INDOT CRO)
13) As many trees as practicable will be planted within the project area to mitigate for the loss of trees. (City of Evansville)

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Table of Contents

Appendix A: INDOT Supporting Documentation

Categorical Exclusion Level Thresholds Table A-1

Appendix B: Graphics

Project Location Map B-1
USGS Topographic Map B-2
2020 Aerial Map B-3
Proposed Conditions (Preferred Alternative) B-4
Project Photographs B-5
Project Plans (Excerpts) B-8

Appendix C: Early Coordination

Sample Early Coordination Letters C-1
IDNR-DFW Letter C-6
Vanderburgh County Surveyor Letter C-8
Evansville Convention and Visitors Bureau Commission Coordination C-12
INDOT Office of Aviation Coordination C-14
IGWS Electronic Letter C-15
USFWS Official Species List C-18
USFWS Concurrence Verification Letter C-33

Appendix D: Section 106 of the National Historic Preservation Act

Minor Projects PA Project Assessment Form (Exceprt) D-1
Phase Ia Archaeological Reconnaissance (Exceprt) D-9

Appendix E: Red Flag Investigation and Hazardous Materials

Red Flag Investigation E-1

Appendix F: Water Resources

National Wetlands Inventory F-1
IDNR Floodplain Information Portal F-2
Waters of the US Report (Excerpts) F-3

PARSONS

Appendix G: Public Involvement

Notice of Entry Letter G-1
Public Involvement Plan (PIP) G-4
Stakeholder & Public Information Meetings G-46
Lloyd Corridor Local Officials Briefing, October 14, 2020 ..... G-47
Lloyd Corridor Local Offcials Briefing #2, March 4, 2021 ..... G-59
Stakeholder Meeting, March 10, 2021 ..... G-62
Transportation Management Plan Stakeholder Meeting, October 14, 2021 ..... G-72
Business Stakeholder Meetings (AM & PM), March 24, 2022 ..... G-74
Public Meeting (Virtual), April 20, 2021 ..... G-82
Public Meeting (In-Person), April 22, 2021 ..... G-88
Public Meeting, March 29, 2022 ..... G-114
Public Meeting (Virtual), March 31, 2022 ..... G-134
INDOT4U Comments G-152
Media Coverage G-157
Public Hearing G-174
Stakeholder Mailing List G-175
Legal Notice G-181
Affidavit of Publication G-183
Sign-In Sheets G-186
Hearing Materials (Handouts, Boards, Presentation) G-190
Comments (Comments Summary & Responses, Comment Forms, Verbal Comments) G-200

Appendix H: Air Quality

Evansville MPO TIP FY 2022-2026 (Excerpt) H-1
STIP FY 2022-2026 (Excerpt) H-2

Appendix I: Additional Studies/ Reports

Lloyd Expressway (SR 62/66) Corridor Study, October 1, 2021 (Excerpts) I-1
LWCF Search Record I-13
Utility Coordination Log I-14
Environmental Justice (EJ) Analysis I-18
Roadway Project Applications (Excerpts) I-29
Logical Termini and Independent Utility Memorandum (Excerpts) I-35
Pedestrian Connectivity Meeting Summary I-38
INDOT EJ Analysis Email I-39

PARSONS

Appendix A

INDOT Supporting Documentation

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4[1]
Section 106	Falls within guidelines of Minor Projects PA	"No Historic Properties Affected"	"No Adverse Effect"	-	"Adverse Effect" Or Historic Bridge involvement[2]
Stream Impacts[3]	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit[4]
Wetland Impacts[3]	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
Right-of-way[5]	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations[6]	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)*	"No Effect", "Not likely to Adversely Affect" (With select AMMs[7])	"Not likely to Adversely Affect" (With any AMMs or commitments)	-	"Likely to Adversely Affect"	Project does not fall under Species Specific Programmatic[8]
Threatened/Endangered Species (Any other species)*	Falls within guidelines of USFWS 2013 Interim Policy or "No Effect"	"Not likely to Adversely Affect"	-	-	"Likely to Adversely Affect"
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential[9]
Sole Source Aquifer	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Section 4(f) Impacts	None	-	-	-	Any[10]
Section 6(f) Impacts	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes[11]
Approval Level • **District Env. (DE)** • **Env. Serv. Div. (ESD)** • **FHWA**	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA

[1] Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.
[2] Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.
[3] Total permanent impacts to streams (linear feet) and wetlands (acres).
[4] US Army Corps of Engineers Individual 404 Permit
[5] Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.
[6] If any relocations are within an area with a known or suspected Environmental Justice (EJ) or disadvantaged population, or has greater than 5 relocations, a conversation with FHWA, through INDOT ESD, is needed to confirm NEPA classification and outreach plan for the project.
[7] Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.
[8] Projects that do not fall under a Species Specific Programmatic and results in a "Likely to Adversely Affect". Other findings can be processed as a lower-level CE.
[9] Potential for causing a disproportionately high and adverse impact.
[10] Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.
[11] Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.
* Includes the threatened/endangered species critical habitat
Note: Substantial public or agency controversy may require a higher-level NEPA document.

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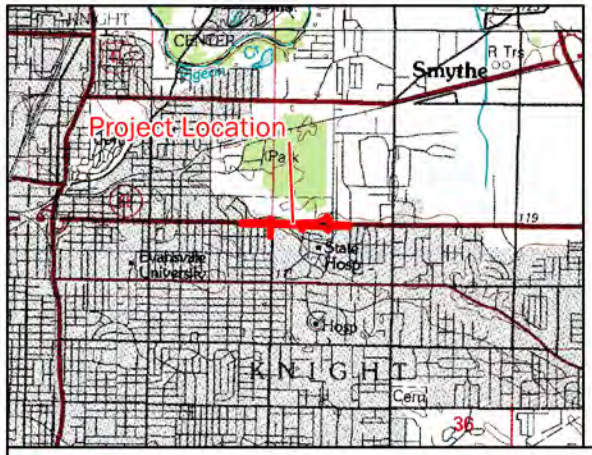
Appendix B

Graphics



Study Area
Streams and Rivers
Incorporated Areas
Interstate
State Road
Local Road

0 0.15 0.3
Miles

Sources:
Non Orthophotography Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

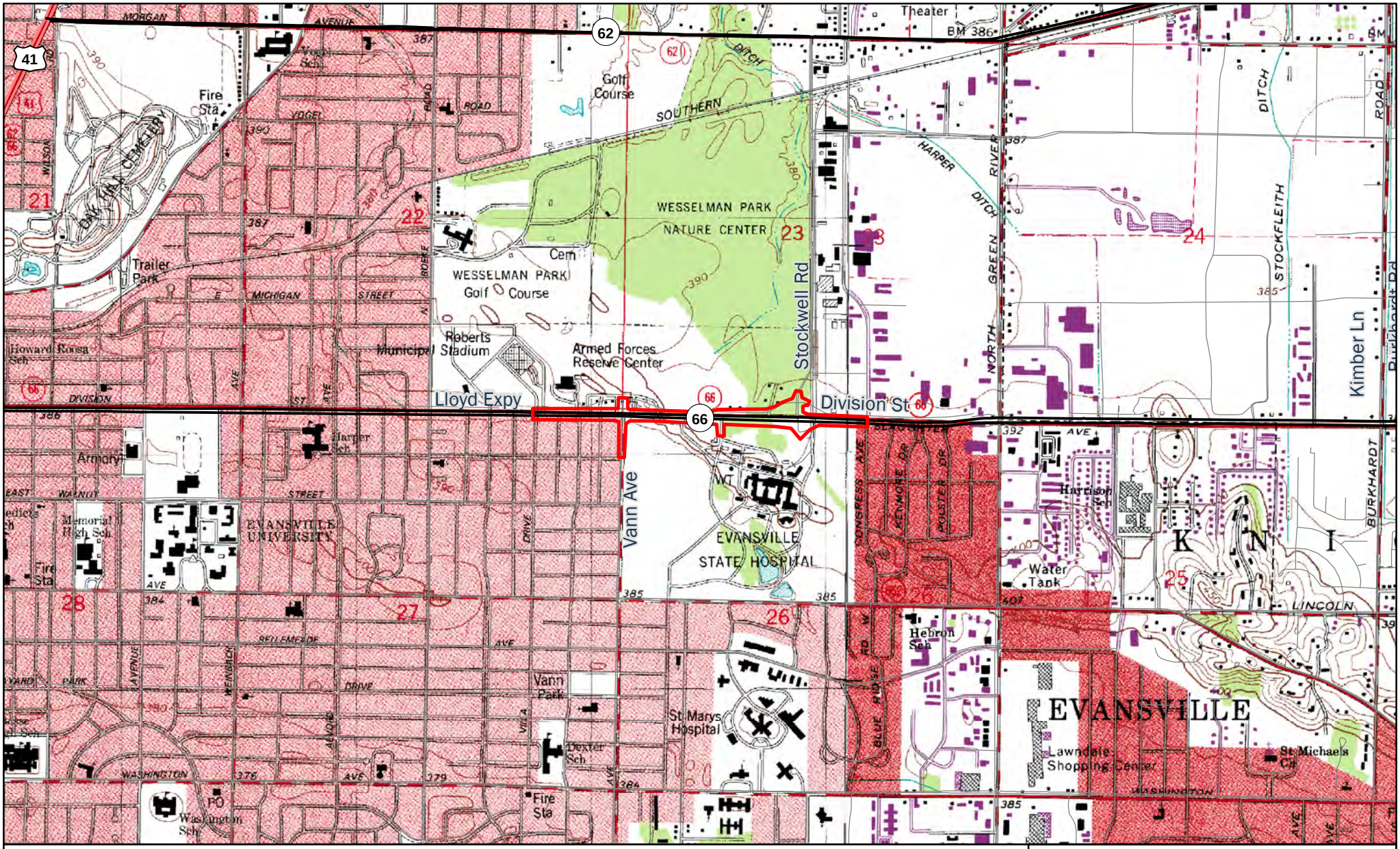
SR 66/Lloyd Expressway at Vann Avenue and Stockwell Road
Vanderburgh County, Indiana
Project Location

Des. 1900268/2000217

Date: 5/11/2022

PARSONS

Created by: KDV


Study Area
0 1,000 2,000
Feet
Lloyd Expressway
Vann Avenue and Stockwell Road
Intersections Project
Vanderburgh County, Indiana
USGS Topographic
Sources:
Non Orthophotography Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Des. Nos. 1900268 & 2000217
Date: 2/18/2022
PARSONS
Created by: KDV

Study Area

0 300 600

Feet

Sources:
Non Orthophotography Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

SR 66/Lloyd Expressway at Vann Avenue and Stockwell Road
Vanderburgh County, Indiana
2020 Aerial

Des. 1900268/2000217

Date: 5/11/2022

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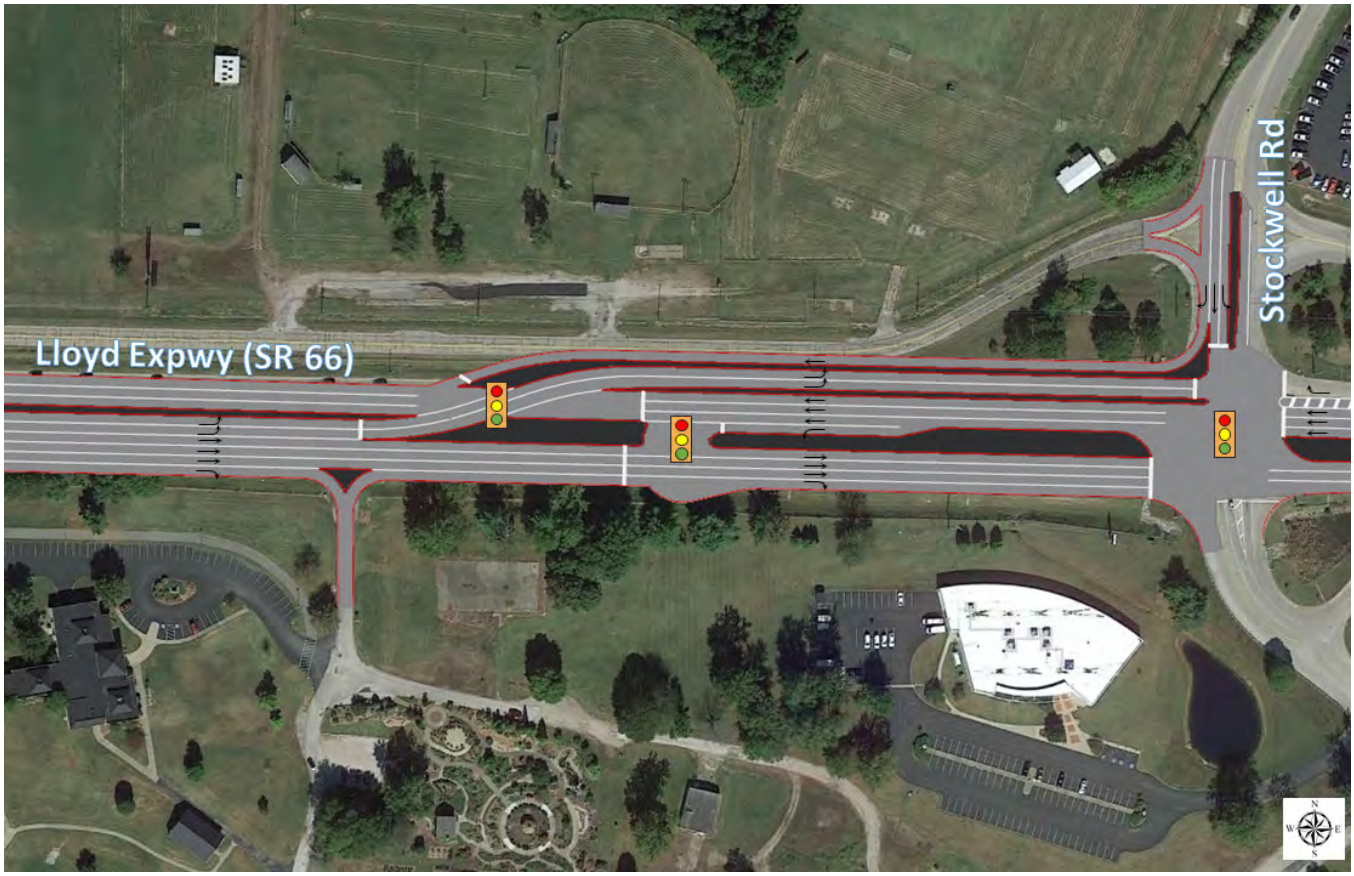
Created by: KDV

Proposed Conditions (Preferred Alternative)

STOCKWELL ROAD INTERSECTION



Hybrid solution with EB displaced left turn and WB boulevard left turn

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VANN AVENUE INTERSECTION

Right-in, right-out (restricted turn movements)

Photo 1— View of the roadside along westbound Lloyd Expressway and Division Street (Left) facing southeast. (6/15/2021).

Photo 2— View of the Vann Avenue and Lloyd Expressway intersection facing south (6/15/2021).

Photo 3 — View of the roadside along Vann Avenue from the East Sycamore Street and Vann Avenue intersection, facing north (06/16/2021).

Photo 4 — View of the pedestrian bridge over Lloyd Expressway facing west (06/15/2021).

Photo 5— View of the roadside along Division Street facing east (06/15/2021).

Photo 6—View of the roadside along Division Street from the Stockwell Road and Division Street intersection, facing southwest (06/15/2021).

Photo 7 —View of the roadside along westbound Lloyd Expressway facing east (06/15/2021).

Photo 8 —View of the roadside along eastbound Lloyd Expressway facing east (06/15/2021).

Photo 9— View of the roadside along eastbound Lloyd Expressway and John Street (Right), facing east (06/15/2021).

Photo 10—View of the roadside along eastbound Lloyd Expressway and John Street (Left), facing west (06/15/2021).

PROJECT	DESIGNATION
1900308	1900268
CONTRACT	
R-42287	

Excerpts

INDIANA DEPARTMENT OF TRANSPORTATION

Excerpts

ROAD PLANS

ROUTE: SR66 - Lloyd Expressway AT: RP 28+0.9

PROJECT NO. 1900308 P.E.
1900308 R/W
1900308 CONST.

Vann Avenue Intersection Improvement at SR66-Lloyd Expressway
Sections 22, 23, 26, and 27 of T-6-S, R-10-W, Knight Township, Vanderburgh County.

KIN PROJECT INFORMATION		
DESIGNATION	PROJECT DESCRIPTION	TYPE
1900308 (LEAD)	Road Reconstruction along SR 62 from Rosenberger Ave. to 2.72 mi W of S Jct. US-41	Roadway
1900317	SR 66 (Lloyd) at 0.58 miles W. of I-69 (Cross Pointe Blvd.) - Intersection Improvements	Roadway
1900292	SR 66 (Lloyd) at 1.20 miles W. of I-69 (Burkhardt Rd.) - Intersection Improvements	Roadway
1900263	SR 62 (Lloyd) at 3.09 miles W. of US 41 (St. Joseph Ave.) - Intersection Improvements	Roadway
1900264	SR 62 (Lloyd) at 4.58 miles W. of US 41 (Rosenberger Ave.) - Intersection Improvements	Roadway
1500041	SR 62 (Lloyd) over CSX Railroad & Evansville Western Railroad - Bridge Replacement	Bridge
1600060	SR 62 (Lloyd) over Tekoppel Ave. - Bridge Replacement	Bridge
1602258	SR 62 (Lloyd) over Carpentier Creek - Bridge Replacement	Bridge

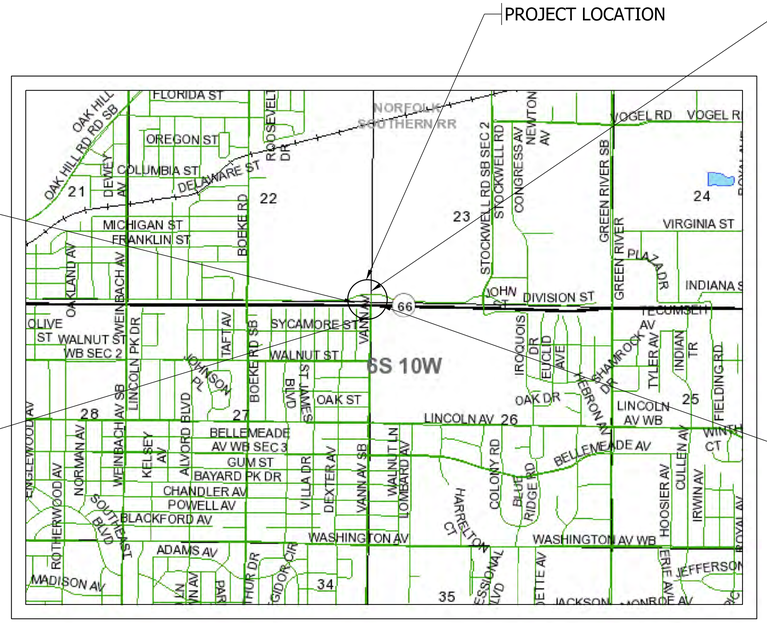
PROJECT LOCATION

End Construction
Sta. 34+68.25 "V"

Begin Project
Sta. 109+49.29 "L-PR"

Begin Construction
Sta. 31+60.28 "V"

End Project
Sta. 116+74.05 "L-PR"



Scale: 1" = 8000'

Traffic Data	SR66 (Lloyd Expressway)	Vann Avenue
A.A.D.T. (2023)	60,390 V.P.D.	10,361 V.P.D.
A.A.D.T. (2043)	71,893 V.P.D.	12,353 V.P.D.
D.H.V (2043)	6,027 V.P.H.	N/A
DIRECTIONAL DISTRIBUTION	55%	N/A
TRUCKS	3% A.A.D.T.	N/A
	1% D.H.V.	N/A
Design Data		
DESIGN SPEED	50 M.P.H.	30 M.P.H.
PROJECT DESIGN CRITERIA	3R (NON-FREEWAY)	3R (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	PRINCIPAL ARTERIAL (OTHER)	STATE COLLECTOR
RURAL/URBAN	URBAN (BUILT-UP)	URBAN (BUILT-UP)
TERRAIN	LEVEL	LEVEL
ACCESS CONTROL	NONE	NONE

PROJECT LOCATION SHOWN BY
VANDERBURGH COUNTY

LATITUDE: 37° 58' 36" N LONGITUDE: 87° 30' 39" W

HUC: 05140202040080

Gross Length: 0.14 MI.
Net Length: 0.14 MI.
Maximum Grade: "V" = 5.00%
"L-PR" = 0.24%

Note, maintenance of traffic (MOT) sheets are intentionally omitted. An MOT overview sheet was not prepared because an official detour is not needed.

Stage 2 Plans

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2022
TO BE USED WITH THESE PLANS

PARSONS
101 W. Ohio St., Suite 2121
Indianapolis, IN 46204
Bus (317) 616-1000
Fax (317) 616-1033

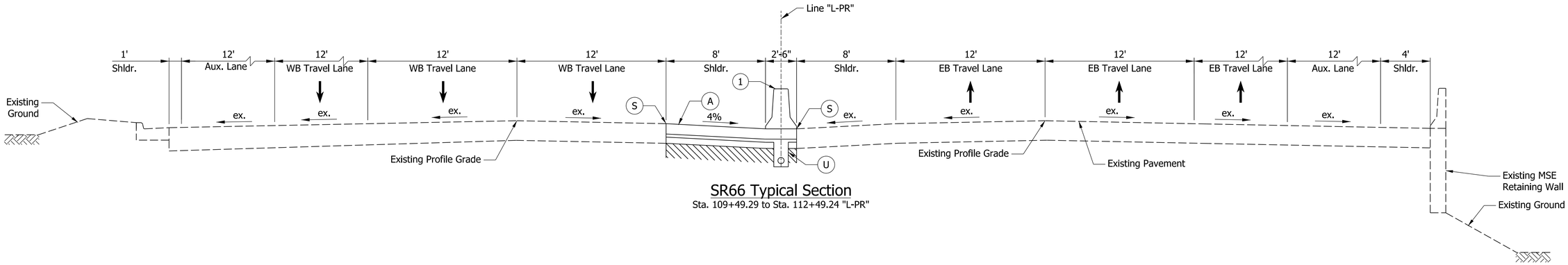
PLANS PREPARED BY: PARSONS 317-616-1000 PHONE NUMBER

CERTIFIED BY: _____ DATE

APPROVED FOR LETTING: _____ INDIANA DEPARTMENT OF TRANSPORTATION DATE

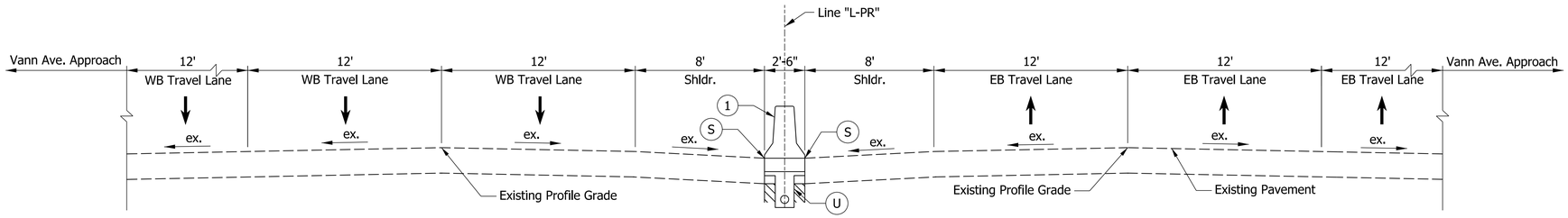
DESIGNATION	
1900268	
SURVEY BOOK	SHEETS
N/A	1 of 96
CONTRACT	PROJECT
R-42287	1900308

$FILE$
\\PARSONS\VANA01PWINT01.Parsons.com:Indiana State/Documents/SR66 Lloyd Expwy Corridor/CADD/Roadway/Sheets/Des. 1900268/VANN_RD_Title Sheet.dgn
$DATE$



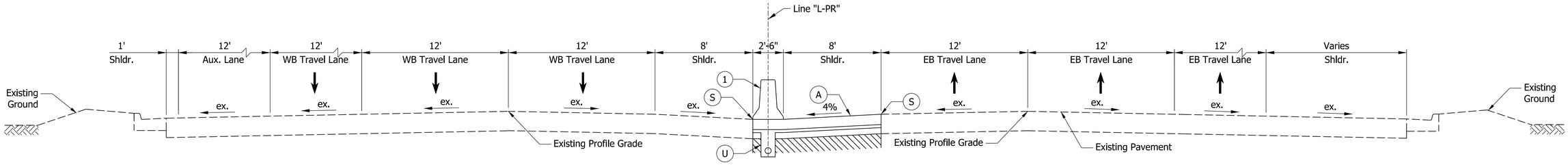
SR66 Typical Section

Sta. 109+49.29 to Sta. 112+49.24 "L-PR"



SR66 Typical Section

Sta. 112+49.24 to Sta. 113+65.09 "L-PR"



SR66 Typical Section

Sta. Sta. 113+65.09 to Sta. 116+74.05 "L-PR"

(1) Concrete Barrier, 33"	(A) QC/QA PCCP, 10 in. on Subbase for PCCP (3 in. of CA No. 8 on 6 in. CA No. 53) on Subgrade Treatment, Type IBC	Subgrade Treatment, Type IBC
(22) Center Curb, Type B (4")	(S) Sawcut	
(27) Mulched Seeding U	(U) Pipe, Type 4, Circular, 6"	

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE

DESIGNED: SRB DRAWN: DH

CHECKED: CWB CHECKED: CWB

INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
LINE "L-PR" (SR 66)

HORIZONTAL SCALE	BRIDGE FILE
1" = 4'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1900268
SURVEY BOOK	SHEETS
N/A	3 of 96
CONTRACT	PROJECT
R-42287	1900308

pw://VANVA01PWINT01.Parsons.com:Indiana State/Documents/SR66 Lloyd Expwy Corridor/CADD/Roadway/Sheets/Des. 1900268/VANN_Lloyd_RD_Sht_Typ_01.dgn
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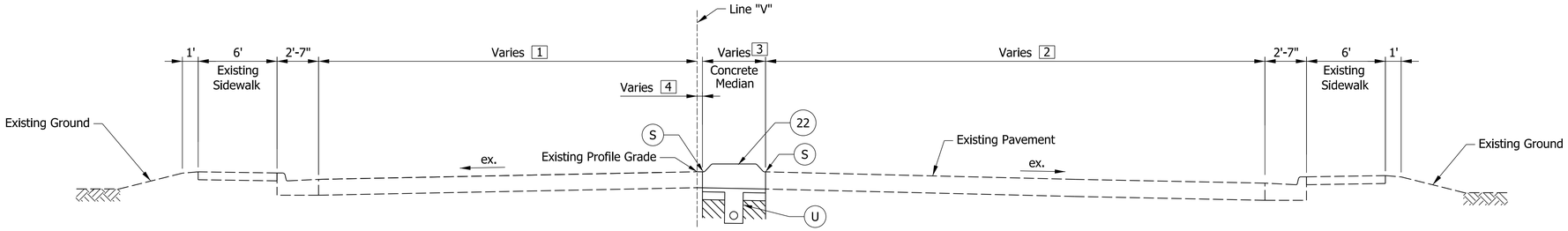


Vann Avenue Typical Section

Sta. 31+60.28 to Sta. 32+10.62 "V"
Pedestrian Median Cut-Through Exception
Sta.32+10.62 to 32+16.56

1 24'
Sta. 31+60.28 to Sta. 31+87.25
Varies 24' to 34'
Sta. 31+87.25 to Sta. 32+10.62

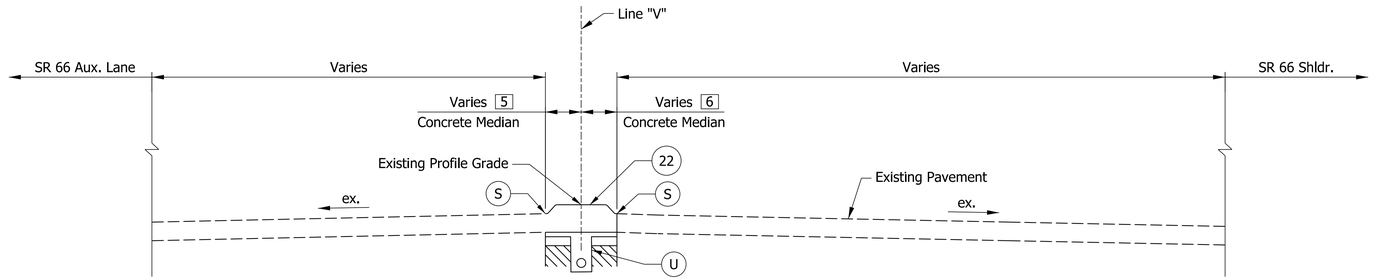
2 34'
Sta. 31+60.28 to 31+87.25
Varies 34' to 42'
Sta. 31+87.25 to Sta. 32+10.62

3 Varies 3'-4 1/2" to 8'-4 1/2"
Sta. 31+60.28 to Sta. 31+62.93
Varies 8'-4 1/2" to 4'-6 1/4"
Sta. 31+62.93 to Sta. 31+80.03
Varies 4'-6 1/4" to 3'-9 7/8"
Sta. 31+80.03 to Sta. 31+86.44
Varies 3'-9 7/8" to 11'-3 1/8"
Sta. 31+86.44 to Sta. 32+10.62

4 Varies 6'-9" to 4'-3"
Sta. 31+60.28 to Sta. 31+62.93
Varies 4'-3" to 6'-9"
Sta. 31+62.93 to Sta. 31+80.03
Varies 6'-9" to 7'-3 3/8"
Sta. 31+80.03 to Sta. 31+86.44
Varies 7'-3 3/8" to 3'-10 1/4"
Sta. 31+86.44 to Sta. 32+10.62

5 Varies 0'-4 1/8" to 7'-3 5/8"

6 Varies 19'-2 3/8" to 25'-6 3/4"



Vann Avenue Typical Section

Sta. 32+16.56 to Sta. 32+32.35 "V"

1 Concrete Barrier, 33"
22 Center Curb, Type B (4")
27 Mulched Seeding U

A QC/QA PCCP, 10 in. on Subbase for PCCP (3 in. of CA No. 8 on 6 in. CA No. 53) on Subgrade Treatment, Type IBC
S Sawcut
U Pipe, Type 4, Circular, 6"

Subgrade Treatment, Type IBC

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE

DESIGNED: SRB DRAWN: DH
CHECKED: CWB CHECKED: CWB

INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
LINE "V" (VANN AVENUE)

HORIZONTAL SCALE	BRIDGE FILE
1" = 4'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1900268
SURVEY BOOK	SHEETS
N/A	4 of 96
CONTRACT	PROJECT
R-42287	1900308

pw://VANVA01PWINT01.Parsons.com:Indiana State/Documents/SR66 Lloyd Expwy Corridor/CADD/Roadway/Sheets/Des. 1900268/VANN_Lloyd_RD_Sht_Typ_02.dgn
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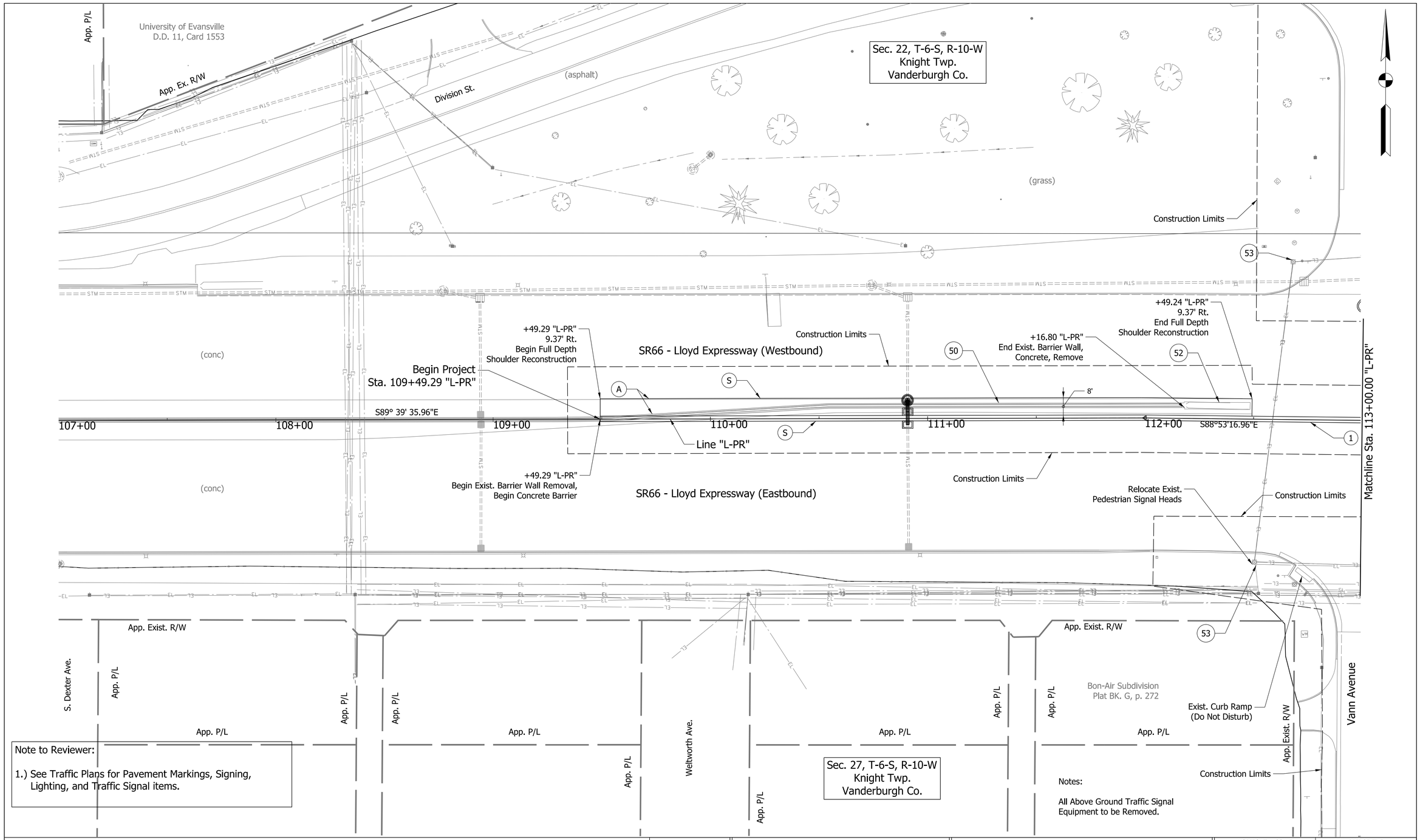

1	Concrete Barrier, 33"	50	Barrier Wall, Concrete, Remove
13	Curb, Concrete Vertical	51	Curb, Concrete, Remove
22	Center Curb, Type B (4")	52	Impact Attenuator, Remove
27	Mulched Seeding U	53	Traffic Signal Equipment, Remove

	Sidewalk, Concrete, Remove
A	QC/QA PCCP, 10 in. on Subbase for PCCP (3 in. of CA No. 8 on 6 in. CA No.53) on Subgrade Treatment, Type IBC
S	Sawcut

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE

DESIGNED: SRB DRAWN: DH

CHECKED: CWB CHECKED: CWB

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 107+00.00 TO STA. 113+00.00
LINE "L-PR"

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1900268
SURVEY BOOK	SHEETS
N/A	71 of 96
CONTRACT	PROJECT
R-42287	1900308

pw://VANVA01PWINT01.Parsons.com:Indiana State/Documents/SR66 Lloyd Expwy Corridor/CADD/Roadway/Sheets/Des. 1900268/VANN_Lloyd_Plan_01.dgn
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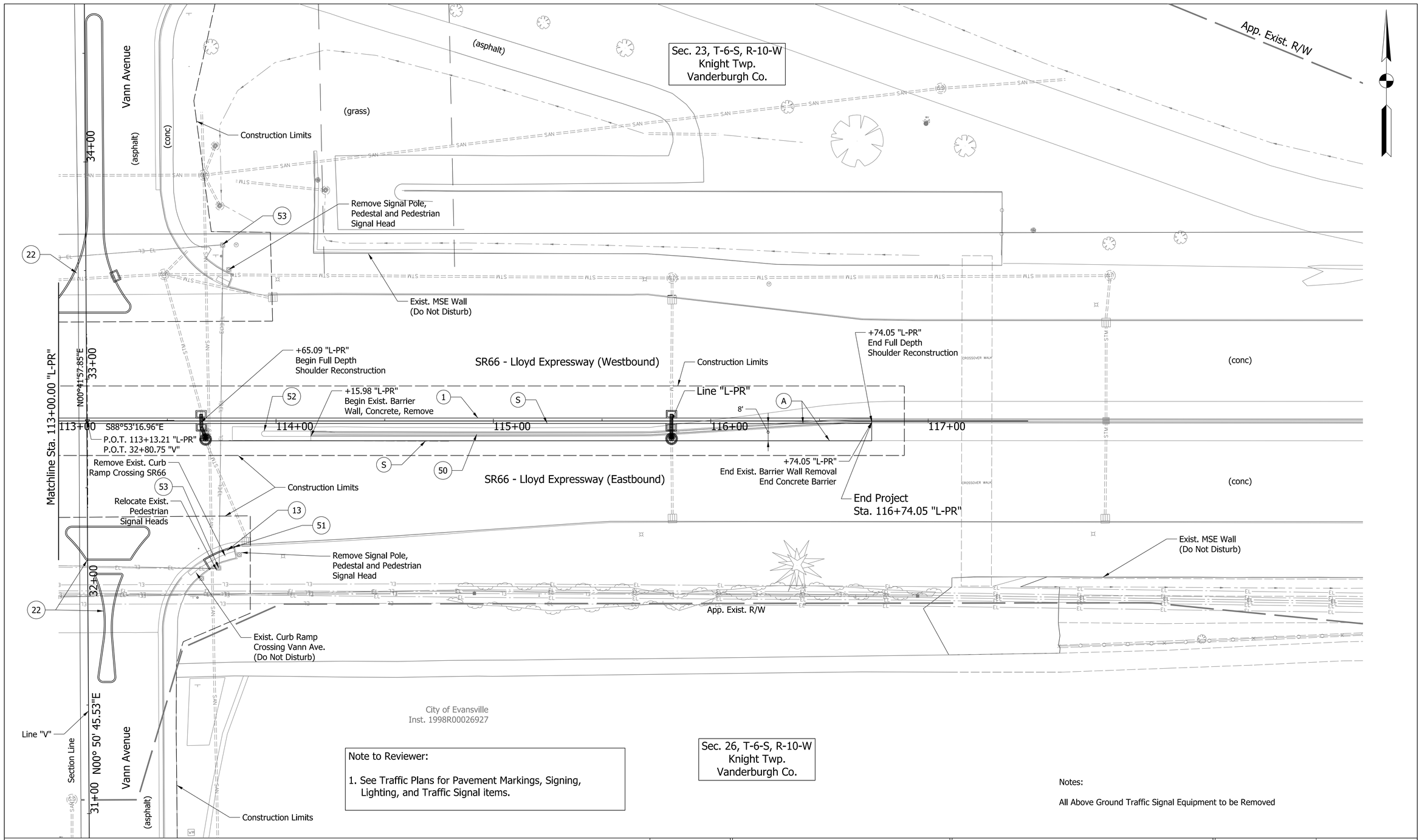

Note to Reviewer:

1. See Traffic Plans for Pavement Markings, Signing, Lighting, and Traffic Signal items.

Notes:

All Above Ground Traffic Signal Equipment to be Removed

1	Concrete Barrier, 33"	50	Barrier Wall, Concrete, Remove		Sidewalk, Concrete, Remove
13	Curb, Concrete Vertical	51	Curb, Concrete, Remove	A	QC/QA PCCP, 10 in. on Subbase for PCCP (3 in. of CA No. 8 on 6 in. CA No.53) on Subgrade Treatment, Type IBC
22	Center Curb, Type B (4")	52	Impact Attenuator, Remove		
27	Mulched Seeding U	53	Traffic Signal Equipment, Remove	S	Sawcut

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE

DESIGNED: SRB DRAWN: DH

CHECKED: CWB CHECKED: CWB

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 113+00.00 TO STA. 119+00.00
LINE "L-PR"

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1900268
SURVEY BOOK	SHEETS
N/A	73 of 96
CONTRACT	PROJECT
R-42287	1900308

pw://VANVA01PWINT01.Parsons.com:Indiana State/Documents/SR66 Lloyd Expwy Corridor/CADD/Roadway/Sheets/Des. 1900268/VANN_Lloyd_Plan_02.dgn
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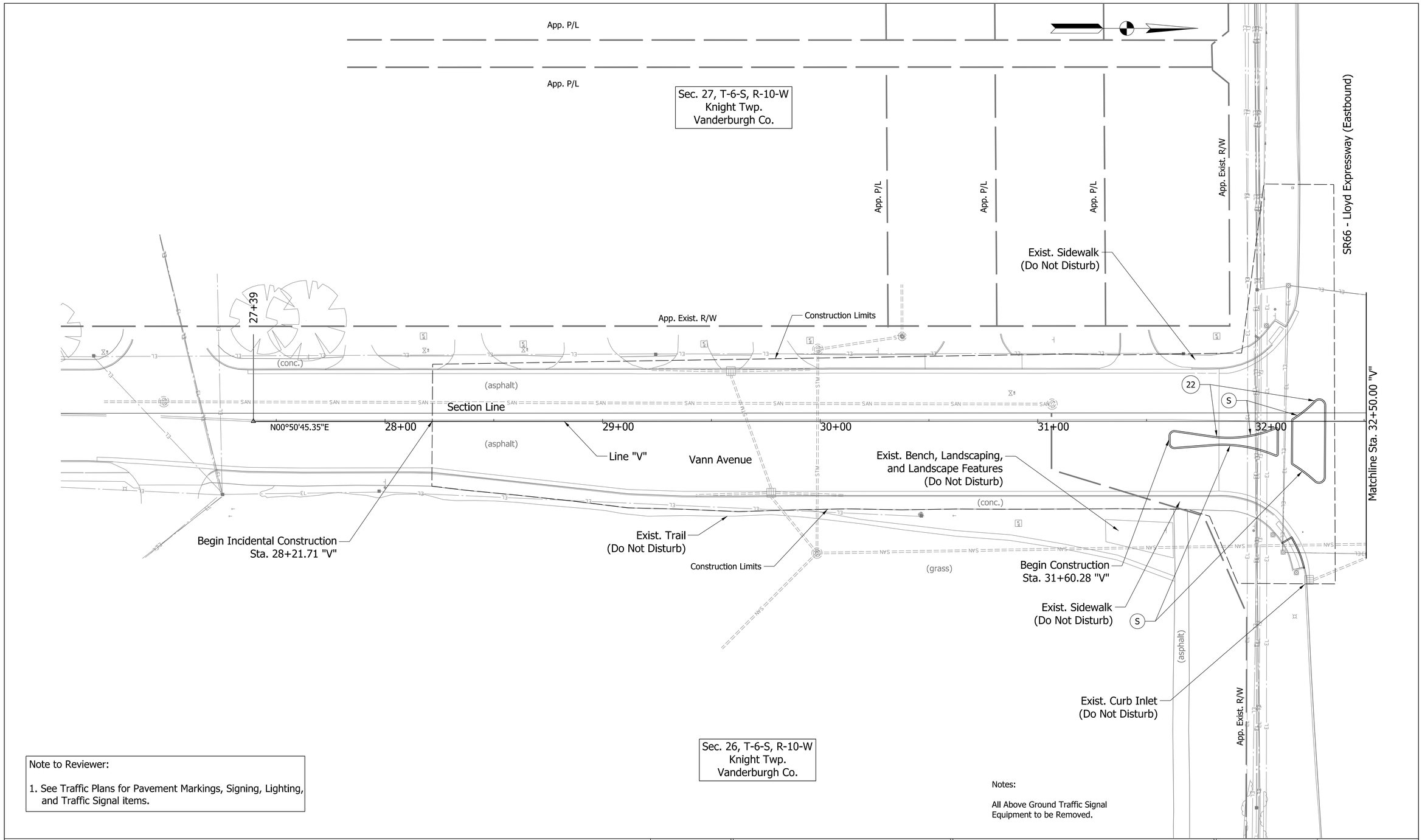


Sec. 26, T-6-S, R-10-W
Knight Twp.
Vanderburgh Co.

Note to Reviewer:

1. See Traffic Plans for Pavement Markings, Signing, Lighting, and Traffic Signal items.

Notes:

All Above Ground Traffic Signal Equipment to be Removed.

(1)	Concrete Barrier, 33"	(50)	Barrier Wall, Concrete, Remove		Sidewalk, Concrete, Remove
(13)	Curb, Concrete Vertical	(51)	Curb, Concrete, Remove	(A)	QC/QA PCCP, 10 in. on Subbase for PCCP (3 in. of CA No. 8 on 6 in. CA No.53) on Subgrade Treatment, Type IBC
(22)	Center Curb, Type B (4")	(52)	Impact Attenuator, Remove		
(27)	Mulched Seeding U	(53)	Traffic Signal Equipment, Remove	(S)	Sawcut

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE

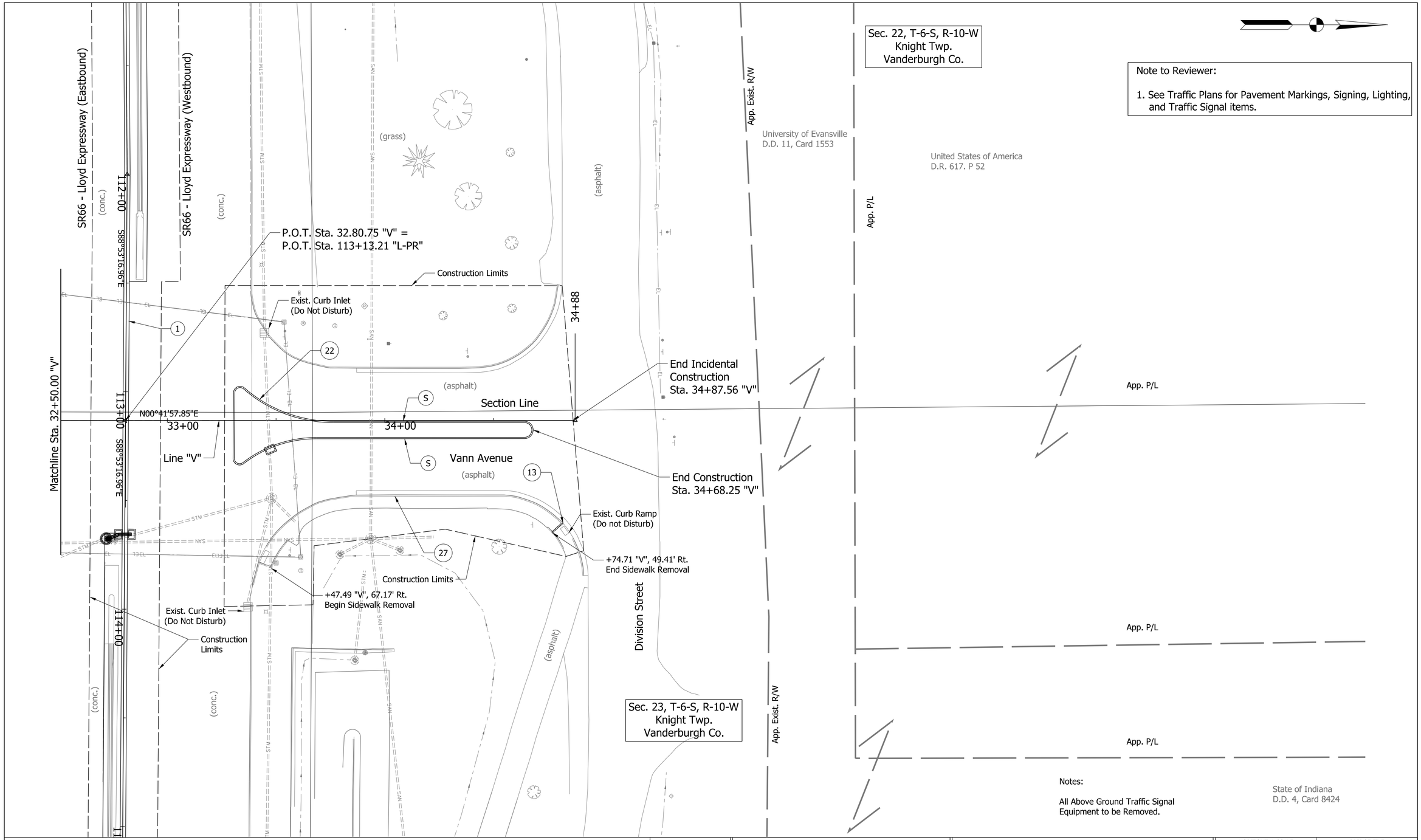
DESIGNED: SRB DRAWN: DH

CHECKED: CWB CHECKED: CWB

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 26+50.00 TO STA. 32+50.00
LINE "V"

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1900268
SURVEY BOOK	SHEETS
N/A	75 of 96
CONTRACT	PROJECT
R-42287	1900308



RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE

DESIGNED: SRB DRAWN: DH

CHECKED: CWB CHECKED: CWB

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 32+50.00 TO STA. 34+87.56
LINE "V"

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1900268
SURVEY BOOK	SHEETS
N/A	77 of 96
CONTRACT	PROJECT
R-42287	1900308

pw://VANVA01PWINT01.Parsons.com:Indiana State/Documents/SR66 Lloyd Expwy Corridor/CADD/Roadway/Sheets/Des. 1900268/VANN_Plan_02.dgn
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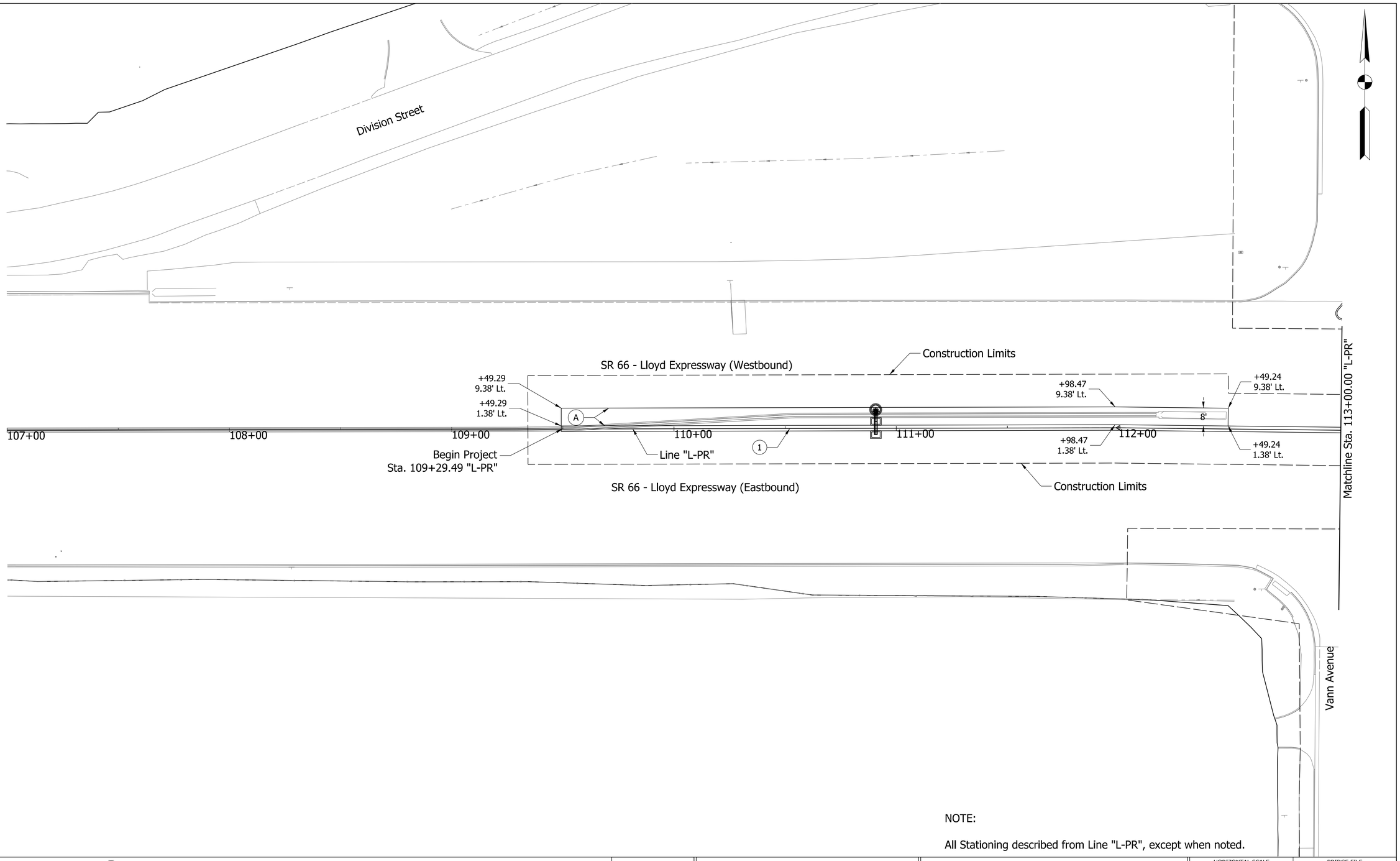


NOTE:

All Stationing described from Line "L-PR", except when noted.

(1)	Concrete Barrier, 33"	(50)	Barrier Wall, Concrete, Remove
(13)	Curb, Concrete Vertical	(51)	Curb, Concrete, Remove
(22)	Center Curb, Type B (4")	(52)	Impact Attenuator, Remove
(27)	Mulched Seeding U	(53)	Traffic Signal Equipment, Remove

Sidewalk, Concrete, Remove	
(A)	QC/QA PCCP, 10 in. on Subbase for PCCP (3 in. of CA No. 8 on 6 in. CA No.53) on Subgrade Treatment, Type IBC
(S)	Sawcut

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE

DESIGNED: SRB DRAWN: DH

CHECKED: CWB CHECKED: CWB

INDIANA
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAIL SHEET
STA. 107+00.00 TO STA. 113+00.00
LINE "L-PR"

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1900268
SURVEY BOOK	SHEETS
N/A	79 of 96
CONTRACT	PROJECT
R-42287	1900308

pw://VANVA01PWINT01.Parsons.com:Indiana State/Documents/SR66 Lloyd Expwy Corridor/CADD/Roadway/Sheets/Des. 1900268/UTIL_VANN_Lloyd_CONSTRUCTION DETAIL SHEET_01.dgn
$DATE$

STRUCTURE DATA

STRUCTURE NUMBER	LOCATION					SIZE	DESCRIPTION		LENGTH	VIDEO INSPECTION LENGTH	SKEW	COVER	FLOW LINE		SERVICE LIFE	SITE DESIGNATION	pH	METHOD	STRUCTURE BACKFILL	TYPE	FLOWABLE BACKFILL	TYPE	GEOTEXTILES FOR RIPRAP TYPE 1A	REVETMENT RIPRAP	SCOUR PROTECTION				CONCRETE, CLASS A, FOR STR.	VIDEO INSPECTION	PIPE END SECTION	GRATED BOX END SECTION			SAFETY METAL END SECTION			CONNECT TO STR.	REMARKS
	STATION	LEFT	RIGHT	CROSS	OFFSET		PIPE TYPE	MANHOLE, INLET, CATCH BASIN, OR SPECIALTY STRUCTURE AND TYPE					UP STREAM	DOWN STREAM											GEOTEXTILE FOR RIPRAP TYPE 1A	REVETMENT RIPRAP	CLASS 1 RIPRAP	CLASS 2 RIPRAP				TYPE	SLOPE	EA.	TYPE	SLOPE	EA.		
					FT	IN.			LFT	LFT		LFT	ELEV.	ELEV.	YRS				CYS		CYS		SYS	TON	SYS	TONS	TONS	TONS	CYS	LFT	EA.								
	Line "H"																																						
IN-107	110+91		X		2.9	12	2	INLET, TYPE H-5	5	5		1.6	383.45	383.41	75	NA	7																					MH-124	
MH-124	110+91	X			-7.7			MANHOLE, TYPE C-4																															Connect to Existing Pipe
IN-111	113+66		X		2.9	12	2	INLET, TYPE H-5																															Connect to Existing Pipe
MH-126	113+68		X		7.8	12	2	MANHOLE, TYPE C-4	5	5		2.3	384.10	383.89	75	NA	7																					IN-111	
IN-114	115+82		X		2.8	12	2	INLET, TYPE H-5																															Connect to Existing Pipe
MH-127	115+82		X		7.5	12	2	MANHOLE, TYPE C-4	5	5		2.1	388.85	388.78	75	NA	7																					IN-114	
	Line "V"																																						
MH-125	33+47		X		13.4			MANHOLE, TYPE J-15 MOD																															Connect to Existing Pipe

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE

DESIGNED: SRB DRAWN: TYW

CHECKED: CWB CHECKED: CWB

INDIANA
DEPARTMENT OF TRANSPORTATION

STRUTURE DATA TABLE

HORIZONTAL SCALE	BRIDGE FILE
N/A	N/A
VERTICAL SCALE	DESIGNATION
N/A	1900268
SURVEY BOOK	SHEETS
N/A	87 of 96
CONTRACT	PROJECT
R-42287	1900308

pw://VANVA01PWINT01.Parsons.com:Indiana State/Documents/SR66 Lloyd Expwy Corridor/CADD/Roadway/Sheets/Des. 1900268/VANN_Lloyd_RD_Sht_Structure Data Table
$DATE$

PROJECT	DESIGNATION
1900308	2000217
CONTRACT	
R-42287	

Excerpts

KIN PROJECT INFORMATION		
DESIGNATION	PROJECT DESCRIPTION	TYPE
1900308 (LEAD)	Road Reconstruction along SR 62 from Rosenberger Ave. to 2.72 mi W of S Jct. US-41	Roadway
1900317	SR 66 (Lloyd) at 0.58 miles W. of I-69 (Cross Pointe Blvd) - Intersection Improvements	Roadway
1900292	SR 66 (Lloyd) at 1.20 miles W. of I-69 (Burkhardt Rd) - Intersection Improvements	Roadway
1900263	SR 66 (Lloyd) at 3.09 miles W. of US 41 (St. Joseph Ave) - Intersection Improvements	Roadway
1900264	SR 66 (Lloyd) at 4.58 miles W. of US 41 (Rosenberger Ave) - Intersection Improvements	Roadway
1500041	SR 66 (Lloyd) over CSX Railroad & Evansville Western Railroad- Bridge Replacement	Roadway
1600060	SR 66 (Lloyd) over Tekoppei Ave - Bridge Replacement	Roadway
1602258	SR 66 (Lloyd) over Carpentier Creek - Bridge Replacement	Roadway

INDIANA DEPARTMENT OF TRANSPORTATION

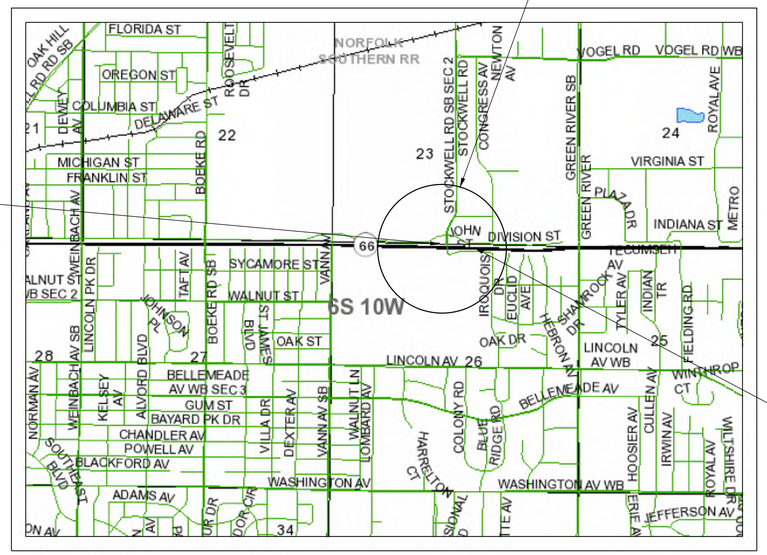
Excerpts

ROAD PLANS

ROUTE: SR66 - Lloyd Expressway - Stockwell Road AT: RP 29+0.4

PROJECT NO. 1900308 P.E.
1900308 R/W
1900308 CONST.

Stockwell Road Intersection Improvement at Lloyd Expressway
Sections 23, and 26 of T-6-S, R-10-W, Knight Township, Vanderburgh County.



Scale: 1" = 8000'

Traffic Data	SR66 (Lloyd Expressway)	Stockwell Road	Division Street
A.A.D.T. (2023)	54,348 V.P.D.	12,205 V.P.D.	N/A
A.A.D.T. (2043)	64,700 V.P.D.	14,529 V.P.D.	N/A
D.H.V (2043)	5,890 V.P.H.	1,414 V.P.H.	N/A
DIRECTIONAL DISTRIBUTION	54%	56%	N/A
TRUCKS	3% A.A.D.T.	3% A.A.D.T.	N/A
	3% D.H.V.	3% D.H.V.	N/A
Design Data			
DESIGN SPEED	50 M.P.H.	35 M.P.H.	30 M.P.H.
PROJECT DESIGN CRITERIA	3R (NON-FREEWAY)	3R (NON-FREEWAY)	4R (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	PRINCIPAL ARTERIAL (OTHER)	STATE COLLECTOR	LOCAL
RURAL/URBAN	URBAN (BUILT-UP)	URBAN (BUILT-UP)	URBAN (BUILT-UP)
TERRAIN	LEVEL	LEVEL	LEVEL
ACCESS CONTROL	NONE	NONE	NONE

PROJECT LOCATION SHOWN BY
VANDERBURGH COUNTY

LATITUDE: 37° 58' 36" N LONGITUDE: 87° 30' 09" W

HUC: 05140202040080

GROSS LENGTH: 0.44 MI.
NET LENGTH: 0.44 MI.
MAX. GRADE: 1.00 %

Note, maintenance of traffic (MOT) sheets are intentionally omitted. An MOT overview sheet was not prepared because an official detour is not needed.

Stage 2 Plans

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2022
TO BE USED WITH THESE PLANS

PARSONS
101 W. Ohio St., Suite 2121
Indianapolis, IN 46204
Bus (317) 616-1000
Fax (317) 616-1033

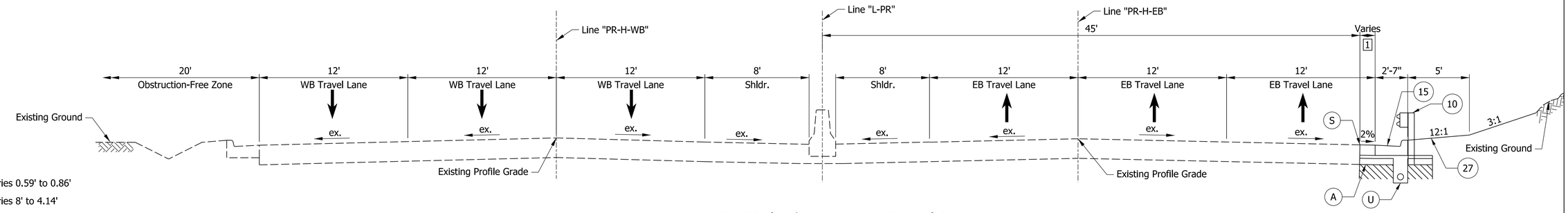
PLANS PREPARED BY: PARSONS 317-616-1000 PHONE NUMBER

CERTIFIED BY: _____ DATE

RECOMMENDED FOR LETTING: _____ INDIANA DEPARTMENT OF TRANSPORTATION DATE

DESIGNATION	
2000217	
SURVEY BOOK	SHEETS
N/A	1 of 129
CONTRACT	PROJECT
R-42287	1900308

pw://VANVA01PWINT01.Parsons.com:Indiana State/Documents/SR66 Lloyd Expwy Corridor/CADD/Roadway/Sheets/Des. 2000217/STOCKWELL_RD_Title Sheet.dgn
$DATE$



SR-66 Lloyd Expressway Typical Section

Sta. 117+58.15 to Sta. 118+53.44 Line "L-PR"

1 Varies 0.59' to 0.86'

2 Varies 8' to 4.14'

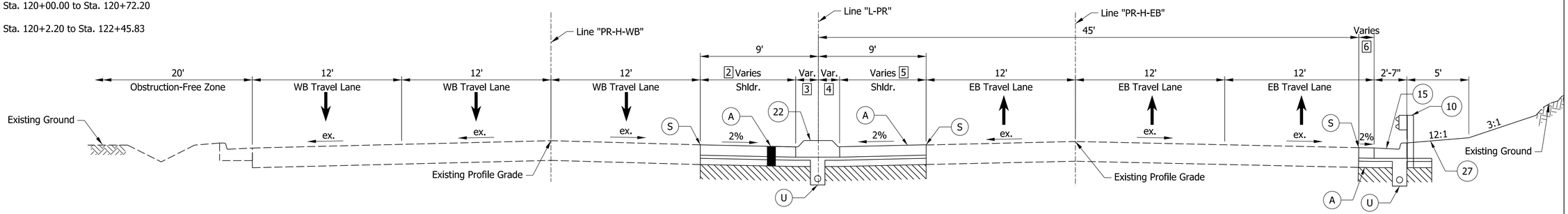
3 Varies 1.88' to 5.03'

4 Varies 1.02' to 7.51'

5 Varies 8' to 1.44'

6 Varies from 0.86' to 3.66'

7 Varies 4.02' to 1.75'
From Sta. 120+00.00 to Sta. 120+72.20
1.75'
From Sta. 120+2.20 to Sta. 122+45.83



SR-66 Lloyd Expressway Typical Section

Sta. 118+53.44 to Sta. 120+00.00 Line "L-PR"

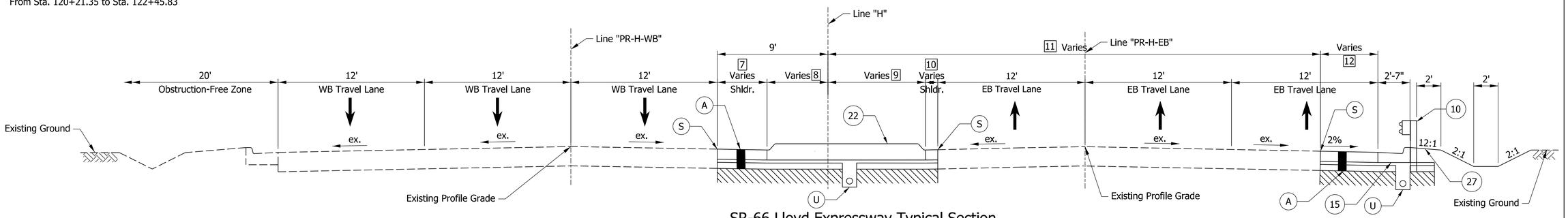
8 Varies 4.97' to 7.25'
From Sta. 120+00.00 to Sta. 120+72.20
7.25'
From Sta. 120+72.20 to Sta. 122+45.83

9 Varies 7.59' to 20.37'

10 Varies 1.40' to 0'
From Sta. 120+00.00 to Sta. 120+21.35
0'
From Sta. 120+21.35 to Sta. 122+45.83

11 45'
From Sta. 120+00.00 to Sta. 121+39.47
Varies 45' to 54.85'
From Sta. 121+39.47 to Sta. 122+45.83

12 Varies 3.74' to 8.85'
From Sta. 120+00.00 to Sta. 121+39.47
Varies 8.85' to 16.33'
From Sta. 121+39.47 to Sta. 122+45.83



SR-66 Lloyd Expressway Typical Section

Sta. 120+00.00 to Sta. 122+45.83 Line "H"

See Legend Sheet for Legend Items

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE

DESIGNED: SRB DRAWN: DH

CHECKED: CWB CHECKED: CWB

INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

HORIZONTAL SCALE	BRIDGE FILE
1" = 4'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000217
SURVEY BOOK	SHEETS
N/A	4 of 129
CONTRACT	PROJECT
R-42287	1900308

pw://VANVA01PWINT01.Parsons.com:Indiana State/Documents/SR66 Lloyd Expwy Corridor/CADD/Roadway/Sheets/Des. 2000217/STOCK_Lloyd_RD_Sht_Typ_01.dgn
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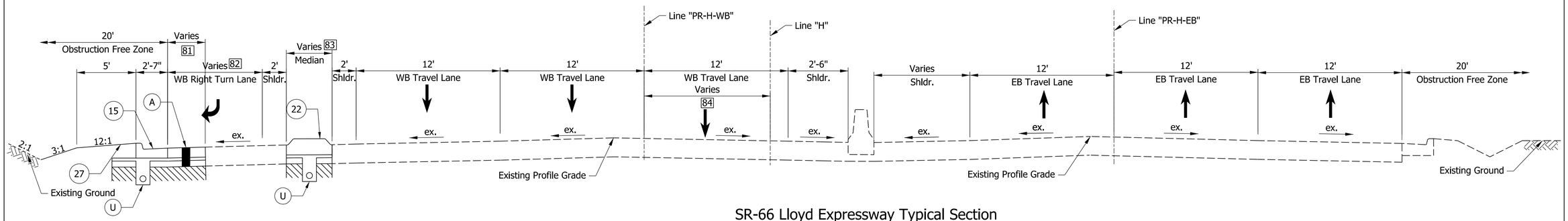


SR-66 Lloyd Expressway Typical Section

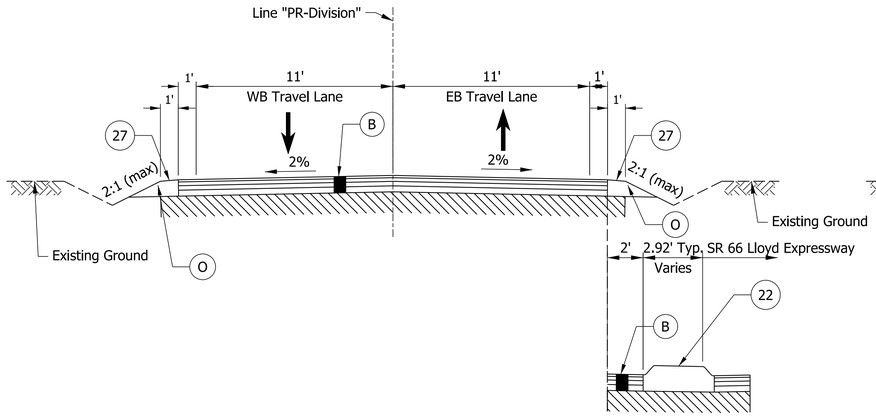
Sta. 138+00.11 to Sta. 140+82.92 Line "H"

81 Varies from 3.36' to 0.50'
Sta. 138+00.11 to 138+55.15
0'
Sta. 138+55.15 to 140+82.92

82 Varies 46.58' to 13'
From Sta. 138+00.11 to Sta. 139+67.41
13'
From Sta. 139+67.41 to Sta. 140+82.92

83 Varies 16' to 8.5'
From Sta. 138+00.11 to Sta. 138+30.93
8.5'
From Sta. 138+30.93 to Sta. 138+68.74
Varies 8.5' to 1.96'
From Sta. 138+68.74 to Sta. 140+82.92

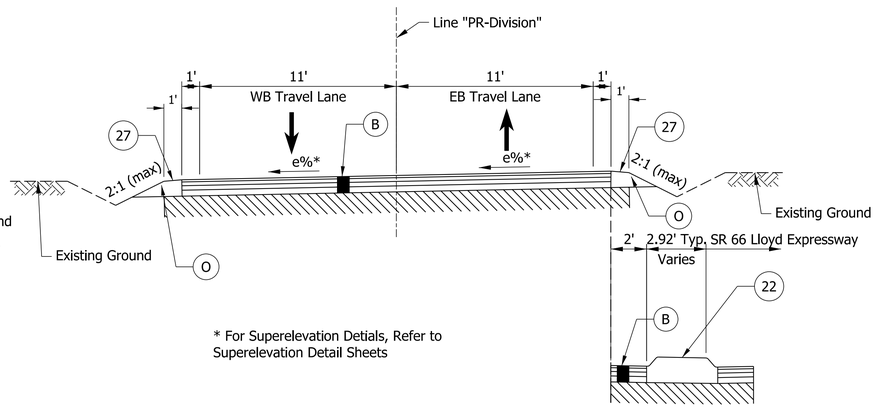
84 Varies 10.10' to 15.18'



Division Street Typical Section

Sta. 11+35.00 to Sta. 15+43.00

Sta. 12+99.69 to Sta. 15+43.00



* For Superelevation Detials, Refer to Superelevation Detail Sheets

Division Street Superelevated Section

Sta. 15+43.00 to Sta. 16+64.10

Sta. 15+43.00 to Sta. 18+62.00

See Legend Sheet for Legend Items

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE

DESIGNED: SRB DRAWN: DH

CHECKED: CWB CHECKED: CWB

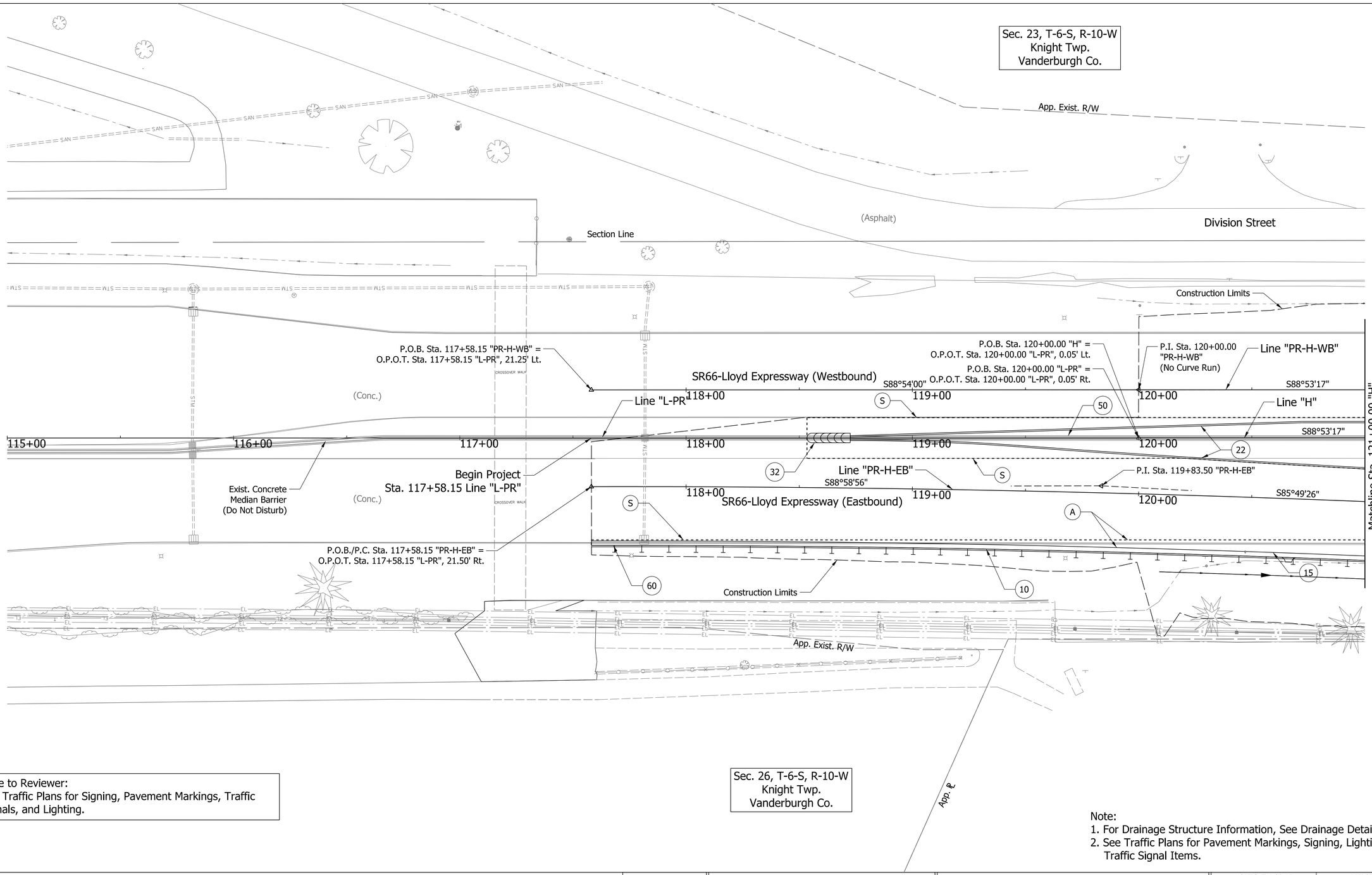
INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

HORIZONTAL SCALE	BRIDGE FILE
1" = 4'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000217
SURVEY BOOK	SHEETS
N/A	11 of 129
CONTRACT	PROJECT
R-42287	1900308

pw://VANVA01PWINT01.Parsons.com:Indiana State/Documents/SR66 Lloyd Expwy Corridor/CADD/Roadway/Sheets/Des. 2000217/STOCK_Lloyd_RD_Sht_Typ_08.dgn
$DATE$

Sec. 23, T-6-S, R-10-W
Knight Twp.
Vanderburgh Co.



Sec. 26, T-6-S, R-10-W
Knight Twp.
Vanderburgh Co.

Note to Reviewer:
See Traffic Plans for Signing, Pavement Markings, Traffic Signals, and Lighting.

Note:
1. For Drainage Structure Information, See Drainage Detail Sheets.
2. See Traffic Plans for Pavement Markings, Signing, Lighting, and Traffic Signal Items.

See Legend Sheet for Legend Items

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE

DESIGNED: SRB DRAWN: DH

CHECKED: CWB CHECKED: CWB

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 117+58.15 "L-PR" TO STA. 121+00.00 "H"

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000217
SURVEY BOOK	SHEETS
N/A	81 of 127
CONTRACT	PROJECT
R-42287	1900308

pw://VANVA01PWINT01.Parsons.com:Indiana State/Documents/SR66 Lloyd Expwy Corridor/CADD/Roadway/Sheets/Des. 2000217/STOCKWELL_Lloyd_Plan_01.dgn
$DATE$

Sec. 23, T-6-S, R-10-W
Knight Twp.
Vanderburgh Co.

App. ℄

App. Exist. R/W

Division Street

(Asphalt)

Section Line

10+00

Wetland 1
Emergent

Construction Limits

Line "PR-H-WB"

P.C. Sta. 125+95.60 "PR-H-WB"

(Conc.)

Str. No. MH-207

Str. No. IN-203

Str. No. IN-206

SR66-Lloyd Expressway (Westbound)

S88°53'17"E

Str. No. IN-211

Str. No. IN-218

121+00

Str. No. IN-201

122+00

123+00

Str. No. MH-205

Str. No. 208

Line "H"

124+00

Str. No. IN-216

50

125+00

S

32

Str. No. IN-214

126+00

Str. No. MH-216

Matchline Sta. 121+00.00 "H"

Matchline Sta. 127+00.00 "H"

121+00

Str. No. MH-267

122+00

Str. No. MH-202

123+00

Str. No. IN-209

S88°53'17"E

124+00

125+00

126+00

22

S

Str. No. IN-212

Str. No. IN-213

A

Str. No. IN-215

Str. No. MH-217

Str. No. IN-200

Str. No. IN-204

121+00

122+00

S85°49'26"E

123+00

SR66-Lloyd Expressway (Eastbound)

124+00

125+00

S88°53'17"E

126+00

Line "PR-H-EB"

P.T. Sta. 126+45.25 "PR-H-EB"

P.R.C. Sta. 122+08.74 "PR-H-EB"

P.I. Sta. 124+27.05 "PR-H-EB"

S

A

Wetland 3
Emergent

22

15

App. Exist. R/W

+35.00 "H"
℄ (88.96')

10

Construction Limits

62

Str. No. CLV-1

61

+35.00 "H"
114.00' Rt.

Prop. R/W

+80.00 "H"
114.00 Rt.

Prop. R/W

Prop. R/W

+50.00 "H"
122.00 Rt.

S

Class III Drive
+89 "H"
W = 26.8'

Sec. 26, T-6-S, R-10-W
Knight Twp.
Vanderburgh Co.

Note:
1. For Drainage Structure Information, See Drainage Detail Sheets.
2. See Traffic Plans for Pavement Markings, Signing, Lighting, and Traffic Signal Items.

See Legend Sheet for Legend Items

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE

DESIGNED: SRB DRAWN: DH

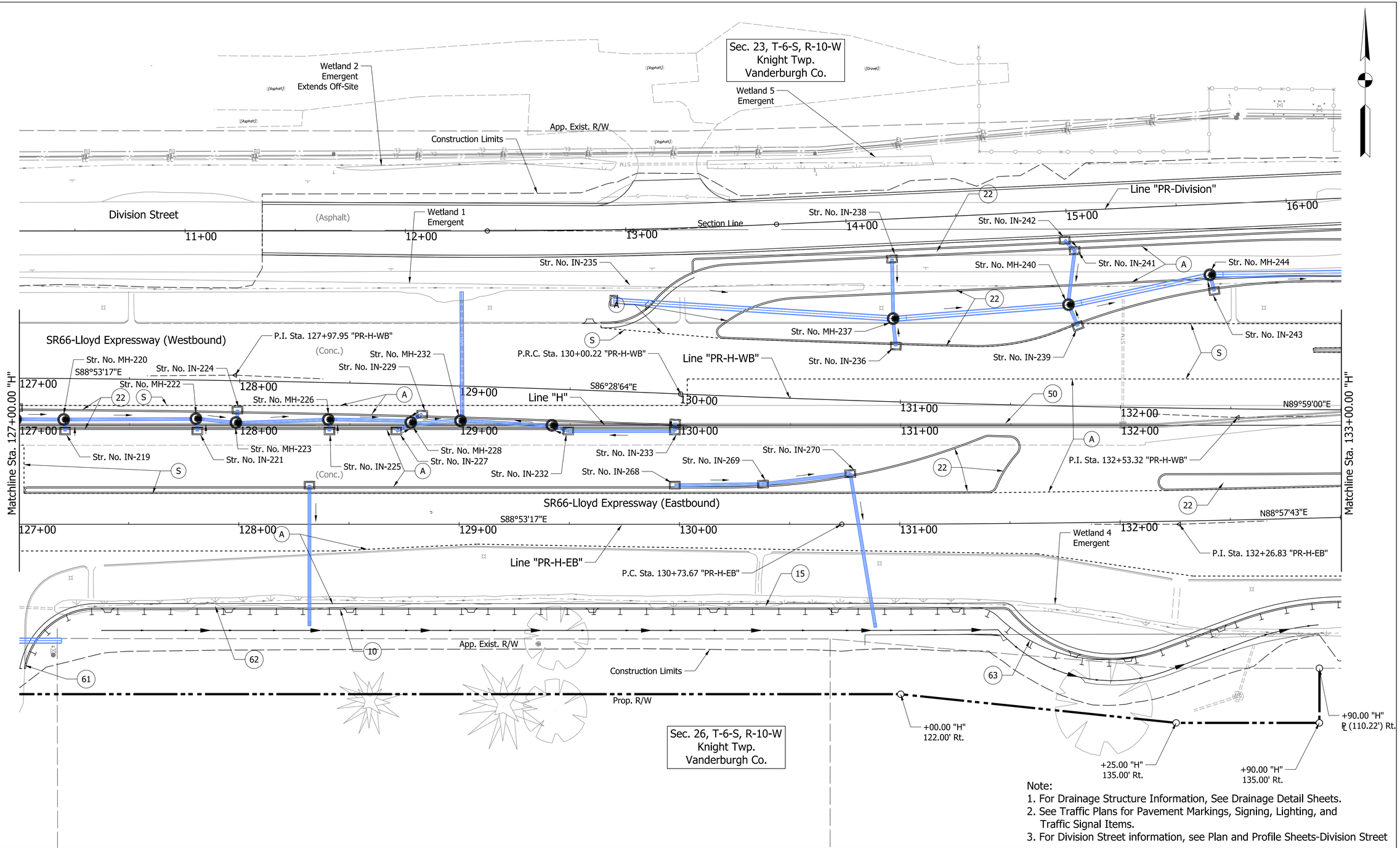
CHECKED: CWB CHECKED: CWB

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 121+00.00 TO STA. 127+00.00
LINE "H"

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000217
SURVEY BOOK	SHEETS
N/A	83 of 127
CONTRACT	PROJECT
R-42287	1900308

pw://VANVA01PWINT01.Parsons.com:Indiana State/Documents/SR66 Lloyd Expwy Corridor/CADD/Roadway/Sheets/Des. 2000217/STOCKWELL_Lloyd_Plan_02.dgn
$DATE$



Note:
1. For Drainage Structure Information, See Drainage Detail Sheets.
2. See Traffic Plans for Pavement Markings, Signing, Lighting, and Traffic Signal Items.
3. For Division Street information, see Plan and Profile Sheets-Division Street

See Legend Sheet for Legend Items

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE

DESIGNED: SRB DRAWN: DH

CHECKED: CWB CHECKED: CWB

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 127+00.00 TO STA. 133+00.00
LINE "H"

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000217
SURVEY BOOK	SHEETS
N/A	85 of 127
CONTRACT	PROJECT
R-42287	1900308

pw://VANVA01PWINT01.Parsons.com:Indiana State/Documents/SR66 Lloyd Expwy Corridor/CADD/Roadway/Sheets/Des. 2000217/STOCKWELL_Lloyd_Plan_03.dgn
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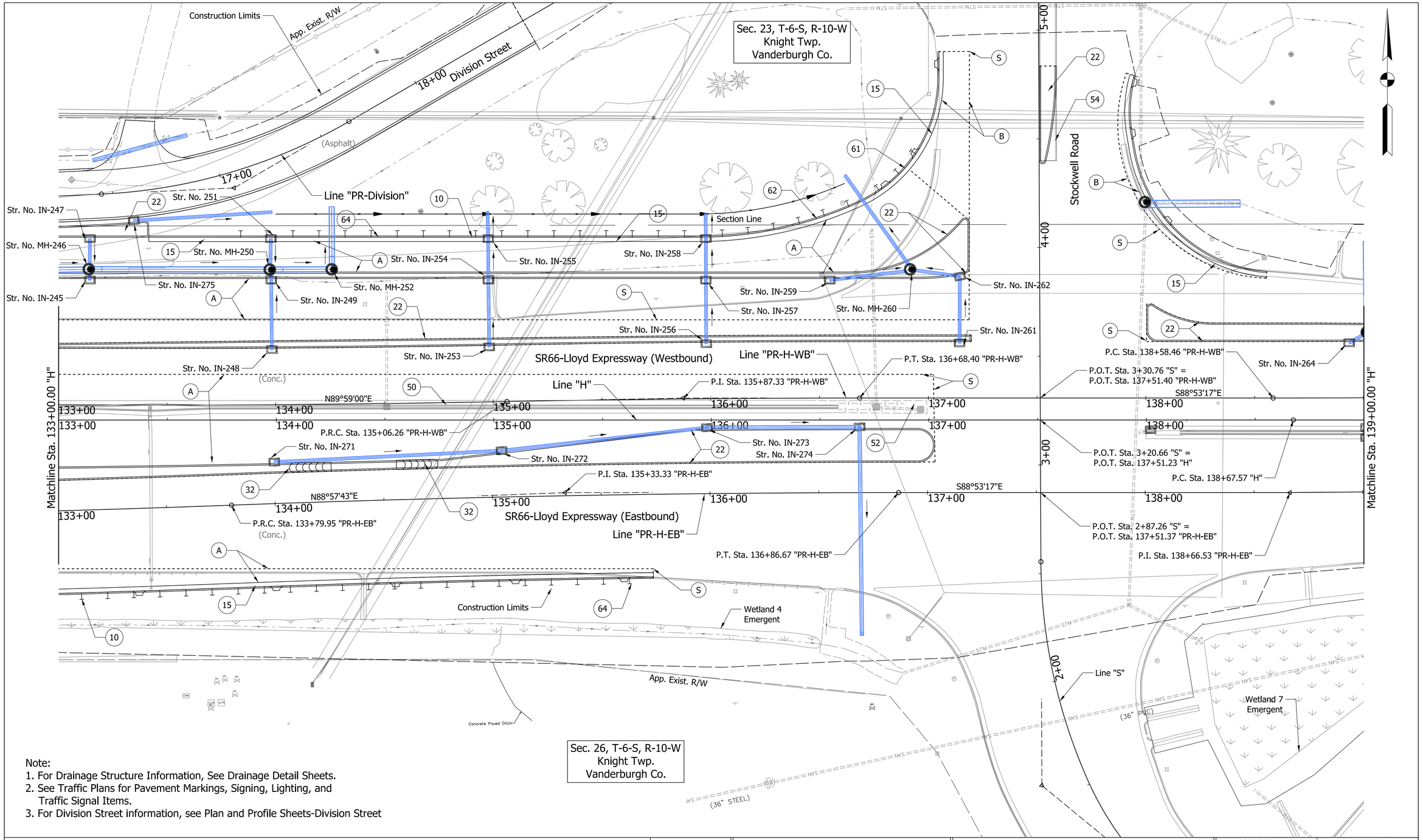

Note:
1. For Drainage Structure Information, See Drainage Detail Sheets.
2. See Traffic Plans for Pavement Markings, Signing, Lighting, and Traffic Signal Items.
3. For Division Street information, see Plan and Profile Sheets-Division Street

See Legend Sheet for Legend Items

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE

DESIGNED: SRB DRAWN: DH

CHECKED: CWB CHECKED: CWB

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 133+00.00 TO STA. 139+00.00
LINE "H"

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000217
SURVEY BOOK	SHEETS
N/A	87 of 127
CONTRACT	PROJECT
R-42287	1900308

pw://VANVA01PWINT01.Parsons.com:Indiana State/Documents/SR66 Lloyd Expwy Corridor/CADD/Roadway/Sheets/Des. 2000217/STOCKWELL_Lloyd_Plan_04.dgn
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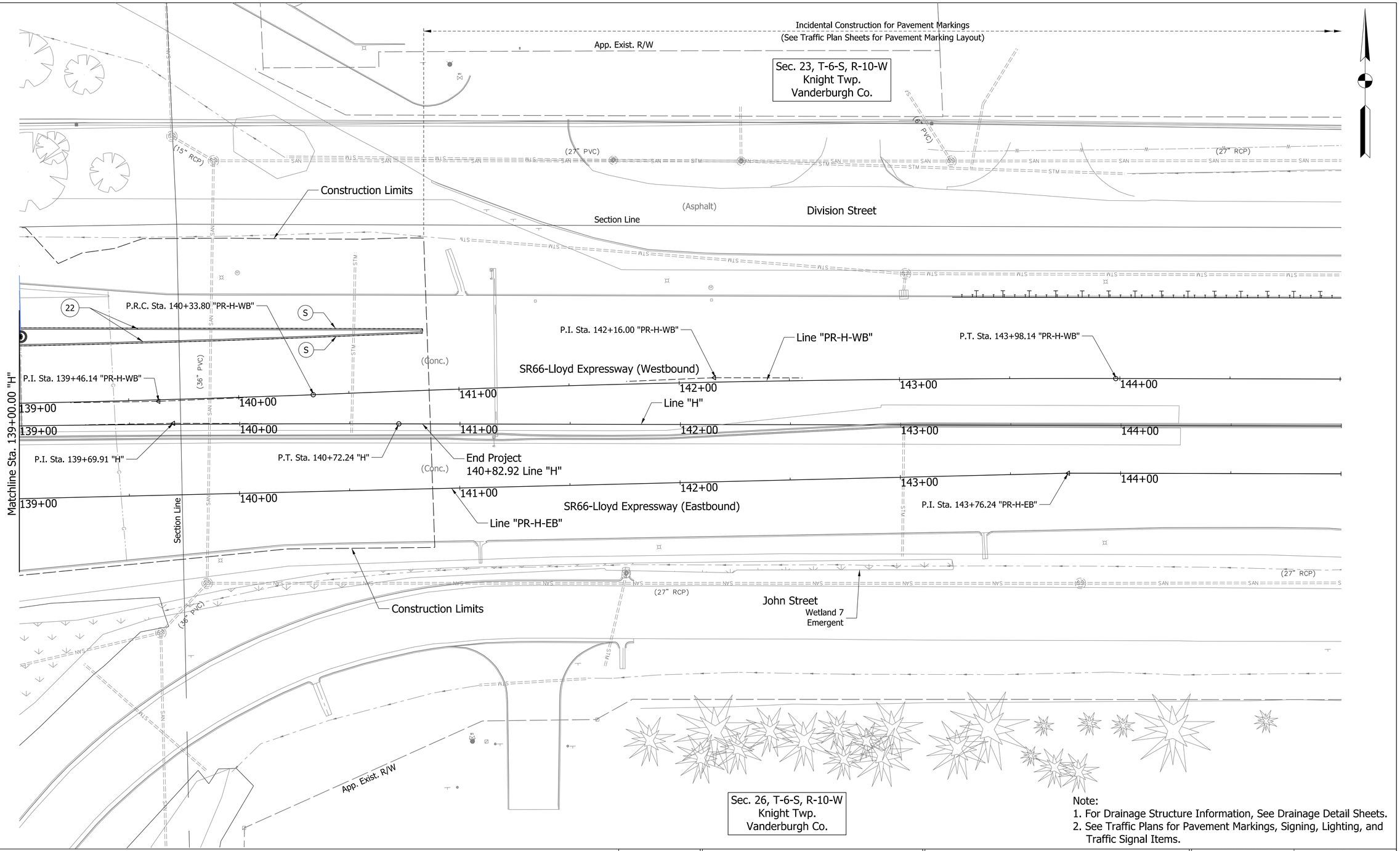


Note:
1. For Drainage Structure Information, See Drainage Detail Sheets.
2. See Traffic Plans for Pavement Markings, Signing, Lighting, and Traffic Signal Items.

See Legend Sheet for Legend Items

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE

DESIGNED: SRB DRAWN: DH

CHECKED: CWB CHECKED: CWB

INDIANA
DEPARTMENT OF TRANSPORTATION

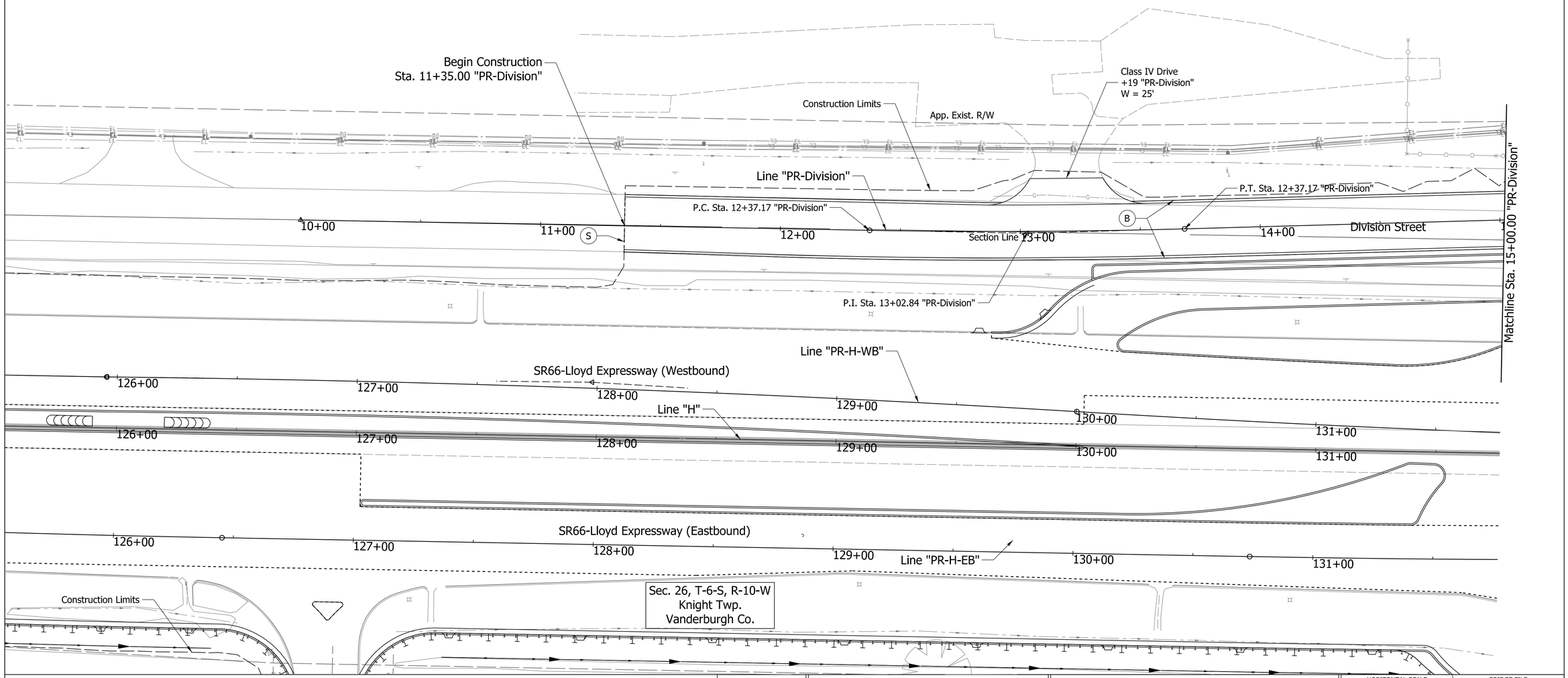
PLAN SHEET
STA. 139+00.00 TO STA. 145+00.00
LINE "H"

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000217
SURVEY BOOK	SHEETS
N/A	89 of 127
CONTRACT	PROJECT
R-42287	1900308

pw://VANVA01PWINT01.Parsons.com:Indiana State/Documents/SR66 Lloyd Expwy Corridor/CADD/Roadway/Sheets/Des. 2000217/STOCKWELL_Lloyd_Plan_05.dgn
$DATE$

Note:
1. For Drainage Structure Information, See Drainage Detail Sheets.
2. See Traffic Plans for Pavement Markings, Signing, Lighting, and Traffic Signal Items.

Sec. 23, T-6-S, R-10-W
Knight Twp.
Vanderburgh Co.



See Legend Sheet for Legend Items

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE

DESIGNED: SRB DRAWN: DH

CHECKED: CWB CHECKED: CWB

INDIANA
DEPARTMENT OF TRANSPORTATION

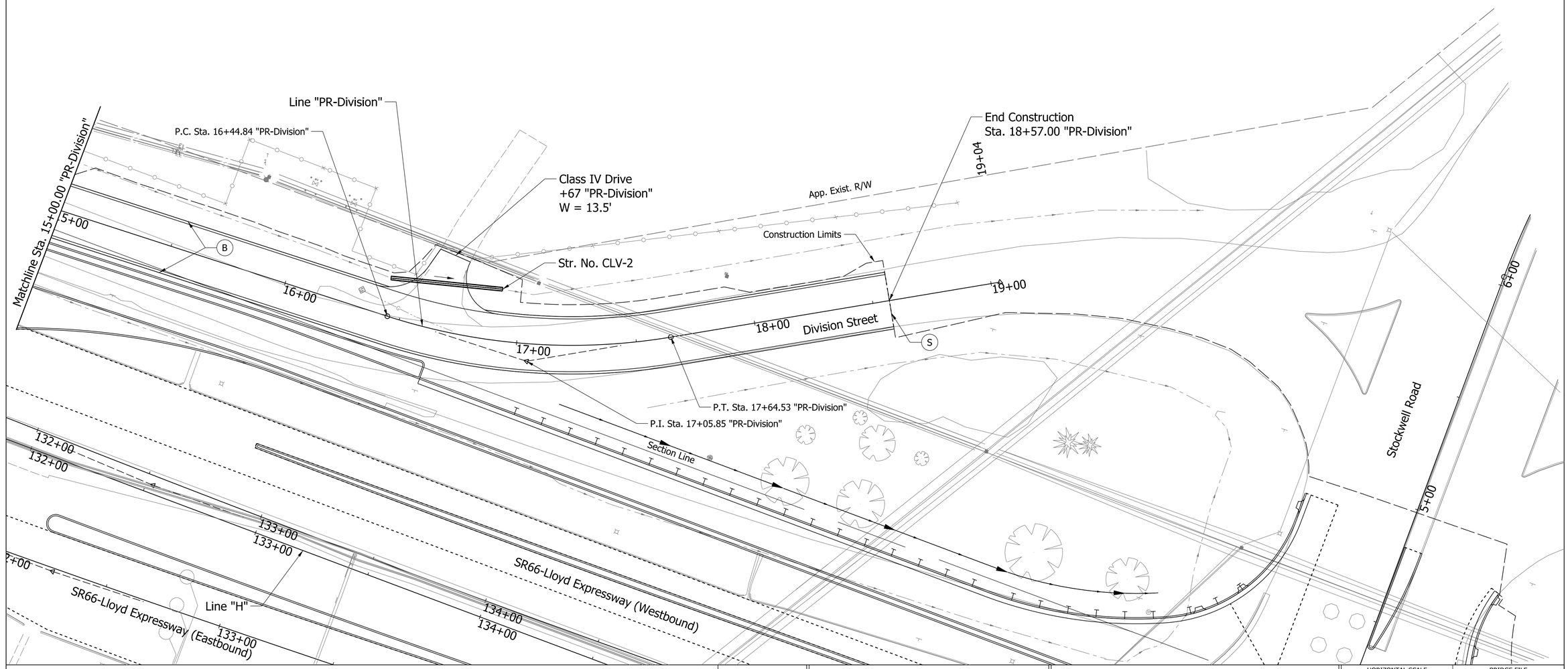
PLAN SHEET
STA. 11+35.00 TO STA. 15+00.00
LINE "PR DIVISION"

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000217
SURVEY BOOK	SHEETS
N/A	91 of 127
CONTRACT	PROJECT
R-42287	1900308

pw://VANVA01PWINT01.Parsons.com:Indiana State/Documents/SR66 Lloyd Expwy Corridor/CADD/Roadway/Sheets/Des. 2000217/STOCKWELL_Division_Plan_01.dgn
$DATE$

Sec. 23, T-6-S, R-10-W
Knight Twp.
Vanderburgh Co.

Note:
1. For Drainage Structure Information, See Drainage Detail Sheets.
2. See Traffic Plans for Pavement Markings, Signing, Lighting, and Traffic Signal Items.



See Legend Sheet for Legend Items

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE: 1" = 20' / BRIDGE FILE: N/A / VERTICAL SCALE: N/A / DESIGNATION: 2000217
DESIGNED: SRB DRAWN: DH / CHECKED: CWB CHECKED: CWB	PLAN SHEET STA. 15+00.00 TO STA. 18+57.00 LINE "PR-Division"	SURVEY BOOK: N/A / SHEETS: 93 of 127 / CONTRACT: R-42287 / PROJECT: 1900308

pw://VANVA01PWINT01.Parsons.com:Indiana State/Documents/SR66 Lloyd Expwy Corridor/CADD/Roadway/Sheets/Des. 2000217/STOCKWELL_Division_Plan_02.dgn
$DATE$

STRUCTURE DATA

STRUCTURE NUMBER	LOCATION					SIZE	DESCRIPTION		LENGTH	VIDEO INSPECTION LENGTH	SKEW	COVER	FLOW LINE		SERVICE LIFE	SITE DESIGNATION	pH	BACKFILL METHOD	STRUCTURE BACKFILL	TYPE	FLOWABLE BACKFILL	TYPE	GEOTEXTILES FOR RIPRAP TYPE 1A	REVETMENT RIPRAP	SCOUR PROTECTION				CONCRETE, CLASS A, FOR STR.	VIDEO INSPECTION	PIPE END SECTION	GRATED BOX END SECTION			SAFETY METAL END SECTION			CONNECT TO STR.	REMARKS
	STATION	LEFT	RIGHT	CROSS	OFFSET		PIPE TYPE	MANHOLE, INLET, CATCH BASIN, OR SPECIALTY STRUCTURE AND TYPE					UP STREAM	DOWN STREAM											GEOTEXTILE FOR RIPRAP TYPE 1A	REVETMENT RIPRAP	CLASS 1 RIPRAP	CLASS 2 RIPRAP				TYPE	SLOPE	EA.	TYPE	SLOPE	EA.		
					FT	IN.			LFT	LFT		LFT	ELEV.	ELEV.	YRS				CYS		CYS		SYS	TON	SYS	TONS	TONS	TONS	CYS	LFT	EA.								
	Line "H"																																						
CT-200	120+77		X		52.9			CURB TURNOUT																															
IN-200	121+18		X		15.3	12	2	INLET, TYPE B-15	18.0	18.0		2.7	393.58	393.49	75	NA	7																				MH-267	
MH-267	121+18	X			-2.8	12	2	MANHOLE, TYPE C-4	56.0	56.0		3.8	393.49	393.17	75	NA	7																				MH-202	
IN-201	121+18	X			-7.9	12	2	INLET, TYPE B-15	2.0	2.0		2.7	393.75	393.75	75	NA	7																				MH-267	
CT-202	121+26		X		55.3			CURB TURNOUT																															
MH-202	121+79	X			-2.7	12	2	MANHOLE, TYPE C-4	56.0	56.0		3.8	393.19	392.89	75	NA	7																				MH-205	
IN-203	121+79	X			-7.91	12	2	INLET, TYPE B-15	3.0	3.0		2.7	393.60	393.55	75	NA	7																				MH-202	
CT-204	121+86		X		63.4			CURB TURNOUT																															
MH-205	122+39	X			-2.7	12	2	MANHOLE, TYPE C-4	58	58		3.8	392.89	392.58	75	NA	7																				MH-207	
IN-206	122+39	X			-7.9	12	2	INLET, TYPE B-15	3	3		2.7	393.47	393.42	75	NA	7																				MH-205	
IN-204	122+41		X		20.9	12	2	INLET, TYPE B-15	22	22		2.7	393.66	392.89	75	NA	7																				MH-205	
CT-205	122+50		X		73.4			CURB TURNOUT																															
MH-207	123+01	X			-2.7	12	2	MANHOLE, TYPE C-4	53	53		4	392.58	392.27	75	NA	7																				OT-1	Outfall
IN-208	123+06	X			-7.9	12	2	INLET, TYPE B-15	6	6		3.2	392.72	392.58	75	NA	7																				MH-207	
CT-206	123+10		X		76.1			CURB TURNOUT																															
IN-209	123+59	X			-7.9	12	2	INLET, TYPE B-15	60	60		3.4	392.40	392.18	75	NA	7																				IN-216	
CT-208	123+70		X		78.3			CURB TURNOUT																															
IN-216	124+19	X			-8	12	2	INLET, TYPE C-15	68	68		3.5	392.18	391.93	75	NA	7																				IN-211	
CT-210	124+30		X		80.1			CURB TURNOUT																															
CT-211	124+85		X		81.3			CURB TURNOUT																															
IN-211	124+87	X			-8	12	2	INLET, TYPE C-15	113	113		3.6	391.93	391.51	75	NA	7																				IN-214	
IN-212	125+35		X		3.9	12	2	INLET, TYPE B-15	65	65		3.5	391.79	391.55	75	NA	7																				IN-213	
CT-212	125+40		X		82.2			CURB TURNOUT																															
CT-214	125+95		X		82.7			CURB TURNOUT																															
IN-214	126+01	X			-8	15	2	INLET, TYPE C-15	49	49		4	391.51	391.32	75	NA	7																				OT-2	Outfall
IN-213	126+01		X		2.6	12	2	INLET, TYPE C-15	11	11		3.6	391.55	391.51	75	NA	7																				IN-214	
CT-215	126+30		X		82.9			CURB TURNOUT																															
IN-215	126+61		X		2.6	12	2	INLET, TYPE B-15	3	3		3.8	391.08	391.06	75	NA	7																				MH-216	
MH-216	126+61	X			-2.8	12	2	MANHOLE, TYPE C-4	34	34		7.3	391.06	390.96	75	NA	7																				MH-217	
IN-218	126+99	X			-8	12	2	INLET, TYPE B-15	3	3		3.8	391.19	390.96	75	NA	7																				MH-217	
MH-217	127+00	X			-2.7	12	2	MANHOLE, TYPE C-4	18	18		4.4	390.96	390.90	75	NA	7																				MH-220	
MH-220	127+21	X			-2.6	12	2	MANHOLE, TYPE C-4	56	56		4.4	390.90	390.74	75	NA	7																				MH-222	
IN-219	127+21		X		2.7	12	2	INLET, TYPE B-15	3	3		3.8	391.10	390.90	75	NA	7																				MH-220	
CT-217	127+40		X		82.9			CURB TURNOUT																															
MH-222	127+81	X			-3	12	2	MANHOLE, TYPE C-4	14	14		4.5	390.74	390.69	75	NA	7																				MH-223	
IN-221	127+81		X		2.6	12	2	INLET, TYPE B-15	4	4		3.8	390.90	390.74	75	NA	7																				MH-222	
CT-218	127+95		X		82.9			CURB TURNOUT																															
IN-224	127+99	X			-6.9	12	2	INLET, TYPE B-15	4	4		3.8	390.96	390.85	75	NA	7																				MH-223	
MH-223	128+00	X			-1.3	12	2	MANHOLE, TYPE C-4	38	38		4.4	390.69	390.58	75	NA	7																				MH-226	
IN-267	128+32		X		27.2	12	2	INLET, TYPE B-15	64	64		3.4	391.42	391.21	75	NA	7																				OT-11	Outfall
MH-226	128+41	X			-2.6	12	2	MANHOLE, TYPE C-4	34	34		4.4	390.58	390.48	75	NA	7																				MH-228	
IN-225	128+41		X		2.5	12	2	INLET, TYPE B-15	3	3		3.8	390.78	390.76	75	NA	7																				MH-226	
CT-219	128+50		X		82.9			CURB TURNOUT																															
IN-227	128+71		X		2.5	12	2	INLET, TYPE B-15	6	6		3.8	390.70	390.48	75	NA	7																				MH-228	
MH-228	128+78	X			-1.3	15	2	MANHOLE, TYPE C-4	19	19		4.7	390.48	390.42	75	NA	7																				MH-232	
IN-229	128+83	X			-4.9	12	2	INLET, TYPE B-15	4	4		3.8	390.60	390.48	75	NA	7																				MH-228	
MH-232	129+01	X			-2.1	15	2	MANHOLE, TYPE C-4	57	57		4.5	390.42	390.11	75	NA	7																				OT-3	Outfall
CT-221	129+05		X		82.9			CURB TURNOUT																															
MH-231	129+42	X			-0.1	12	2	MANHOLE, TYPE C-4	38	38		4.3	390.53	390.42	75	NA	7																				MH-232	
IN-232	129+49		X		2.6	12	2	INLET, TYPE C-15	6	6		3.8	390.74	390.53	75	NA	7																				MH-231	
CT-223	129+59	X			-46.2			CURB TURNOUT																															
CT-222	129+60		X		82.9			CURB TURNOUT																															
IN-235	129+79	X			-62.2	30	2	INLET, TYPE P-12	117	117		2	388.18	388.06	75	NA	7																				MH-237	
CT-224	129+87	X			-53.7			CURB TURNOUT																															
IN-233	129+97		X		2.6	12	2	INLET, TYPE C-15	48	48		2.8	391.70	390.74	75	NA	7																				IN-232	
IN-268	129+97		X		27.2	12	2	INLET, TYPE B-15	40	40		3.4	391.07	390.79	75	NA	7																				IN-269	
IN-234	129+98	X			-0.9	12	2	INLET, TYPE B-15	4	4		3.4	391.72	391.70	75	NA	7																				IN-233	

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE

DESIGNED: SRB DRAWN: TYW

CHECKED: CWB CHECKED: CWB

INDIANA
DEPARTMENT OF TRANSPORTATION

STRUCTURE DATA TABLE

HORIZONTAL SCALE	BRIDGE FILE
N/A	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000217
SURVEY BOOK	SHEETS
N/A	108 of 127
CONTRACT	PROJECT
R-42287	1900308

pw:/VANVA01PWINT01.Parsons.com:Indiana State/Documents/SR66 Lloyd Expwy Corridor/CADD/Roadway/Sheets/Des. 2000217/STOCKWELL_Lloyd_RD_Sht_Structure Data Table01
$DATE$

STRUCTURE DATA

STRUCTURE NUMBER	LOCATION					SIZE	DESCRIPTION		LENGTH	VIDEO INSPECTION LENGTH	SKEW	COVER	FLOW LINE		SERVICE LIFE	SITE DESIGNATION	pH	METHOD	STRUCTURE BACKFILL	TYPE	FLOWABLE BACKFILL	TYPE	GEOTEXTILES FOR RIPRAP TYPE 1A	REVETMENT RIPRAP	SCOUR PROTECTION				CONCRETE, CLASS A, FOR STR.	VIDEO INSPECTION	PIPE END SECTION	GRATED BOX END SECTION			SAFETY METAL END SECTION			CONNECT TO STR.	REMARKS
	STATION	LEFT	RIGHT	CROSS	OFFSET	SIZE	PIPE TYPE	MANHOLE, INLET, CATCH BASIN, OR SPECIALTY STRUCTURE AND TYPE	LENGTH	VIDEO INSPECTION LENGTH	SKEW	COVER	UP STREAM	DOWN STREAM	SERVICE LIFE	SITE DESIGNATION	pH	BACKFILL METHOD	STRUCTURE BACKFILL	TYPE	FLOWABLE BACKFILL	TYPE	GEOTEXTILES FOR RIPRAP TYPE 1A	REVETMENT RIPRAP	GEOTEXTILE FOR RIPRAP TYPE 1A	REVETMENT RIPRAP	CLASS 1 RIPRAP	CLASS 2 RIPRAP	CONCRETE, CLASS A, FOR STR.	VIDEO INSPECTION	PIPE END SECTION	TYPE	SLOPE	EA.	TYPE	SLOPE	EA.	CONNECT TO STR.	REMARKS
					FT	IN.			LFT	LFT		LFT	ELEV.	ELEV.	YRS				CYS		CYS		SYS	TON	SYS	TONS	TONS	TONS	CYS	LFT	EA.								
	Line "H"																																						
CT-225	130+15		X		82.9			CURB TURNOUT																															
IN-269	130+37		X		26.7	12	2	INLET, TYPE C-15	40	40		3.4	390.79	390.51	75	NA	7																					IN-270	
CT-226	130+70		X		82.9			CURB TURNOUT																															
IN-270	130+77		X		22	12	2	INLET, TYPE C-15	71	71		3.5	390.51	390.16	75	NA	7																					OT-283	Outfall
MH-237	130+97	X			-48.4	30	2	MANHOLE, TYPE H-4	76	76		8	388.06	387.06	75	NA	7																					MH-240	
IN-236	130+98	X			-36	12	2	INLET, TYPE B-15	10	10		4.3	389.78	389.65	75	NA	7																					MH-237	
IN-238	131+81	X			-45	12	2	INLET, TYPE B-15	25	25		3.8	389.46	389.20	75	NA	7																					MH-237	
CT-227	131+25		X		82.7			CURB TURNOUT																															
IN-241	131+74	X			-83.9	12	2	INLET, TYPE C-15	23	23		5.2	388.16	387.76	75	NA	7																					MH-240	
IN-242	131+74	X			-84.1	12	2	INLET, TYPE B-15	7	7		3.3	389.92	389.85	75	NA	7																					IN-241	
MH-240	131+77	X			-54.7	30	2	MANHOLE, TYPE H-4	62	62		7.7	387.76	387.51	75	NA	7																					MH-244	
IN-239	131+79	X			-79.1	12	2	INLET, TYPE B-15	9	9		4.4	389.30	389.20	75	NA	7																					MH-240	
CT-246	131+79		X		103.7			CURB TURNOUT																															
CT-247	132+03		X		105.3			CURB TURNOUT																															
MH-244	132+41	X			-68.2	30	2	MANHOLE, TYPE H-4	70	70		7	387.51	387.23	75	NA	7																					MH-246	
IN-243	132+42	X			-60.8	12	2	INLET, TYPE B-15	6	6		3.8	389.35	389.19	75	NA	7																					MH-244	
CT-228	132+45		X		91.4			CURB TURNOUT																															
IN-245	133+14	X			-64.4	12	2	INLET, TYPE B-15	3	3		3.8	389.16	389.11	75	NA	7																					MH-246	
IN-247	133+14	X			-83.4	12	2	INLET, TYPE B-15	12	12		3.8	388.75	388.47	75	NA	7																					MH-246	
MH-246	133+15	X			-69.4	30	2	MANHOLE, TYPE H-4	79	79		7.2	387.23	386.91	75	NA	7																					MH-250	
IN-275	133+35	X			-91.7	12	2	INLET, TYPE B-15	64	64		2.4	390.53	390.24	75	NA	7																					OT-6	Outfall
CT-229	133+37		X		78.7			CURB TURNOUT																															
MH-250	133+98	X			-69.2	30	2	MANHOLE, TYPE H-4	24	24		7.2	386.91	386.80	75	NA	7																					MH-252	
IN-249	133+98	X			-64.4	12	2	INLET, TYPE C-15	3	3		3.7	389.00	388.91	75	NA	7																					MH-250	
IN-248	133+98	X			-32.5	12	2	INLET, TYPE B-15	32	32		3.8	389.58	389.00	75	NA	7																					IN-249	
IN-251	133+98	X			-83.4	12	2	INLET, TYPE B-15	12	12		4.6	387.80	387.65	75	NA	7																					MH-250	
CT-230	133+98		X		77.4			CURB TURNOUT																															
IN-271	133+99		X		19.3	12	2	INLET, TYPE B-15	105	105		3.4	389.59	389.17	75	NA	7																					IN-272	
MH-252	134+26	X			-69.3	30	2	MANHOLE, TYPE H-4	27	27		7.3	386.80	386.69	75	NA	7																					HW-1	Outfall
CT-231	134+55		X		74.7			CURB TURNOUT																															
IN-255	134+97	X			-83.4	12	2	INLET, TYPE C-15	13	13		4.3	387.87	387.65	75	NA	7																					HW-4	
IN-254	134+98	X			-64.4	12	2	INLET, TYPE C-15	19	19		4.3	388.21	387.87	75	NA	7																					IN-255	
IN-253	134+98	X			-33.7	12	2	INLET, TYPE B-15	31	31		4.3	388.77	388.21	75	NA	7																					IN-254	
IN-272	135+04		X		14.1	12	2	INLET, TYPE B-15	95	95		3.6	389.17	388.79	75	NA	7																					IN-273	
CT-232	135+10		X		73.3			CURB TURNOUT																															
CT-233	135+60		X		72.4			CURB TURNOUT																															
IN-257	135+97	X			-64.5	12	2	INLET, TYPE C-15	19	19		3.8	388.40	388.02	75	NA	7																					IN-258	
IN-256	135+98	X			-35.11	12	2	INLET, TYPE B-15	29	29		3.8	389.01	388.40	75	NA	7																					IN-257	
IN-258	135+98	X			-83.5	12	2	INLET, TYPE C-15	11	11		3.9	388.02	387.80	75	NA	7																					HW-3	Outfall
IN-273	135+98		X		3.5	15	2	INLET, TYPE C-15	70	70		4.2	388.79	388.51	75	NA	7																					IN-274	
IN-259	136+55	X			-64.4	12	2	INLET, TYPE B-15	35	35		3.8	388.26	387.34	75	NA	7																					MH-260	
IN-274	136+68		X		3.2	15	2	INLET, TYPE C-15	96	96		4.3	388.51	388.00	75	NA	7																					OT-8	Outfall
CT-235	136+81	X			-107.2			CURB TURNOUT																															
MH-260	136+92	X			-69.3	12	2	MANHOLE, TYPE C-4	51	51		5.1	387.34	387.16	75	NA	7																					HW-2	Outfall
CT-236	136+95	X			-122.1			CURB TURNOUT																															
CT-237	137+05	X			-163.7			CURB TURNOUT																															
IN-262	137+14	X			-65.9	12	2	INLET, TYPE C-15	21	21		4.4	387.51	387.34	75	NA	7																					MH-260	
IN-261	137+14	X			-35.4	12	2	INLET, TYPE B-15	31	31		4.8	387.71	387.51	75	NA	7																					IN-262	
CT-244	137+93	X			-131.8			CURB TURNOUT																															
CT-245	137+94	X			-155			CURB TURNOUT																															
CT-243	138+09	X			-91.7			CURB TURNOUT																															
IN-264	138+93	X			-35.5	12	2	INLET, TYPE B-15	8	8		3.3	388.88	388.78	75	NA	7																					MH-265	
MH-265	139+00	X			-40.4	12	2	MANHOLE, TYPE C-4	42	42		5.5	387.13	385.00	75	NA	7																					OT-7	Outfall

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE

DESIGNED: SRB DRAWN: TYW

CHECKED: CWB CHECKED: CWB

INDIANA
DEPARTMENT OF TRANSPORTATION

STRUCTURE DATA TABLE

HORIZONTAL SCALE	BRIDGE FILE
N/A	N/A
VERTICAL SCALE	DESIGNATION
N/A	2000217
SURVEY BOOK	SHEETS
N/A	109 of 127
CONTRACT	PROJECT
R-42287	1900308

pw://VANVA01PWINT01.Parsons.com:Indiana State/Documents/SR66 Lloyd Expwy Corridor/CADD/Roadway/Sheets/Des. 2000217/STOCKWELL_Lloyd_RD_Sht_Structure Data Table02
$DATE$

PARSONS

Appendix C

Early Coordination

INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Eric Holcomb, Governor
Michael Smith, Commissioner

March 2, 2022

Sample early coordination letter

«First_Name» «Last_Name»
«Organization»
«Department»
«Street_Address»
«City_State_Zip»

Re: Early Coordination Letter, Des. Nos.: **1900268** and **2000217**, **Lloyd Expressway Intersections Improvement Project** at **Vann Avenue** 1.8 miles east of US 41 and 3.2 miles west of Interstate 69 (I-69) and **Stockwell Road** 2.3 miles east of US 41 and 2.7 miles west of I-69, Vanderburgh County, Indiana

Dear Stakeholder,

The Indiana Department of Transportation (INDOT), with federal funding intends to proceed with an intersections improvement project involving a 0.9-mile section of State Road (SR) 66/Lloyd Expressway in the City of Evansville, in Vanderburgh County (Attachments: Page 1). This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. **Please use the above designation number and description in your reply.** We will incorporate your comments into a study of the project's environmental impacts.

Existing Conditions: The proposed undertaking is a part of "TheLloyd4U" initiative https://thelloyd4u.com/, which includes several improvement projects along the Lloyd Expressway. This project involves the intersections of Vann Avenue and Stockwell Road, and is located along Lloyd Expressway from Villa Drive to Congress Avenue. The project also includes a portion of the following roads and intersections: Vann Avenue, Stockwell Road, and Division Street. Based on information from the Metropolitan Evansville Transit System, there are no fixed transit routes that currently operate within the study area. The project setting is urban. Surrounding properties are a mixture of residential, commercial, and institutional properties. Additionally, the City of Evansville State Hospital Grounds Park is adjacent to the project area at the southeast corner of Lloyd Expressway and Vann Avenue, and the University of Evansville owns athletics fields adjacent to the northwest of Division Street and Stockwell Road.

This section of Lloyd Expressway is a divided highway that has three 12-foot wide travel lanes in each direction, with variable auxiliary and turn lanes at the signalized intersections, and variable paved shoulders that average 8-foot (inside) and 4-foot (outside) wide. Existing overhead lighting is present throughout the project corridor. Stormwater is currently handled by a mixture of curb and gutter, subgrade sewer lines, and roadside ditches, as well as a detention basin at the southeast corner of Lloyd Expressway and Stockwell Road.

The Lloyd Expressway and Vann Avenue intersection is signalized. There are dedicated left-turn and right-turn lanes onto Vann Avenue in both the eastbound (EB) and westbound (WB) directions. Vann Avenue has five lanes at the intersection, consisting of northbound (NB) and southbound (SB) through, left-turn, and right-turn lanes, with discontinuous sidewalk, curb and gutter.

The Lloyd Expressway and Stockwell Road intersection is also signalized. Lloyd Expressway has dedicated right-turn slip lanes onto Stockwell Road in both the EB and WB directions, as well as left-turn lanes in each direction (two NB and one SB). Stockwell Road has six lanes at the intersection, consisting of two through lanes in each direction, two left-turn lanes, and a right-turn lane, with curb and gutter. There are no pedestrian facilities at the Stockwell Road intersection, including Division Street.

Existing pedestrian facilities include sidewalks along each side of Vann Avenue south of Lloyd Expressway, a 10-foot wide asphalt path and overpass east of the intersection, and trails at the southeast abutting State Hospital Grounds Park. The sidewalks have a crosswalk with curb ramps on the south side of the intersection. The overpass is a pedestrian bridge over Lloyd Expressway, located approximately 365 feet east of Vann Avenue. This facility includes 320-foot long retaining walls within Lloyd Expressway right-of-way (ROW) on both the north and south sides. The path connects to the southeastern Vann Avenue sidewalk, a curb ramp at the southeast corner of Division Street and Vann Avenue, and park trails.

Purpose and Need: The need for this project stems from a high rate of crashes at both intersections, and congestion issues at Stockwell Road. Safety is evaluated using the Road Hazard Analysis Tool (RoadHAT) software. RoadHAT provides results as an Index of Crash Frequency (ICF) and Index of Crash Cost (ICC), which illustrate how the facility is performing. Per the *Indiana Design Manual*, an ICF and ICC of zero or less represents average or below-average crash frequency. Per the INDOT Roadway Application for the Lloyd Expressway/Vann Avenue intersection, for the years 2015 to 2017, the ICF and ICC were 3.18 and 4.87, respectively. Per the INDOT Roadway Application for the Lloyd Expressway /Stockwell Road intersection, for the years 2014 to 2016 the ICF and ICC were 2.11 and 3.42, respectively.

Traffic capacity is evaluated in terms of Levels of Service (LOS). LOS is a performance measure that represents quality of service, measured on an A – F scale, with LOS A representing a free flow of traffic and LOS F representing a breakdown in flow (e.g., start-and-stop congestion). The project area is within an urban area, therefore the minimum criteria during peak travel hours (i.e., rush hour) is LOS D. Per the 2020 INDOT Roadway Project Application, at the Lloyd Expressway and Stockwell Road intersection the following movements are "currently failing (LOS F)" during the PM peak: EB through, EB left, WB through, WB left, SB left, and NB left.

The purpose of this intersection improvement project is to reduce the rate of crashes at both intersections and improve the LOS at Lloyd Expressway and Stockwell Road to a minimum of LOS D in the design year, 2045.

Proposed Project: The proposed project would reconfigure both intersections to remove left turns. The preliminary recommended alternative would convert the intersection of Lloyd Expressway and Vann Avenue to a right-in/right-out (RIRO) intersection. The recommended alternative for the intersection improvement at Stockwell Road would convert the traditional signalized intersection to a hybrid Displaced Left-Turn (DLT) intersection that includes both a displaced left-turn and a boulevard left-turn (Attachments: Page 3). The proposed work would also realign and reconstruct Division Street, including pavement removal and full depth pavement construction.

The recommended alternative for the intersection improvement work at Lloyd Expressway and Vann Avenue would remove the existing signals, close the median along Lloyd Expressway with permanent concrete barrier walls, eliminate the left turn lanes along Lloyd Expressway and Vann Avenue with restriping of pavement markings, and construct new concrete splitter islands at the Vann Avenue approaches. The existing curb lines at all four quadrants of the intersection would be maintained with proposed concrete splitter islands.

The existing sidewalk and curb ramps along the south approach of Vann Avenue would remain in-place and undisturbed, as well as the curb ramp at the southeast corner of Vann Avenue and Division Street. A pedestrian refuge is proposed for the southern splitter island. The legacy northeast and southeast curb ramps and northeast sidewalk would be removed, because the existing pedestrian overpass is now utilized for this movement. No impacts to the adjoining park, trails, and pedestrian overpass are expected. The existing sidewalk on both sides of Vann Avenue would remain in place along with the curb ramps associated with the east-west pedestrian movement across Vann Avenue. Pedestrian accommodations are being coordinated with the City of Evansville and INDOT.

The recommended alternative for the intersection improvement work at Lloyd Expressway and Stockwell Road would include a crossover in advance of the intersection in the EB direction to displace the left-turn lanes along Lloyd Expressway to be on the opposite side of the through traffic, a bypass right-turn lane for movements from SB Stockwell Road to WB Lloyd Expressway, two proposed signals at the crossover to control the left-turn movements and the bypass right-turn lane, a boulevard left-turn in the WB direction, one proposed signal and a bump-out for turning movements (also known as a "truck loon") at the boulevard left-turn, modification of the existing signals to accommodate updated traffic movements, and proposed concrete splitter islands to separate opposing directions of traffic. Partial pavement replacement would be done as needed in order to construct the proposed concrete splitter islands and the right slip lane in addition to pavement replacement where the existing concrete median barrier would be demolished. The WB Lloyd Expressway left-turn onto SB Stockwell Road would be eliminated and replaced with the proposed boulevard left-turn west of the intersection. The entrance and exit to the private drive for the Boy Scouts of America and American Red Cross properties would be widened, and full depth replacement of Stockwell Road pavement within the project limits is proposed. Existing drives to the athletic fields owned by University of Evansville would be maintained.

There are no existing pedestrian facilities located within the project limits at Stockwell Road, therefore no pedestrian facilities are proposed.

Existing guardrail would be upgraded. In addition to the proposed added signals and changes to signal heads, existing streetlights would be moved and/or upgraded. Likewise, improvements to the existing storm water system would include piping of existing roadside ditches/tributaries where pavement widening is proposed.

This project would mostly occur within existing, previously disturbed right-of-way (ROW). Up to 1.25 acres of temporary and/or permanent ROW, consisting of strips from commercial properties, may be needed. The proposed maintenance of traffic (MOT) includes phased construction that would allow at least two lanes of EB and WB traffic along Lloyd Expressway to remain open at all times. Detours may be needed for portions of Vann Avenue and Stockwell Road, as well as other local roads. Access to all properties would be maintained. Work is expected to start in the spring of 2024 and to last two to three years.

The project is in an urban area. The USGS 7.5-minute quadrangle topographical map does not depict water resources within the project area (Attachments: Page 2). A water investigation would be conducted to determine the presence of jurisdictional streams and wetlands, and all applicable permits would be prepared.

This project is within the range of the federally endangered Indiana bat (*Myotis sodalis*) and federally threatened northern long-eared bat (*Myotis septentrionalis*). The Indiana Bat and Northern Long-eared Bat Range-Wide Standard Informal Programmatic Consultation is anticipated to be applied to this project. Project information was uploaded to the United States Fish and Wildlife Service's (USFWS) Information for Planning and Consultation (IPaC) website to identify if any species listed or proposed to be listed may be present in the area of the proposed action. An Official Species List was generated, and there were no other species listed in addition to the aforementioned bats. Tree clearing is anticipated to be less than 0.5 acre.

Coordination is occurring with INDOT's Cultural Resources Office (CRO) to evaluate the project area for archaeological and historic resources and for Section 106 compliance.

Please provide your response within **thirty (30) calendar days from the date of this letter.** However, should you find that an extension to the response time is necessary, a reasonable amount may be granted upon request. If you have any questions regarding this matter, please feel free to contact me at (317) 616-1000 or via email at angela.mamukuyomi@parsons.com, or the INDOT Project Manager, Brian Malone, at (812) 836-2112 or via email at bmalone@indot.in.gov. Thank you in advance for your input.

Sincerely,

Angela Mamukuyomi
Parsons

Attachments –
Maps (Project Location, USGS Topographic, Proposed Conditions Handout)
Project Area Photographs

Attachments intentionally omitted, refer to Appendix B.

Lloyd4U website: https://thelloyd4u.com

The following agencies received Early Coordination Letters:

Sent on March 2, 2022 unless otherwise noted.

Federal Highway Administration
Federal Office Building
575 N Pennsylvania St., Rm. 254
Indianapolis, IN 46204

Field Environmental Officer
Chicago Regional Office
US Department of Housing & Urban Development
Metcalf Fed. Bldg.
77 W Jackson Blvd. Rm. 2401
Chicago, IL 60604

Regional Environmental Coordinator
Midwest Regional Office
National Park Service
601 Riverfront Dr.
Omaha, Nebraska 68102

Indiana Department of Natural Resources
Division of Fish and Wildlife
Rm. W264, IGC South
402 W Washington St.
Indianapolis, IN 46204

Indiana Department of Environmental Management
100 N Senate Ave.
Indianapolis, IN 46204

IDEM electronic coordination omitted per INDOT guidance.

Indiana Geological and Water Survey
611 N Walnut Grove
Bloomington, IN 47405
(Electronic Coordination)

Evansville Fire Department Administration
Fire Chief
550 SE 8th St.
Evansville, IN 47713

Evansville Police Department
Police Chief
15 NW Martin Luther King Blvd.
Evansville, IN 47708

City of Evansville
Mayor
Civic Center Complex, Rm. 302
1 NW Martin Luther King Blvd.
Evansville, IN 47708

Vanderburgh County
Surveyor
Civic Center Complex, Rm. 325
1 NW Martin Luther King Blvd.
Evansville, IN 47708

Indiana Department of Transportation
Vincennes District Office
3650 S US Hwy. 41
Vincennes, IN 47591

Evansville Metropolitan Planning Organization (MPO)
Executive Director
Civic Center Complex, Rm. 316
1 NW Martin Luther King Blvd.
Evansville, IN 47708

Indiana Department of Transportation
Office of Aviation
100 N Senate Ave., Rm. 955
Indianapolis, IN 46204

Indiana Department of Transportation
Utilities and Rail Office ICGN 758- UT/RR
100 N Senate Ave.,
Indianapolis, IN 46204

Metropolitan Evansville Transit System
Director
601 John St.
Evansville, IN 47713

Vanderburgh County
County Commission President
Civic Center Complex, Rm. 305
1 NW Martin Luther King Blvd.
Evansville, IN 47708

Vanderburgh County
County Council President
Civic Center Complex, Rm. 303
1 NW Martin Luther King Blvd.
Evansville, IN 47708

Vanderburgh County
County Council Personnel Chair
Civic Center Complex, Rm. 303
1 NW Martin Luther King Blvd.
Evansville, IN 47708

Evansville Vanderburgh School Corporation
Superintendent
951 Walnut St.
Evansville, IN 47713

Evansville Vanderburgh School Corporation
Bus Transportation
951 Walnut St.
Evansville, IN 47713

Vanderburgh County
Highway Superintendent
5105 N Saint Joseph Ave.
Evansville, IN 47720

Vanderburgh County
Building Commissioner, Local Floodplain Administrator
Civic Center Complex, Rm. 310
1 NW Martin Luther King Blvd.
Evansville, IN 47708

City of Evansville
City Engineer, Storm Water Coordinator/MS4
Civic Center Complex, Rm. 321
1 NW Martin Luther King Blvd.
Evansville, IN 47708

City of Evansville
City Engineer
Civic Center Complex, Rm. 321
1 NW Martin Luther King Blvd.
Evansville, IN 47708

City of Evansville
Parks and Recreation
1 NW Martin Luther King Blvd.
Evansville, IN 47708

City of Evansville
Transportation Executive Director
Civic Center Complex, Rm. 321
1 NW Martin Luther King Blvd.
Evansville, IN 47708

United States Army Corps of Engineers (USACE)
Louisville District Indianapolis Regulatory Office
Indianapolis, IN 46216

City of Evansville
City Councilor, Ward 1
639 Plaza Drive
Evansville, IN 47714

City of Evansville
City Councilor, Ward 3
521 S Villa Dr.
Evansville, IN 47714

Evansville Convention and Visitors Bureau Commission
Board President
401 SE Riverside Drive
Evansville, IN 47714

Evansville State Hospital
Hospital Administrator
3400 Lincoln Avenue
Evansville, IN 47714

Harper Elementary School
Superintendent
21 South Alvord Boulevard
Evansville, IN 47714

Harrison High School
Superintendent
211 Fielding Road
Evansville, IN 47715

University of Evansville
Administrator
1800 Lincoln Avenue
Evansville, IN 47722

Vanderburgh County Health Department
Administrator
420 Mulberry Street
Evansville, IN 47713

Ascension St. Vincent Evansville
Administrator
3700 Washington Avenue
Evansville, IN 47714

Deaconess Gateway Hospital
Administrator
4011 Gateway Boulevard
Newburgh, IN 47630

Catholic Diocese of Evansville
Superintendent
P.O. Box 4169
Evansville, IN 47724-0169

THIS IS NOT A PERMIT

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-24533 **Request Received:** March 2, 2022

Requestor:
Parsons
Angela Mamukuyomi
101 West Ohio Street, Suite 2121
Indianapolis, IN 46204

Project: Lloyd Expressway intersection improvements at Vann Avenue, Stockwell Road, Burkhard Road, and Cross Pointe Boulevard, Evansville; Des #1900268, 2000219, 1900292 & 1900317

County/Site info: Vanderburgh

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: This proposal may require the formal approval of our agency pursuant to the Flood Control Act (IC 14-28-1) for any proposal to construct, excavate, or fill in or on the floodway of a stream or other flowing waterbody which has a drainage area greater than one square mile, unless it qualifies for a bridge exemption (see enclosure). Please include a copy of this letter with the permit application, if required.

Natural Heritage Database: The Natural Heritage Program's data have been checked.
To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the vicinity of the Burkhardt and Cross Pointe intersections. However, the managed lands, community, and species below have been documented within 1/2 mile of the Vann and Stockwell intersections. The Division of Nature Preserves does not anticipate any impacts to the community or plants as a result of this project.
A) MANAGED LANDS (Evansville Parks & Rec):
1. State Hospital Grounds Park
2. Wesselman Park
3. Wesselman Park Woods Nature Preserve
B) NATURAL COMMUNITY: Wet-mesic Floodplain Forest
C) PLANTS:
1. Land Of Gold Sedge (Carex aureolensis); state endangered
2. Blue Scorpionweed (Phacelia ranunculacea); state endangered
3. Social Sedge (Carex socialis); state threatened

Fish & Wildlife Comments: The measures below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:
1. Revegetate all bare and disturbed areas with a mixture of grasses (excluding all varieties of tall fescue) and legumes as soon as possible upon completion; low endophyte tall fescue may be used in the ditch bottom and side slopes only.
2. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
3. Seed and protect all disturbed streambanks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty,

biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.

Contact Staff: Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.

JoAnne D. Cummings **Date:** March 31, 2022

for Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife

Linda Freeman
VANDERBURGH COUNTY SURVEYOR

Room 325 Civic Center Complex
1 NW Martin Luther King Jr Blvd
Evansville, IN 47708-1880
Phone (812) 435-5210
Fax (812) 435-5023

Ms. Angela Mamukuyomi
Parsons Corporation

March 7, 2022

Regarding: Early Coordination Letter
Des. No. 1900268 & 2000217
Lloyd Expressway Intersections Improvement Project
Evansville, Vanderburgh County, Indiana

Dear Ms. Mamukuyomi,

The Vanderburgh County Surveyor has reviewed the Early Coordination Letter, dated March 2, 2022, regarding the proposed Lloyd Expressway Intersections Improvement Project. The project extents appear to include multiple sections corners that have been perpetuated by the office. From the Surveyor's Office inventory list of section corners, these points include Point 2856 located in 22-6-10, Point 2173 in 27-6-10, and Point 1838 in Section 26-6-10. I have included with this letter the witness drawings produced by our office that detail the locations of and data associated with these points.

If any of these points were disturbed as a result of the Intersections Improvement Project, our office would request that they be replaced by a Licensed Surveyor and updated coordinate data be provided to the Surveyor's Office.

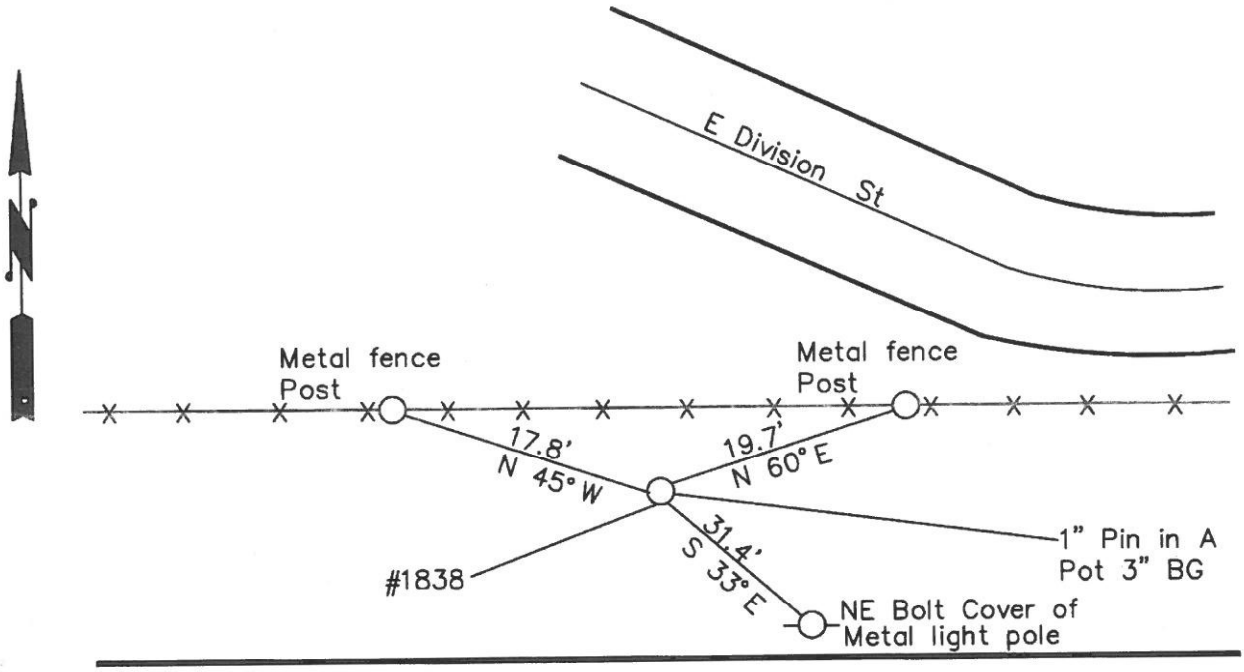
If you have any additional questions or comments regarding this, please do contact the office through the information listed above.

Respectfully yours,

Linda Freeman
Vanderburgh County Surveyor

Included: Point 1838 Reference Drawing
Point 2173 Reference Drawing
Point 2856 Reference Drawing

Vanderburgh County, Indiana
Section Corner References



E LLoyd EXP

Referenced: 8–16–2013
Surveyed: 8–20–2003

Survey Control

Northing 994140.352
Easting 2832172.552
Elev 385.938

Indiana West Zone Coordinates
NAD 1983

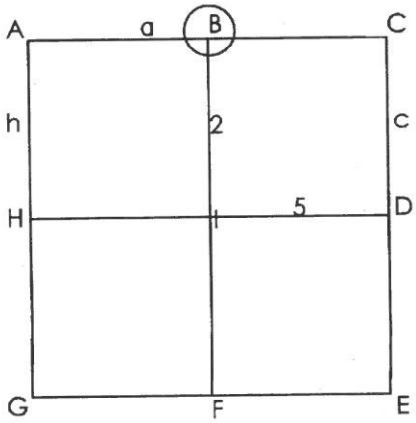
Notes:

GPS static surveyor network utilizing Harn PT B326 and Harn PT R356 @ 95% standard error

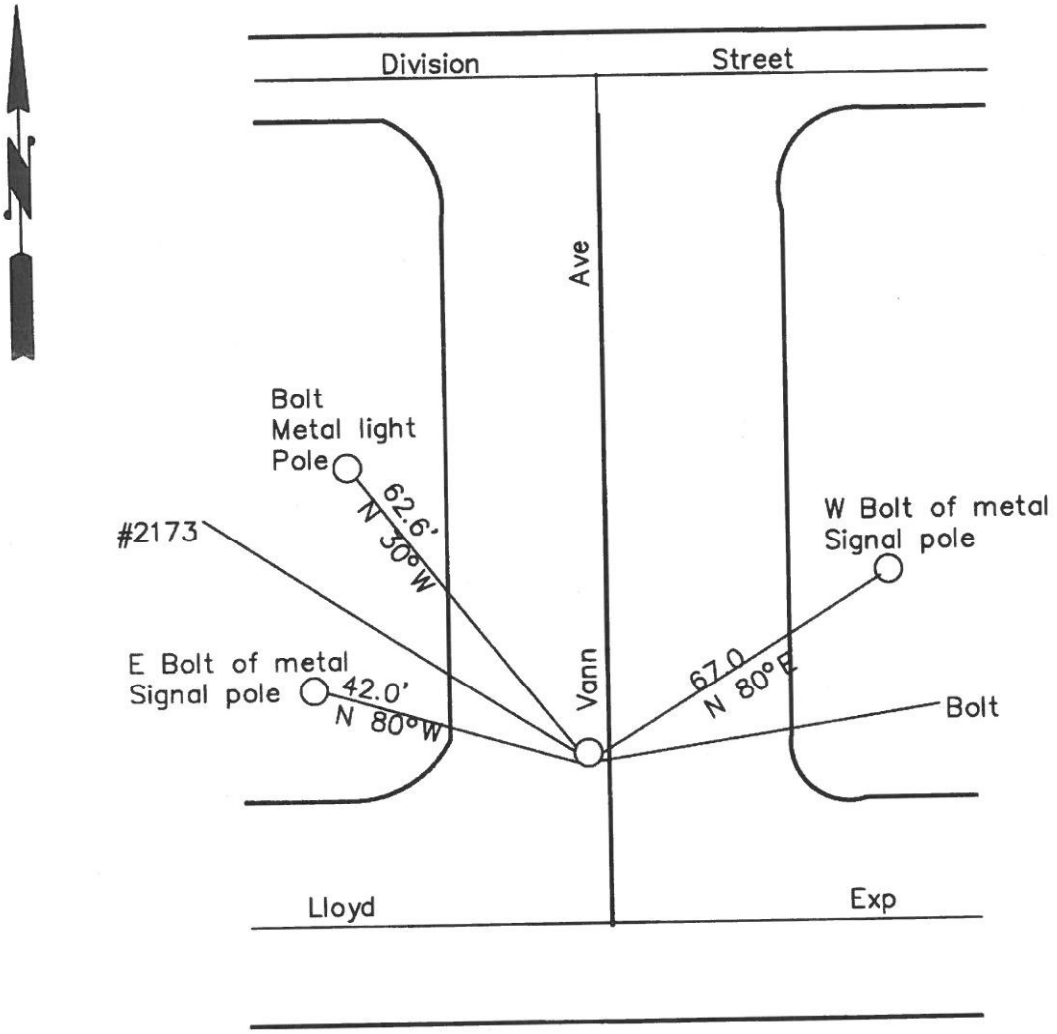
X=0.031'; Y=0.021'; Z=0.043'

Section Diagram

26–T6S–R10W



Vanderburgh County, Indiana
Section Corner References



Referenced: 5–8–2009
Surveyed: 5–8–2009

Survey Control

Northing 994181.744
Easting 2829509.684
Elev 387.57

Indiana West Zone Coordinates
NAD 1983

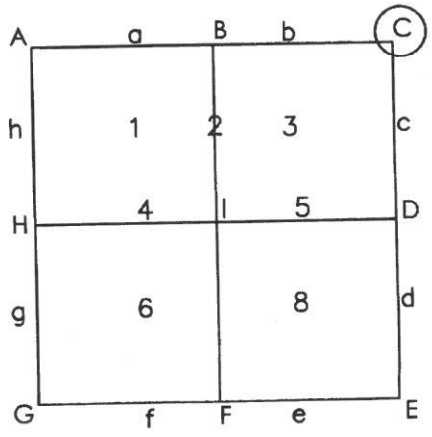
Notes:

GPS static survey network utilizing Harn PT R356 and Harn PT N326 @ 95% standard error

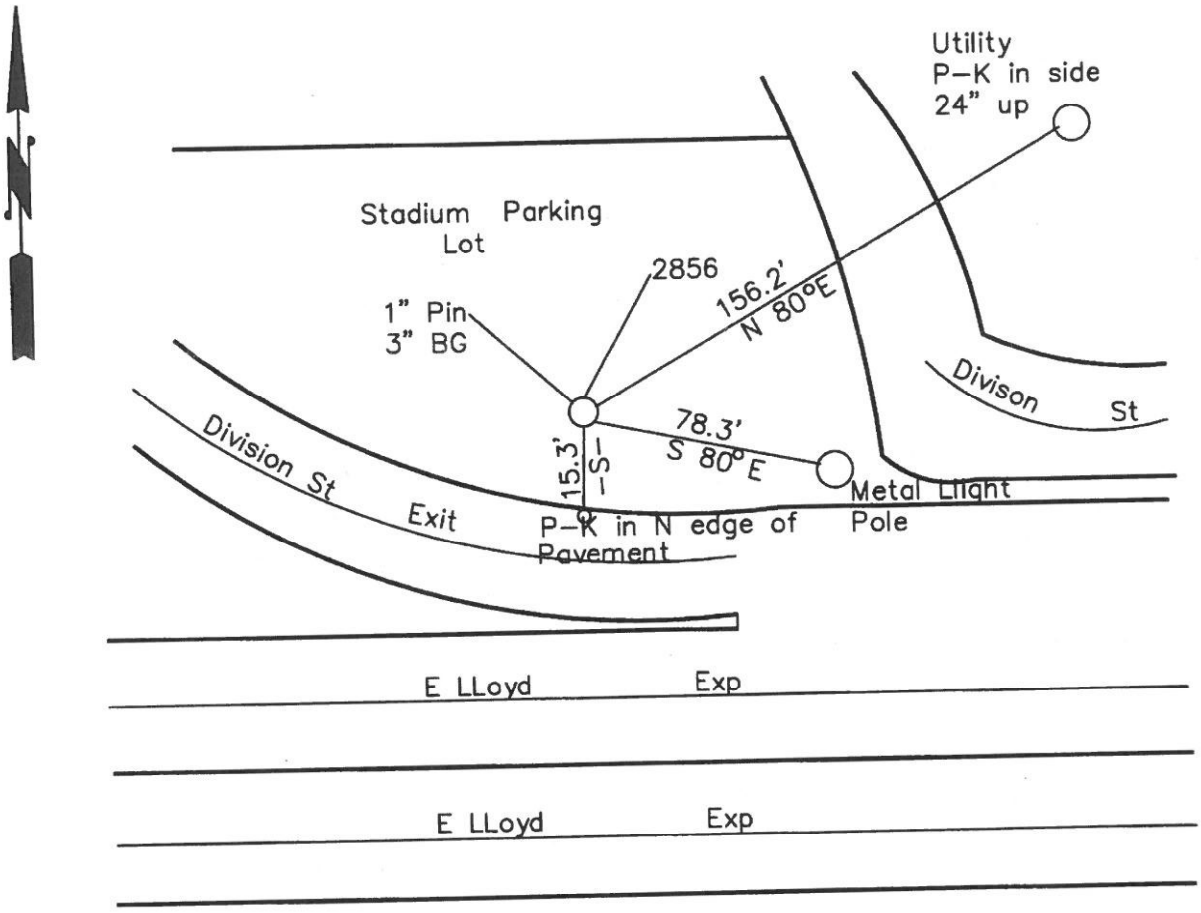
X=0.066'; Y=0.06'; Z=0.078'

Section Diagram

27–6–10



Vanderburgh County, Indiana
Section Corner References



Referenced: 5–12–2009
Surveyed: 5–12–2009

Survey Control

Northing 994219.983
Easting 2828153.416
Elev 378.819

Indiana West Zone Coordinates
NAD 1983

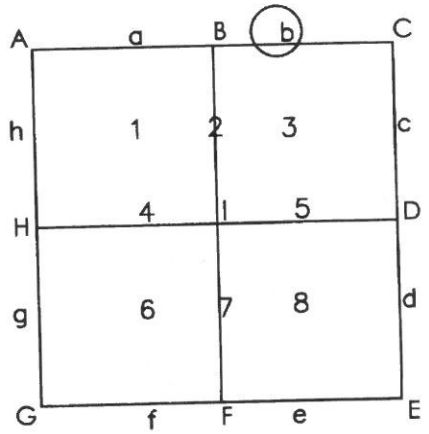
Notes:

GPS static survey network utilizing Harn PT R356 and Harn PT B326 @ 95% standard error

X=0.008 Y=0.005'; z=0.012'

Section Diagram

27–6–10



From: Alexis Berggren
To: Mamukuyomi, Angela [US-US]
Cc: Julia Pillow
Subject: [EXTERNAL] Des. Nos.: 1900268 and 2000217
Date: Friday, March 25, 2022 4:37:51 PM

Hello Angela,

We are in receipt of your Early Coordination Letter for Des. Nos: 1900268 and 2000217. Unfortunately, I am new to my position, and due to the transition in staff we did not process your mailing until this past week. I am sharing with our board members, but I am requesting an additional 10 business days to offer feedback, so that they may have the time to review and respond if necessary. May I respectfully ask that we be able to submit responses until April 15, 2022?

Thank you so much for your consideration,

Alexis

Visit Evansville INDIANA

Alexis Berggren (she/her)
President & CEO
20 NW 3rd St, Suite 410
Evansville, IN 47708
O: 812-421-2205
C: 812-893-8232

From: Alexis Berggren
To: Diefenbaugh, Cedric [NN-US]
Subject: [EXTERNAL] RE: Lloyd4U Early Coordination Letters (Des. Nos.: 1900268/2000217 and 1900292/1900317)
Date: Tuesday, April 19, 2022 12:31:40 PM
Attachments: image001.png

Hi Cedric! Thanks for the follow up – I think we are actually ok. Appreciate the response.

Alexis

Alexis Berggren
President & CEO
Visit Evansville

From: Cedric.Diefenbaugh@parsons.com <Cedric.Diefenbaugh@parsons.com>
Sent: Tuesday, April 19, 2022 9:35 AM
To: Alexis Berggren <aberggren@visitevansville.com>
Subject: Lloyd4U Early Coordination Letters (Des. Nos.: 1900268/2000217 and 1900292/1900317)

Good morning Alexis Berggren,

I apologize for not getting back to you sooner. We have also been in the process of transitioning positions for our early coordination responses. I wanted to follow up with you on whether Visit Evansville had any additional responses from our Lloyd4U Early Coordination Letters sent on March 2, 2022? Please let me know if you have any questions or concerns.

Thank you,

Cedric Diefenbaugh
Environmental Planner
101 West Ohio Street, Suite 2121 - Indianapolis, IN 46204
cedric.diefenbaugh@parsons.com
Mobile: 260.578.2797
PARSONS – Envision More
www.parsons.com | LinkedIn | Twitter | Facebook

Port, Juliet [US-US]

From: Courtade, Julian <JCourtade@indot.IN.gov>
Sent: Tuesday, March 8, 2022 9:23 AM
To: Mamukuyomi, Angela [US-US]
Subject: [EXTERNAL] RE: Des. Nos. 1900268 and 2000217 Lloyd Expressway Early Coordination Letter

Angela –

I reviewed the Early Coordination Letter and found no issues with any surrounding airspace or public-use airports. This is due to the project meeting the required glideslope criteria from the nearest public-use facility according to 14 CFR Part 77 – Safe, efficient use, and preservation of the navigable airspace.

If any object will exceed 200 ft in height regardless of location, the object will need to be airspaced with the FAA 45 days prior to construction through the OEAAA portal below.

https://oeaaa.faa.gov/oeaaa/external/searchAction.jsp [oeaaa.faa.gov]

Please let me know if you have any questions!

Thanks,

Julian L. Courtade
Chief Airport Inspector
100 North Senate Ave, N758-MM
Indianapolis, IN 46204
Cell: (317) 954-7385
Email: jcourtade@indot.in.gov

[facebook.com] [twitter.com] [youtube.com] [in.gov] Indiana [in.gov]

[in.gov]

From: Angela.Mamukuyomi@parsons.com <Angela.Mamukuyomi@parsons.com>
Sent: Thursday, March 3, 2022 8:48 AM
To: Courtade, Julian <JCourtade@indot.IN.gov>
Subject: Des. Nos. 1900268 and 2000217 Lloyd Expressway Early Coordination Letter

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Good morning,

The Early Coordination Letter attached is being sent to you on behalf of the Indiana Department of Transportation.

INDIANA
GEOLOGICAL SURVEY

Organization and Project Information

Project ID:	
Des. ID:	1900268 and 2000217
Project Title:	Lloyd Expressway Intersections Improvement Project at Vann Avenue and Stockwell Road
Name of Organization:	Parsons
Requested by:	Cedric Diefenbaugh

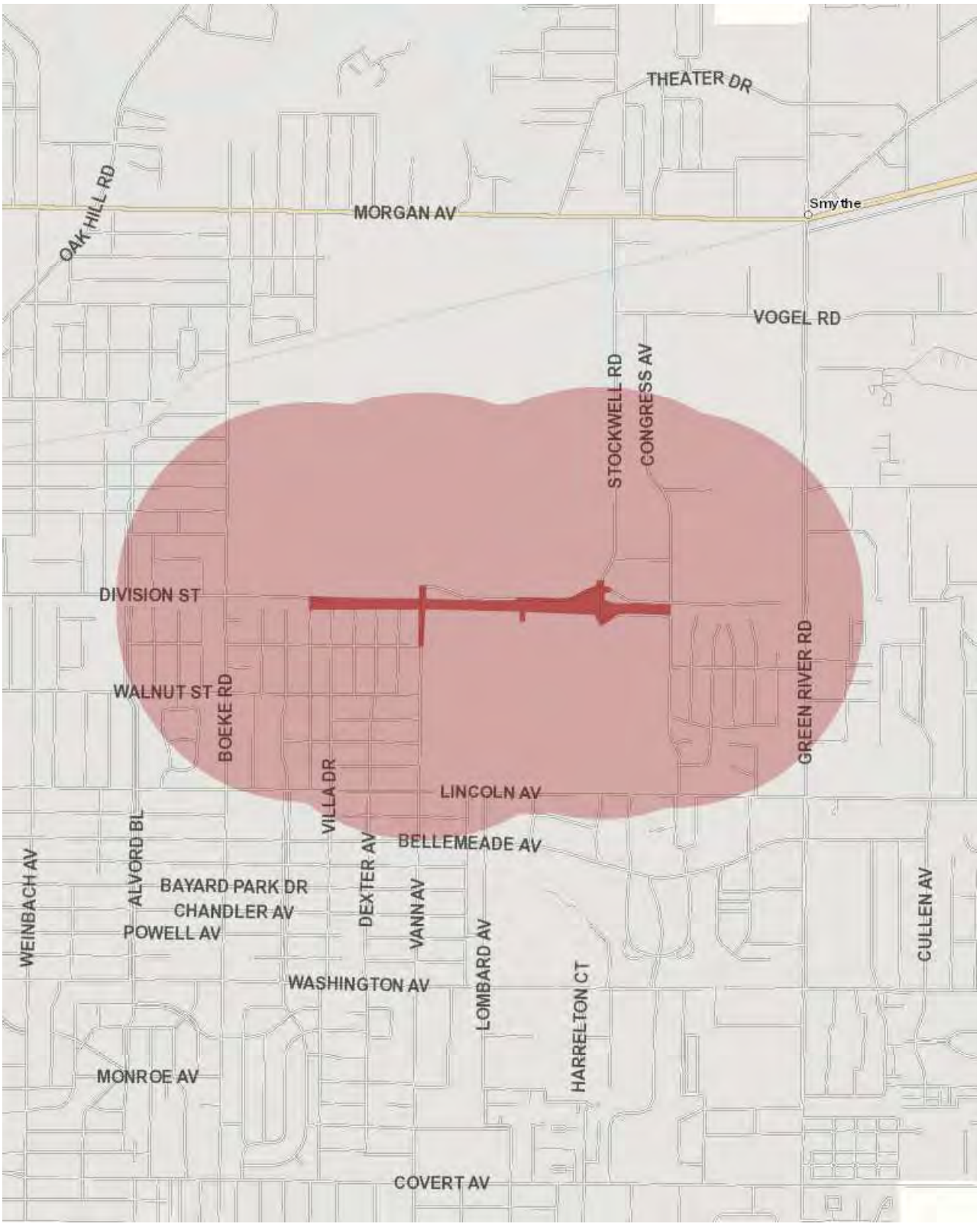
Environmental Assessment Report

1. Geological Hazards:
   - High liquefaction potential
   - 1% Annual Chance Flood Hazard
2. Mineral Resources:
   - Bedrock Resource: High Potential
   - Sand and Gravel Resource: High Potential
3. Active or abandoned mineral resources extraction sites:
   - None documented in the area

*All map layers from Indiana Map (maps.indiana.edu)

This information was furnished by Indiana Geological Survey
Address: 420 N. Walnut St., Bloomington, IN 47404
Email: IGSEnvir@indiana.edu
Phone: 812 855-7428

Date: April 21, 2022



Metadata:

- https://maps.indiana.edu/metadata/Geology/Seismic_Earthquake_Liquefaction_Potential.html
- https://maps.indiana.edu/metadata/Geology/Industrial_Minerals_Sand_Gravel_Resources.html
- https://maps.indiana.edu/metadata/Hydrology/Floodplains_FIRM.html
- https://maps.indiana.edu/metadata/Geology/Bedrock_Geology.html

United States Department of the Interior

FISH AND WILDLIFE SERVICE
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273
http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html

In Reply Refer To: April 21, 2022
Project Code: 2022-0005463
Project Name: Des. Nos. 1900268 & 2000217 (Lead Des 1900308) Lloyd Expressway (SR 66) Corridor Improvement Project

Subject: List of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The enclosed species list identifies threatened, endangered, proposed and candidate species, as well as proposed and final designated critical habitat, that may occur within the boundary of your proposed project and/or may be affected by your proposed project. The species list fulfills the requirements of the U.S. Fish and Wildlife Service (Service) under section 7(c) of the Endangered Species Act (Act) of 1973, as amended (16 U.S.C. 1531 *et seq.*).

New information based on updated surveys, changes in the abundance and distribution of species, changed habitat conditions, or other factors could change this list. Please feel free to contact us if you need more current information or assistance regarding the potential impacts to federally proposed, listed, and candidate species and federally designated and proposed critical habitat. Please note that under 50 CFR 402.12(e) of the regulations implementing section 7 of the Act, the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally as desired. The Service recommends that verification be completed by visiting the ECOS-IPaC website at regular intervals during project planning and implementation for updates to species lists and information. An updated list may be requested through the ECOS-IPaC system by completing the same process used to receive the enclosed list.

The purpose of the Act is to provide a means whereby threatened and endangered species and the ecosystems upon which they depend may be conserved. Under sections 7(a)(1) and 7(a)(2) of the Act and its implementing regulations (50 CFR 402 *et seq.*), Federal agencies are required to utilize their authorities to carry out programs for the conservation of threatened and endangered species and to determine whether projects may affect threatened and endangered species and/or designated critical habitat.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - http://www.fws.gov/midwest/endangered/section7/

s7process/index.html. This website contains step-by-step instructions which will help you determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process. For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

A Biological Assessment is required for construction projects (or other undertakings having similar physical impacts) that are major Federal actions significantly affecting the quality of the human environment as defined in the National Environmental Policy Act (42 U.S.C. 4332(2)(c)). For projects other than major construction activities, the Service suggests that a biological evaluation similar to a Biological Assessment be prepared to determine whether the project may affect listed or proposed species and/or designated or proposed critical habitat. Recommended contents of a Biological Assessment are described at 50 CFR 402.12.

If a Federal agency determines, based on the Biological Assessment or biological evaluation, that listed species and/or designated critical habitat may be affected by the proposed project, the agency is required to consult with the Service pursuant to 50 CFR 402. In addition, the Service recommends that candidate species, proposed species and proposed critical habitat be addressed within the consultation. More information on the regulations and procedures for section 7 consultation, including the role of permit or license applicants, can be found in the "Endangered Species Consultation Handbook" at:

http://www.fws.gov/endangered/esa-library/pdf/TOC-GLOS.PDF

Migratory Birds: In addition to responsibilities to protect threatened and endangered species under the Endangered Species Act (ESA), there are additional responsibilities under the Migratory Bird Treaty Act (MBTA) and the Bald and Golden Eagle Protection Act (BGEPA) to protect native birds from project-related impacts. Any activity, intentional or unintentional, resulting in take of migratory birds, including eagles, is prohibited unless otherwise permitted by the U.S. Fish and Wildlife Service (50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)). For more information regarding these Acts see https://www.fws.gov/birds/policies-and-regulations.php.

The MBTA has no provision for allowing take of migratory birds that may be unintentionally killed or injured by otherwise lawful activities. It is the responsibility of the project proponent to comply with these Acts by identifying potential impacts to migratory birds and eagles within applicable NEPA documents (when there is a federal nexus) or a Bird/Eagle Conservation Plan (when there is no federal nexus). Proponents should implement conservation measures to avoid or minimize the production of project-related stressors or minimize the exposure of birds and their resources to the project-related stressors. For more information on avian stressors and recommended conservation measures see https://www.fws.gov/birds/bird-enthusiasts/threats-to-birds.php.

In addition to MBTA and BGEPA, Executive Order 13186: *Responsibilities of Federal Agencies to Protect Migratory Birds*, obligates all Federal agencies that engage in or authorize activities that might affect migratory birds, to minimize those effects and encourage conservation measures that will improve bird populations. Executive Order 13186 provides for the protection of both migratory birds and migratory bird habitat. For information regarding the implementation of

Executive Order 13186, please visit https://www.fws.gov/birds/policies-and-regulations/executive-orders/e0-13186.php.

We appreciate your concern for threatened and endangered species. The Service encourages Federal agencies to include conservation of threatened and endangered species into their project planning to further the purposes of the Act. **Please include the Consultation Code in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.**

Attachment(s):

- Official Species List
- Migratory Birds
- Wetlands

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
(812) 334-4261

Project Summary

Project Code: 2022-0005463
Event Code: None
Project Name: Des. Nos. 1900268 & 2000217 (Lead Des 1900308) Lloyd Expressway (SR 66) Corridor Improvement Project
Project Type: Road/Hwy - Maintenance/Modification
Project Description: The Indiana Department of Transportation (INDOT) proposes a corridor improvement project along State Road (SR) 66/Lloyd Expressway (Lloyd Expy) in the City of Evansville, Vanderburgh County, Indiana. The SR 66 and Vann Ave intersection is located approximately 1.79 miles east of US 41, and the SR 66 and Stockwell Rd intersection is located approximately 2.25 miles east of US 41. This project involves the intersections of Vann Ave and Stockwell Rd, located along Lloyd Expy from Villa Dr to Congress Ave. The project also includes a portion of the following roads and intersections: Vann Ave, Stockwell Rd, and Division St. The project setting is urban. Surrounding properties are a mixture of residential, commercial, and institutional properties.

This section of Lloyd Expy is a divided highway that has three 12-foot wide travel lanes in each direction, with variable auxiliary and turn lanes at the signalized intersections, and variable paved shoulders that average 8 feet (inside) and 4 feet (outside) wide. Existing overhead lighting is present throughout the project corridor.

The preliminary recommended alternative would convert the intersection of Lloyd Expy and Vann Ave to a right-in/right-out (RIRO) intersection. The proposed work would remove the existing signals, close the median along Lloyd Expy with permanent concrete barrier walls, eliminate the left turn lanes along Lloyd Expy and Vann Ave, and construct new concrete splitter islands at the Vann Ave approaches. Stockwell Rd would be converted to a hybrid Displaced Left-Turn (DLT) intersection that includes both a displaced left-turn and a blvd left-turn. Private drives would be widened, and full depth replacement of Stockwell Rd pavement within the project limits is proposed. The work would also realign and reconstruct Division St. Existing guardrail would be upgraded. In addition to the proposed added signals and changes to signal heads, existing streetlights would be moved and/or upgraded. Likewise, improvements to the existing storm water system may include piping of existing roadside ditches.

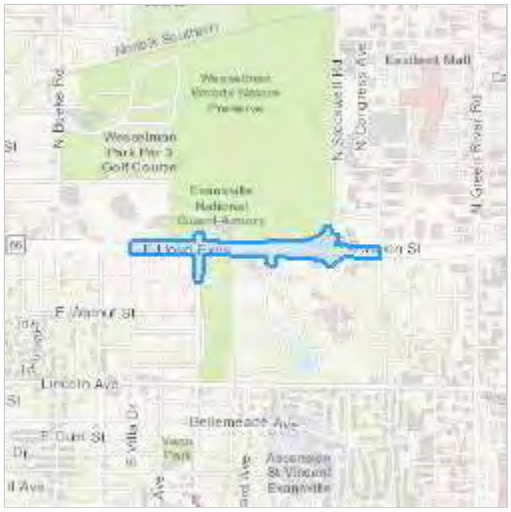
Up to 1.25 acres of temporary and/or permanent ROW, consisting of strips from commercial properties, may be needed. Most of the trees within the project action areas are urban street trees. There is no suitable summer habitat within the project area; however, there is some suitable

habitat within the project action area. Less than 0.5 acre of tree clearing/ trimming is anticipated. All tree clearing will occur within 100 feet of existing pavement. Trees within the project area are unsuitable, thus time of year restrictions do not apply. The primary tree species observed within the project area were crabapple (Malus sp.) and red mulberry (Morus rubra). Construction is anticipated to start in the spring of 2024 and is expected to last two to three years. The contractor will likely use temporary lighting during construction.

A review of the USFWS GIS database for Indiana bat and northern long-eared bat roosting, hibernacula, and capture sites was conducted for Des. Nos. 1900268 and 2000217 on December 3, 2021. There are no documented sites within a half mile of the project area. No structures that would need to be inspected exist within the project area.

Project Location:

Approximate location of the project can be viewed in Google Maps: https://www.google.com/maps/@37.97648955,-87.50642145407548,14z



Counties: Vanderburgh County, Indiana

Endangered Species Act Species

There is a total of 3 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries[1], as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

---

1. NOAA Fisheries, also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Mammals

NAME	STATUS
Indiana Bat *Myotis sodalis* There is **final** critical habitat for this species. The location of the critical habitat is not available. Species profile: https://ecos.fws.gov/ecp/species/5949	Endangered
Northern Long-eared Bat *Myotis septentrionalis* No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: ▪ Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See www.fws.gov/midwest/endangered/mammals/nleb/index.html Species profile: https://ecos.fws.gov/ecp/species/9045	Threatened

Insects

NAME	STATUS
Monarch Butterfly *Danaus plexippus* No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9743	Candidate

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

Migratory Birds

Certain birds are protected under the Migratory Bird Treaty Act[1] and the Bald and Golden Eagle Protection Act[2].

Any person or organization who plans or conducts activities that may result in impacts to migratory birds, eagles, and their habitats should follow appropriate regulations and consider implementing appropriate conservation measures, as described below.

---

1. The Migratory Birds Treaty Act of 1918.
2. The Bald and Golden Eagle Protection Act of 1940.
3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

The birds listed below are birds of particular concern either because they occur on the USFWS Birds of Conservation Concern (BCC) list or warrant special attention in your project location. To learn more about the levels of concern for birds on your list and how this list is generated, see the FAQ below. This is not a list of every bird you may find in this location, nor a guarantee that every bird on this list will be found in your project area. To see exact locations of where birders and the general public have sighted birds in and around your project area, visit the E-bird data mapping tool (Tip: enter your location, desired date range and a species on your list). For projects that occur off the Atlantic Coast, additional maps and models detailing the relative occurrence and abundance of bird species on your list are available. Links to additional information about Atlantic Coast birds, and other important information about your migratory bird list, including how to properly interpret and use your migratory bird report, can be found below.

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, click on the PROBABILITY OF PRESENCE SUMMARY at the top of your list to see when these birds are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
Bald Eagle *Haliaeetus leucocephalus* This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities.	Breeds Sep 1 to Jul 31
Black-billed Cuckoo *Coccyzus erythropthalmus* This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9399	Breeds May 15 to Oct 10

NAME	BREEDING SEASON
Cerulean Warbler *Dendroica cerulea* This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/2974	Breeds Apr 23 to Jul 20
Kentucky Warbler *Oporornis formosus* This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.	Breeds Apr 20 to Aug 20
Prairie Warbler *Dendroica discolor* This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.	Breeds May 1 to Jul 31
Prothonotary Warbler *Protonotaria citrea* This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.	Breeds Apr 1 to Jul 31
Red-headed Woodpecker *Melanerpes erythrocephalus* This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.	Breeds May 10 to Sep 10
Rusty Blackbird *Euphagus carolinus* This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA	Breeds elsewhere
Wood Thrush *Hylocichla mustelina* This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska.	Breeds May 10 to Aug 31

Probability Of Presence Summary

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read and understand the FAQ "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Each green bar represents the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during a particular week of the year. (A year is represented as 12 4-week months.) A taller bar indicates a higher probability of species presence. The survey effort (see below) can be used to establish a level of confidence in the presence score. One can have higher confidence in the presence score if the corresponding survey effort is also high.

How is the probability of presence score calculated? The calculation is done in three steps:

1. The probability of presence for each week is calculated as the number of survey events in the week where the species was detected divided by the total number of survey events for that week. For example, if in week 12 there were 20 survey events and the Spotted Towhee

was found in 5 of them, the probability of presence of the Spotted Towhee in week 12 is 0.25.

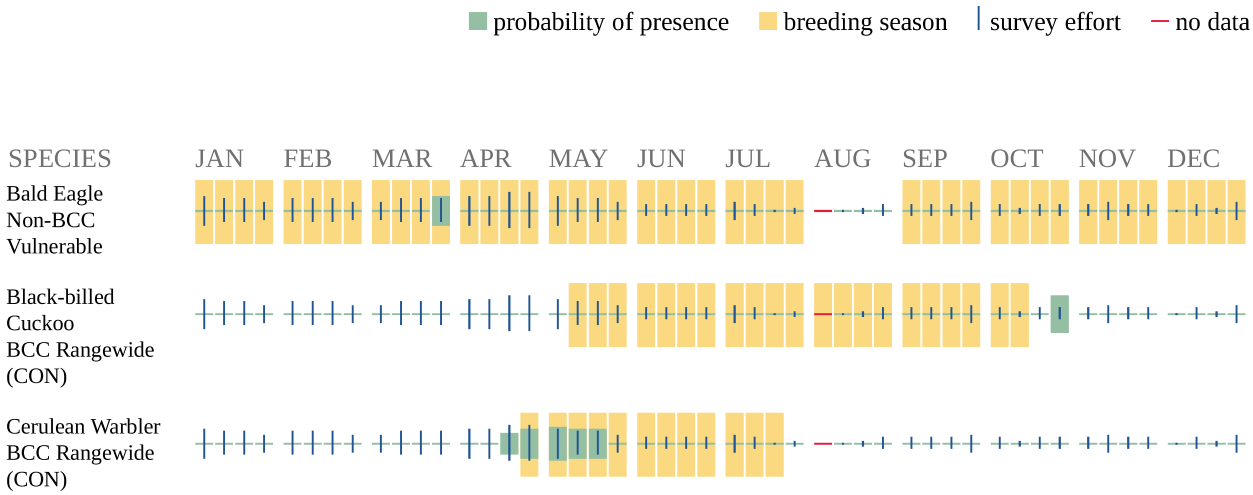
2. To properly present the pattern of presence across the year, the relative probability of presence is calculated. This is the probability of presence divided by the maximum probability of presence across all weeks. For example, imagine the probability of presence in week 20 for the Spotted Towhee is 0.05, and that the probability of presence at week 12 (0.25) is the maximum of any week of the year. The relative probability of presence on week 12 is 0.25/0.25 = 1; at week 20 it is 0.05/0.25 = 0.2.
3. The relative probability of presence calculated in the previous step undergoes a statistical conversion so that all possible values fall between 0 and 10, inclusive. This is the probability of presence score.

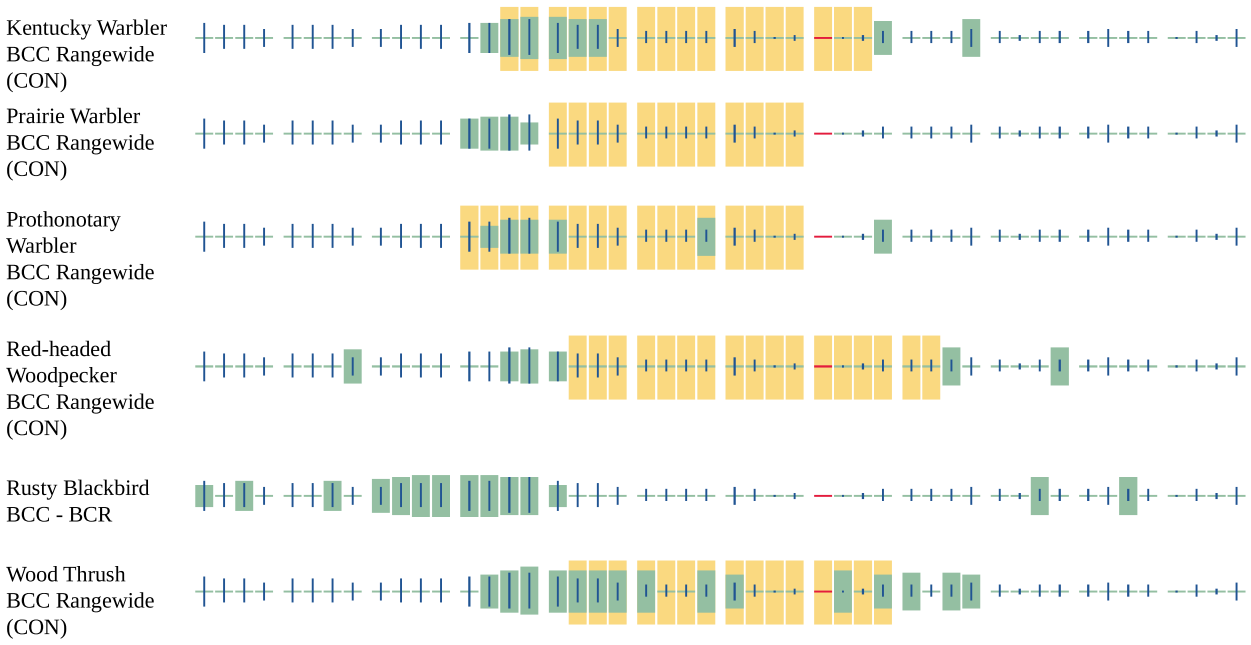
Breeding Season (▪)
Yellow bars denote a very liberal estimate of the time-frame inside which the bird breeds across its entire range. If there are no yellow bars shown for a bird, it does not breed in your project area.

Survey Effort (|)
Vertical black lines superimposed on probability of presence bars indicate the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps. The number of surveys is expressed as a range, for example, 33 to 64 surveys.

No Data (–)
A week is marked as having no data if there were no survey events for that week.

Survey Timeframe
Surveys from only the last 10 years are used in order to ensure delivery of currently relevant information. The exception to this is areas off the Atlantic coast, where bird returns are based on all years of available data, since data in these areas is currently much more sparse.





Additional information can be found using the following links:

- Birds of Conservation Concern https://www.fws.gov/program/migratory-birds/species
- Measures for avoiding and minimizing impacts to birds https://www.fws.gov/library/collections/avoiding-and-minimizing-incidental-take-migratory-birds
- Nationwide conservation measures for birds https://www.fws.gov/sites/default/files/documents/nationwide-standard-conservation-measures.pdf

Migratory Birds FAQ

Tell me more about conservation measures I can implement to avoid or minimize impacts to migratory birds.

Nationwide Conservation Measures describes measures that can help avoid and minimize impacts to all birds at any location year round. Implementation of these measures is particularly important when birds are most likely to occur in the project area. When birds may be breeding in the area, identifying the locations of any active nests and avoiding their destruction is a very helpful impact minimization measure. To see when birds are most likely to occur and be breeding in your project area, view the Probability of Presence Summary. Additional measures or permits may be advisable depending on the type of activity you are conducting and the type of infrastructure or bird species present on your project site.

What does IPaC use to generate the migratory birds potentially occurring in my specified location?

The Migratory Bird Resource List is comprised of USFWS Birds of Conservation Concern (BCC) and other species that may warrant special attention in your project location.

The migratory bird list generated for your project is derived from data provided by the Avian Knowledge Network (AKN). The AKN data is based on a growing collection of survey, banding, and citizen science datasets and is queried and filtered to return a list of those birds reported as occurring in the 10km grid cell(s) which your project intersects, and that have been identified as warranting special attention because they are a BCC species in that area, an eagle (Eagle Act requirements may apply), or a species that has a particular vulnerability to offshore activities or development.

Again, the Migratory Bird Resource list includes only a subset of birds that may occur in your project area. It is not representative of all birds that may occur in your project area. To get a list of all birds potentially present in your project area, please visit the AKN Phenology Tool.

What does IPaC use to generate the probability of presence graphs for the migratory birds potentially occurring in my specified location?
The probability of presence graphs associated with your migratory bird list are based on data provided by the Avian Knowledge Network (AKN). This data is derived from a growing collection of survey, banding, and citizen science datasets .

Probability of presence data is continuously being updated as new and better information becomes available. To learn more about how the probability of presence graphs are produced and how to interpret them, go the Probability of Presence Summary and then click on the "Tell me about these graphs" link.

How do I know if a bird is breeding, wintering, migrating or present year-round in my project area?
To see what part of a particular bird's range your project area falls within (i.e. breeding, wintering, migrating or year-round), you may refer to the following resources: The Cornell Lab of Ornithology All About Birds Bird Guide, or (if you are unsuccessful in locating the bird of interest there), the Cornell Lab of Ornithology Neotropical Birds guide. If a bird on your migratory bird species list has a breeding season associated with it, if that bird does occur in your project area, there may be nests present at some point within the timeframe specified. If "Breeds elsewhere" is indicated, then the bird likely does not breed in your project area.

What are the levels of concern for migratory birds?
Migratory birds delivered through IPaC fall into the following distinct categories of concern:

1. "BCC Rangewide" birds are Birds of Conservation Concern (BCC) that are of concern throughout their range anywhere within the USA (including Hawaii, the Pacific Islands, Puerto Rico, and the Virgin Islands);
2. "BCC - BCR" birds are BCCs that are of concern only in particular Bird Conservation Regions (BCRs) in the continental USA; and
3. "Non-BCC - Vulnerable" birds are not BCC species in your project area, but appear on your list either because of the Eagle Act requirements (for eagles) or (for non-eagles) potential susceptibilities in offshore areas from certain types of development or activities (e.g. offshore energy development or longline fishing).

Although it is important to try to avoid and minimize impacts to all birds, efforts should be made, in particular, to avoid and minimize impacts to the birds on this list, especially eagles and BCC species of rangewide concern. For more information on conservation measures you can implement to help avoid and minimize migratory bird impacts and requirements for eagles, please see the FAQs for these topics.

Details about birds that are potentially affected by offshore projects
For additional details about the relative occurrence and abundance of both individual bird species and groups of bird species within your project area off the Atlantic Coast, please visit the Northeast Ocean Data Portal. The Portal also offers data and information about other taxa besides birds that may be helpful to you in your project review. Alternately, you may download the bird model results files underlying the portal maps through the NOAA NCCOS Integrative Statistical Modeling and Predictive Mapping of Marine Bird Distributions and Abundance on the Atlantic Outer Continental Shelf project webpage.

Bird tracking data can also provide additional details about occurrence and habitat use throughout the year, including migration. Models relying on survey data may not include this information. For additional information on marine bird tracking data, see the Diving Bird Study and the nanotag studies or contact Caleb Spiegel or Pam Loring.

What if I have eagles on my list?
If your project has the potential to disturb or kill eagles, you may need to obtain a permit to avoid violating the Eagle Act should such impacts occur.

Proper Interpretation and Use of Your Migratory Bird Report
The migratory bird list generated is not a list of all birds in your project area, only a subset of birds of priority concern. To learn more about how your list is generated, and see options for identifying what other birds may be in your project area, please see the FAQ "What does IPaC use to generate the migratory birds potentially occurring in my specified location". Please be aware this report provides the "probability of presence" of birds within the 10 km grid cell(s) that overlap your project; not your exact project footprint. On the graphs provided, please also look carefully at the survey effort (indicated by the black vertical bar) and for the existence of the "no data" indicator (a red horizontal bar). A high survey effort is the key component. If the survey effort is high, then the probability of presence score can be viewed as more dependable. In contrast, a low survey effort bar or no data bar means a lack of data and, therefore, a lack of certainty about presence of the species. This list is not perfect; it is simply a starting point for identifying what birds of concern have the potential to be in your project area, when they might be there, and if they might be breeding (which means nests might be present). The list helps you know what to look for to confirm presence, and helps guide you in knowing when to implement conservation measures to avoid or minimize potential impacts from your project activities, should presence be confirmed. To learn more about conservation measures, visit the FAQ "Tell me about conservation measures I can implement to avoid or minimize impacts to migratory birds" at the bottom of your migratory bird trust resources page.

Wetlands

Impacts to NWI wetlands and other aquatic habitats may be subject to regulation under Section 404 of the Clean Water Act, or other State/Federal statutes.

For more information please contact the Regulatory Program of the local U.S. Army Corps of Engineers District.

Please note that the NWI data being shown may be out of date. We are currently working to update our NWI data set. We recommend you verify these results with a site visit to determine the actual extent of wetlands on site.

WETLAND INFORMATION WAS NOT AVAILABLE WHEN THIS SPECIES LIST WAS GENERATED. PLEASE VISIT HTTPS://WWW.FWS.GOV/WETLANDS/DATA/MAPPER.HTML OR CONTACT THE FIELD OFFICE FOR FURTHER INFORMATION.

IPaC User Contact Information

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Address Line 2: Suite 2121
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Phone: 2605782797

Lead Agency Contact Information

Lead Agency: Federal Highway Administration

United States Department of the Interior

FISH AND WILDLIFE SERVICE
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273
http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html

In Reply Refer To: April 22, 2022
Project code: 2022-0005463
Project Name: Des. Nos. 1900268 & 2000217 (Lead Des 1900308) Lloyd Expressway (SR 66) Corridor Improvement Project

Subject: Concurrence verification letter for the 'Des. Nos. 1900268 & 2000217 (Lead Des 1900308) Lloyd Expressway (SR 66) Corridor Improvement Project' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request dated April 22, 2022 to verify that the **Des. Nos. 1900268 & 2000217 (Lead Des 1900308) Lloyd Expressway (SR 66) Corridor Improvement Project** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*). Consultation with the Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C. 1531 et seq.) is required.

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period

allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances, Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

For Proposed Actions that include bridge/culvert or structure removal, replacement, and/or maintenance activities: If your initial bridge/culvert or structure assessments failed to detect Indiana bats, but you later detect bats prior to, or during construction, please submit the Post Assessment Discovery of Bats at Bridge/Culvert or Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

The following species may occur in your project area and **are not** covered by this determination:

- Monarch Butterfly *Danaus plexippus* Candidate

Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

Name

Des. Nos. 1900268 & 2000217 (Lead Des 1900308) Lloyd Expressway (SR 66) Corridor Improvement Project

Description

The Indiana Department of Transportation (INDOT) proposes a corridor improvement project along State Road (SR) 66/Lloyd Expressway (Lloyd Expy) in the City of Evansville, Vanderburgh County, Indiana. The SR 66 and Vann Ave intersection is located approximately 1.79 miles east of US 41, and the SR 66 and Stockwell Rd intersection is located approximately 2.25 miles east of US 41. This project involves the intersections of Vann Ave and Stockwell Rd, located along Lloyd Expy from Villa Dr to Congress Ave. The project also includes a portion of the following roads and intersections: Vann Ave, Stockwell Rd, and Division St. The project setting is urban. Surrounding properties are a mixture of residential, commercial, and institutional properties.

This section of Lloyd Expy is a divided highway that has three 12-foot wide travel lanes in each direction, with variable auxiliary and turn lanes at the signalized intersections, and variable paved shoulders that average 8 feet (inside) and 4 feet (outside) wide. Existing overhead lighting is present throughout the project corridor.

The preliminary recommended alternative would convert the intersection of Lloyd Expy and Vann Ave to a right-in/right-out (RIRO) intersection. The proposed work would remove the existing signals, close the median along Lloyd Expy with permanent concrete barrier walls, eliminate the left turn lanes along Lloyd Expy and Vann Ave, and construct new concrete splitter islands at the Vann Ave approaches. Stockwell Rd would be converted to a hybrid Displaced Left-Turn (DLT) intersection that includes both a displaced left-turn and a blvd left-turn. Private drives would be widened, and full depth replacement of Stockwell Rd pavement within the project limits is proposed. The work would also realign and reconstruct Division St. Existing guardrail would be upgraded. In addition to the proposed added signals and changes to signal heads, existing streetlights would be moved and/or upgraded. Likewise, improvements to the existing storm water system may include piping of existing roadside ditches.

Up to 1.25 acres of temporary and/or permanent ROW, consisting of strips from commercial properties, may be needed. Most of the trees within the project action areas are urban street trees. There is no suitable summer habitat within the project area; however, there is some suitable habitat within the project action area. Less than 0.5 acre of tree clearing/trimming is anticipated. All tree clearing will occur within 100 feet of existing pavement. Trees within the project area are unsuitable, thus time of year restrictions do not apply. The primary tree species observed within the project area were crabapple (Malus sp.) and red mulberry (Morus rubra). Construction is anticipated to start in the spring of 2024 and is expected to last two to three years. The contractor will likely use temporary lighting during construction.

A review of the USFWS GIS database for Indiana bat and northern long-eared bat roosting, hibernacula, and capture sites was conducted for Des. Nos. 1900268 and 2000217 on December 3, 2021. There are no documented sites within a half mile of the project area. No structures that would need to be inspected exist within the project area.

Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

Qualification Interview

1. Is the project within the range of the Indiana bat[1]?

   [1] See Indiana bat species profile

   Automatically answered
   Yes

2. Is the project within the range of the Northern long-eared bat[1]?

   [1] See Northern long-eared bat species profile

   Automatically answered
   Yes

3. Which Federal Agency is the lead for the action?

   A) Federal Highway Administration (FHWA)

4. Are *all* project activities limited to non-construction[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

   [1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

   No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/ rail surfaces[1]?

   [1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

   No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum[1]?

   [1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

   No

7. Is the project located **within** a karst area?

   No

8. Is there *any* suitable[1] summer habitat for Indiana Bat or NLEB **within** the project action area[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

   [1] See the Service's summer survey guidance for our current definitions of suitable habitat.

   [2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the national consultation FAQs.

   Yes

9. Will the project remove *any* suitable summer habitat[1] and/or remove/trim any existing trees **within** suitable summer habitat?

   [1] See the Service's summer survey guidance for our current definitions of suitable habitat.

   No

10. Have presence/probable absence (P/A) summer surveys[1][2] been conducted[3][4] **within** the suitable habitat located within your project action area?

    [1] See the Service's summer survey guidance for our current definitions of suitable habitat.

    [2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

    [3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

    [4] Negative presence/probable absence survey results obtained using the summer survey guidance are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

    No

11. Does the project include activities **within documented Indiana bat habitat**[1][2]?

    [1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

    [2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

    No

12. Does the project include activities **within documented NLEB habitat**[1][2]?

    [1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

    [2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

    No

13. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

    No

14. Does the project include slash pile burning?

    No

15. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

    No

16. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

    No

17. Will the project involve the use of **temporary** lighting *during* the active season?

    Yes

18. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

    Yes

19. Will the project install new or replace existing **permanent** lighting?

    Yes

20. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **permanent** lighting will be installed or replaced?

    Yes

21. Does the project include percussives or other activities (**not including tree removal/ trimming or bridge/structure work**) that will increase noise levels above existing traffic/ background levels?

    No

22. Are *all* project activities that are **not associated with** habitat removal, tree removal/ trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

    Examples: lining roadways, unlighted signage , rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

    Yes

23. Will the project raise the road profile **above the tree canopy**?

    No

24. Are the project activities that are not associated with habitat removal, tree removal/ trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives consistent with a No Effect determination in this key?

    Automatically answered
    Yes, other project activities are limited to actions that DO NOT cause any additional stressors to the bat species as described in the BA/BO

25. **General AMM 1**
    Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

    Yes

26. **Lighting AMM 1**
    Will *all* **temporary** lighting be directed away from suitable habitat during the active season?

    Yes

27. **Lighting AMM 2**
    Does the lead agency use the BUG (Backlight, Uplight, and Glare) system developed by the Illuminating Engineering Society[1][2] to rate the amount of light emitted in unwanted directions?

    [1] Refer to Fundamentals of Lighting - BUG Ratings

    [2] Refer to The BUG System—A New Way To Control Stray Light

    Yes

28. **Lighting AMM 2**
    Will the **permanent** lighting be designed to be as close to 0 for all three BUG ratings as possible, with a priority of "uplight" of 0 and "backlight" as low as practicable?

    Yes

Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

   Yes

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

   No

Avoidance And Minimization Measures (AMMs)

This determination key result includes the committment to implement the following Avoidance and Minimization Measures (AMMs):

LIGHTING AMM 2

When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable.

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

LIGHTING AMM 1

Direct temporary lighting away from suitable habitat during the active season.

Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat

This key was last updated in IPaC on February 24, 2022. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects. The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

IPaC User Contact Information

Agency: Indiana Department of Transportation
Name: Ryan Falls
Address: 3650 South U.S. Highway 41
City: Vincennes
State: IN
Zip: 47591
Email rfalls@indot.in.gov
Phone: 8125821387

Lead Agency Contact Information

Lead Agency: Federal Highway Administration

PARSONS

Appendix D

Section 106 of the National Historic Preservation Act

Excerpt

SECTION 1

Submittal of this form is only required for projects where Category B applies. Projects qualifying under Category A do not require submittal of this form. SECTION 2 (for Conditions of Category B.1 for curb/sidewalk) or SECTION 3 (for Conditions of Category B.9 for drainage structures) may be required as determined by INDOT-Cultural Resources Office (INDOT-CRO) review. INDOT-CRO will notify applicant if the Minor Projects PA does not apply.

Part 1: Project Information-Completed by Applicant (Consultant/PM/Project Sponsor/INDOT District Staff)*

**A qualified professional historian (QP) is not required to complete Part I INDOT-Cultural Resources Office (INDOT-CRO) staff will be responsible for completion of Part II.*

Original Submission Date: 4/14/2022 **Amended Submission Date*:**

**Consult with INDOT-CRO to determine whether an amendment is required. For revisions/updates to original form, please detail in applicable sections below. Please use red font to distinguish the revisions/updates.*

Submitted By (Provide Name and Firm/Organization): Hannah Blad, Lochmueller Group

Project Designation Number: 1900268 & 2000217

Route Number: State Road (SR) 66

Feature crossed (if applicable): N/A

City/Township: City of Evansville/ Knight Township **County:** Vanderburgh County

Project Description: Intersection Improvement, 1.79 mi E of US-41.

The need for this project stem from a high rate of crashes at the intersection of SR 66 with Stockwell Road and Vann Avenue, and congestion issues at Stockwell Road. Safety was evaluated using the Road Hazard Analysis Tool (RoadHAT) software. RoadHAT provides results as an Index of Crash Frequency (ICF) and Index of Crash Cost (ICC), which illustrate how the facility is performing. Per the Indiana Design Manual, an ICF and ICC of zero or less represents average or below-average crash frequency. Per the INDOT Roadway Application for the SR66/Lloyd Expressway and Vann Avenue intersection, for the years 2015 to 2017, the ICF and ICC were 3.18 and 4.87, respectively. Per the INDOT Roadway Application for the SR 66/Lloyd Expressway and Stockwell Road intersection, for the years 2014 to 2016 the ICF and ICC were 2.11 and 3.42, respectively.

Traffic capacity was evaluated in terms of Levels of Service (LOS). LOS is a performance measure that represents quality of service, measured on an A – F scale, with LOS A representing a free flow of traffic and LOS F representing a breakdown in flow (e.g., start-and-stop congestion). The project area is within an urban area, therefore the minimum criteria during peak travel hours (i.e., rush hour) is LOS D. Per the 2020 INDOT Roadway Project Application, at the Lloyd Expressway and Stockwell Road intersection the following movements are "currently failing (LOS F)" during the PM peak: EB through, EB left, WB through, WB left, SB left, and NB left.

The purpose of this intersection improvement project is to reduce the rate of crashes at both intersections and improve the LOS at Lloyd Expressway and Stockwell Road to a minimum of LOS D in the design year, 2043.

Proposed Improvements: Lloyd Expressway/Vann Avenue, Des. No. 1900268

The recommended alternative would convert the intersection of Lloyd Expressway and Vann Avenue to a right-in/right-out (RIRO) intersection. The recommended alternative for the intersection improvement work at Lloyd Expressway and Vann Avenue would remove the existing signals, close the median along Lloyd Expressway with permanent concrete barrier walls, eliminate the left turn lanes along Lloyd Expressway and Vann Avenue with restriping of pavement markings, and construct new concrete splitter islands at the Vann Avenue approaches. The existing curb lines at all four quadrants of the intersection would be maintained with proposed concrete splitter islands.

The existing sidewalk and curb ramps along the south approach of Vann Avenue would remain in-place and undisturbed, as well as the curb ramp at the southeast corner of Vann Avenue and Division Street. A pedestrian refuge is proposed for the southern splitter island. The legacy northeast and southeast curb ramps and northeast sidewalk would be removed because the existing pedestrian overpass is now utilized for this movement. No impacts to the adjoining park, trails, and pedestrian overpass are expected. The existing sidewalk on both sides of Vann Avenue would remain in place along with the curb ramps associated with the east-west pedestrian movement across Vann Avenue. Pedestrian accommodations are being coordinated with the City of Evansville and INDOT.

All proposed work at the Lloyd Expressway/Vann Avenue intersection will remain within existing right-of-way (ROW) in previously disturbed soils. All proposed excavation at this intersection will be within previously disturbed soils and will not extend below previously disturbed depth levels. Due to all work being limited to within previously disturbed soils, and archaeological assessment was not required by INDOT (Matt Coon, personal communication, January 19, 2022). The archaeological report accompanying this submittal is limited to the assessment of the Lloyd Expressway/Stockwell Road intersection.

Proposed Improvements: Lloyd Expressway/Stockwell Road, Des. No. 2000217

The recommended alternative for the intersection improvement at Stockwell Road would convert the traditional signalized intersection to a hybrid Displaced Left-Turn (DLT) intersection that includes both a displaced left-turn and a boulevard left-turn. The recommended alternative for the intersection improvement work at Lloyd Expressway and Stockwell Road would include a crossover in advance of the intersection in the EB direction to displace the left-turn lanes along Lloyd Expressway to be on the opposite side of the through traffic, a bypass right-turn lane for movements from SB Stockwell Road to WB Lloyd Expressway, two proposed signals at the crossover to control the left-turn movements and the bypass right-turn lane, a boulevard left-turn in the WB direction, one proposed signal and a bump-out for turning movements (also known as a “truck loon”) at the boulevard left-turn, modification of the existing signals to accommodate updated traffic movements, and proposed concrete splitter islands to separate opposing directions of traffic. Partial pavement replacement would be done as needed in order to construct the proposed concrete splitter islands and the right slip lane in addition to pavement replacement where the existing concrete median barrier would be demolished. The WB Lloyd Expressway left-turn onto SB Stockwell Road would be eliminated and replaced with the proposed boulevard left-turn west of the intersection. The entrance and exit to the private drive for the Boy Scouts of America and American Red Cross properties would be widened, and full depth replacement of Stockwell Road pavement within the project limits is proposed. Existing drives to the athletic fields owned by University of Evansville would be maintained. The proposed work would also realign and reconstruct Division Street, including pavement removal and full depth pavement construction.

There are no existing pedestrian facilities located within the project limits at Stockwell Road, therefore no pedestrian facilities are proposed.

Existing guardrail would be upgraded. In addition to the proposed added signals and changes to signal heads, existing streetlights would be moved and/or upgraded. Likewise, improvements to the existing storm water system would include piping of existing roadside ditches/tributaries where pavement widening is proposed. Depth of excavation is anticipated to be up to 10 feet below ground surface.

This project would mostly occur within existing, previously disturbed right-of-way (ROW). It is anticipated that an additional 0.66 acre of permanent ROW will be needed near the Stockwell Road intersection.

The proposed maintenance of traffic (MOT) includes phased construction that would allow at least two lanes of EB and WB traffic along Lloyd Expressway to remain open at all times. Detours may be needed for portions of Vann Avenue and Stockwell Road, as well as other local roads. Access to all properties would be maintained. Work is expected to start in the spring of 2024 and last two to three years.

If the project includes any curb, curb ramp, or sidewalk work, please specify the location(s) of such work:
Removal of northeast and southeast curb ramps and northeast sidewalk at the intersection of Vann Avenue and Lloyd Expressway.

For bridge or small structure projects, please list feature crossed, structure number, NBI number, and structure type:

For bridge projects, is the bridge included in INDOT's Historic Bridge Inventory (https://www.in.gov/indot/2531.htm)?

☐ **Yes** ☐ **No**

If yes, did the inventory determine the bridge eligible for or listed in the National Register of Historic Places? Please provide page # of entry in Historic Bridge Inventory.
☐ **Yes** ☐ **No**
Inventory Page #_____

Will there be right-of-way acquisition as part of this project?
☒ **Yes** ☐ **No**

If yes was checked above, please check all that apply:
☒ **Permanent** ☐ **Temporary** ☐ **Reacquisition**

If applicable, identify right-of-way acquisition locations in text below and in attached mapping. Please specify how much (both temporary and permanent) and indicate what activities are included in the proposed right-of-way:
0.66 acre of permanent ROW near the Stockwell Road intersection

Is there *any* potential for additional temporary right-of-way to be needed later for purposes such as access, staging, etc.?
☐ **Yes** ☐ **No**

Archaeology (check one):

- ☐ **All proposed activities are presumed to occur in previously disturbed soils***
  **INDOT-CRO will notify you if project area incudes undisturbed soils and requires an archaeological reconnaissance.*
- ☒ **Project takes place in undisturbed soils and the archaeology report is included in submission or will be forthcoming***
  ** If an archaeology report is required, the Minor Projects PA Form will not be finalized until the report is reviewed and approved by INDOT-CRO. For INDOT-sponsored projects, INDOT-CRO may be able to complete the archaeological investigation. If you would like to request that INDOT-CRO complete an archaeological investigation, please contact the INDOT-CRO archaeology team lead. See CRM Pt. 1 Ch. 3 for current contact information.*

Please specify all applicable categories and condition(s) (highlight applicable conditions in yellow)*:
**Include full category text, including any conditions. INDOT-CRO will finalize categories upon their review.*

B-1. Replacement, repair, or installation of curbs, curb ramps, or sidewalks, including when such projects are associated with roadway work such as surface replacement, reconstruction, rehabilitation, or resurfacing projects, including overlays, shoulder treatments, pavement repair, seal coating, pavement grinding, and pavement marking, under the following conditions ***[BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied]***:

Condition A (Archaeological Resources)
One of the two conditions listed below must be satisfied *(EITHER Condition i or Condition ii must be satisfied)*:

i. Work occurs in previously disturbed soils; *OR*
ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the Division of Historic Preservation and Archaeology (DHPA) and any archaeological site form information will be entered directly into the State Historic Architectural and Archaeological Database (SHAARD) by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

Condition B (Above-Ground Resources)
One of the two conditions listed below must be satisfied *(EITHER Condition i or Condition ii must be satisfied)*:

i. Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; *OR*
ii. Work occurs adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource under one of the two additional conditions listed below *(EITHER Condition a OR Condition b must be met, and field work and documentation must be completed as described below)*:
    a. No unusual features, including but not limited to historic brick or stone sidewalks, curbs, or curb ramps, stepped or elevated sidewalks and historic brick or stone retaining walls are present in the project area adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; *OR*
    b. Unusual features, including but not limited to historic brick or stone sidewalks, curbs, or curb ramps, stepped or elevated sidewalks and historic brick or stone retaining walls are present in the project

area adjacent to or within a National Register-listed or National Register-eligible individual above-ground resource or district and ANY ONE of the conditions *(1, 2, or 3)* listed below must be fulfilled:

1. Unusual features described above will not be impacted by the project. Firm commitments regarding the avoidance of these features must be listed in the MPPA determination form and the NEPA document and must be entered into the INDOT Project Commitments Database. These projects will also be flagged for quality assurance reviews by INDOT Cultural Resources Office during/after project construction.

2. Unusual features described above have been determined not to contribute to the significance of the historic resource by INDOT Cultural Resources Office in consultation with the SHPO based on an analysis and justification prepared by their staff or review of such information from other qualified professional historians.
3. Impacts to unusual features described above have been determined by INDOT Cultural Resources Office to be so minimal that they do not diminish any of the characteristics that contribute to the significance of the historic resource, based on an analysis and justification prepared by their staff or review of such information from other qualified professional historians.

B-2. Installation of new lighting, signals, signage, and other traffic control devices under the following conditions ***[BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied]***:

Condition A (Archaeological Resources)

One of the two conditions listed below must be met *(EITHER Condition i or Condition ii must be satisfied)*:

i. Work occurs in previously disturbed soils; *OR*

ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the DHPA and any archaeological site form information will be entered directly into the SHAARD by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

Condition B (Above-Ground Resources)

Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource.

B-3. Construction of added travel, turning, or auxiliary lanes (e.g., bicycle, truck climbing, acceleration, and deceleration lanes) and shoulder widening under the following conditions ***[BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied]***:

Condition A (Archaeological Resources)

One of the two conditions listed below must be met *(EITHER Condition i or Condition ii must be satisfied):*

i. Work occurs in previously disturbed soils; *OR*

ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the DHPA and any archaeological site form information

will be entered directly into the SHAARD by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

Condition B (Above-Ground Resources)
Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource.

Check ☐ if SECTION 2: Minor Projects PA Category B-1, Condition B-ii Submission is included

Check ☐ if SECTION 3: Minor Projects PA Category B-9, Condition B-i-c-2 or B-ii-b-3 Submission is included

Part II: Completed by INDOT-CRO

Amendments will be shown in red font.

Information reviewed (please check all that apply):

General project location map ☒ USGS map ☐ Aerial photograph ☒ Soil survey data ☐

General project area photos ☐ Archaeology Reports ☒ Historic Property Reports ☐

Indiana Historic Buildings, Bridges, and Cemeteries Map/Interim Report ☒

Bridge inspection information/BIAS ☐ Historic Bridge Inventory Database ☐

SHAARD ☒ SHAARD GIS ☒ Streetview Imagery ☒ County GIS Data/Property Cards ☒
Other (please specify): Indiana State Historic Architectural and Archaeological Research Database (SHAARD); Indiana Buildings, Bridges, and Cemeteries Map (IBBCM) website; *Vanderburgh County Interim Report*; Arc Map GIS; Vanderburgh County GIS (accessed via https://evvc-evvc.opendata.arcgis.com/); online street-view imagery; MPPA form (including maps and photographs) sent April 14th, 2022 by Lochmueller Group, on file at Cultural Resources Office (CRO);

Bybee, Alexandra D.
2016 Archaeological Excavation of the Historic Graves from the Evansville State Hospital Cemetery (12Vg598), Knight Township, Evansville, Vanderburgh County, Indiana (DES #0100574). Contract Publication Series 14-007. Cultural Resource Analysts, Inc., Evansville, Indiana.

Bybee, Alexandra D., and Karen Supak
2013 Management Summary: Additional Archaeological Investigation at the Evansville State Hospital Cemetery (12Vg598), Vanderburgh County, Indiana (Des #0100574). Cultural Resource Analysts, Inc., Evansville, Indiana.

Cantin, Mark, C. Russell Stafford and John Schwegman
2003 Archaeological Investigations of a Potential Unmarked Cemetery on the Former Grounds of the Evansville State Hospital as Related to INDOT Project CMAQ-006-4 (041), DES. #9804080, Improvements to Lloyd's Expressway (SR 66)/Stockwell Road Interchange, Evansville, Vanderburgh County, Indiana. Cultural Resources Management Report No. 03-15. Anthropology Laboratory Indiana University, Bloomington.

Kelley, Lisa
2022 A Phase Ia Archaeological Reconnaissance for Two Proposed Intersection Improvement Projects (Vann Avenue and Stockwell Road) on SR 66, at 1.79 mi E of US 41, in Evansville, Vanderburgh County, Indiana (INDOT Des. Nos. 1900268 and 2000217).

Martin, Andrew V., and Karen B. Supak
2013 Geophysical Survey and Phase Ia Archaeological Reconnaissance for the Pigeon Creek Greenway Passage Project – SR 66/Lloyd Expressway Pedestrian Overpass, Vanderburgh County, Indiana (DES #0100574). Contract Publication Series 12-290. Cultural Resource Analysts, Inc., Evansville, Indiana.

Are there any commitments associated with this project? If yes, please explain and include in the Additional Comments Section below. Yes ☐ No ☒

Does the project result in a de minimis impact to a Section 4(f) protected historic resource? If yes, please explain in the Additional Comments Section below. Yes ☐ No ☒

Additional Comments:

Above-ground Resources

An INDOT Cultural Resources Office (CRO) historian who meets the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61 performed a desktop review, checking the Indiana Register of Historic Sites and Structures (State Register) and National Register of Historic Places (National Register) lists for Vanderburgh County. No listed resources are located immediately adjacent to the project area, a distance that serves as an adequate potential area of effects given the scope of the project and the surrounding terrain.

The Indiana Historic Sites and Structures Inventory (IHSSI) and National Register information for Vanderburgh County are available in the Indiana State Historic Architectural and Archaeological Research Database (SHAARD) and the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBCM). The *Vanderburgh County Interim Report* (1994; Evansville Scattered Sites; Evansville State Hospital Historic District) was also consulted. All sites were reviewed through the IHBBCM, which contains the most recently updated SHAARD information. One (1) IHSSI documented resource rated higher than "Contributing" is located immediately adjacent to the project area:

- IHSSI# 163-196-44(001-028), Evansville State Hospital Historic District

According to the IHSSI rating system, generally properties rated "Contributing" do not possess the level of historical or architectural significance necessary to be considered individually National Register-eligible, although they would contribute to a historic district. If they retain material integrity, properties rated "Notable" might possess the necessary level of significance after further research. Properties rated "Outstanding" usually possess the necessary level of significance to be considered National Register eligible if they retain material integrity.

Because the location of the project is not adjacent to a National Register-listed or eligible resource, a field visit by a Qualified Professional historian is not required. The INDOT-CRO historian reviewed structures adjacent to the project area utilizing online aerial, street-view photography, The IBBCM, and the Vanderburgh County GIS website. The project area is located in an urban setting; the adjacent building stock consists primarily of mid twentieth to early twenty-first century residential and commercial buildings. In regard to the Evansville State Hospital Historic District, noted above, ariel imagery shows that a new hospital building was constructed in the early 2000s. By around 2007, the former hospital buildings, and the other ancillary buildings documented in the 1994 Interim Report had been demolished. None of the resources adjacent to the project area appear to possess the integrity or significance necessary to be considered National Register-eligible.

Based on the available information, as summarized above, no above-ground concerns exist.

Archaeological Resources

An INDOT-CRO archaeologist who meets the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61 reviewed the Phase Ia field reconnaissance survey report completed for the project by Cultural Resource Analysts (CRA) (Kelley 2022). Three archaeological sites (12Vg536, 12Vg597, and 12Vg598) were previously recorded within or adjacent to the project area.

Therefore, there are no archaeological concerns provided the project scope does not change.

Accidental Discovery: If any archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, construction within 100 feet of the discovery will be stopped, and INDOT-CRO and the Division of Natural Resources-Division of Historic Preservation and Archaeology (DNR-DHPA) will be notified immediately.

INDOT-CRO staff reviewer(s): Clint Kelly and Matt Coon

INDOT Approval Date: 6/28/22

Amendment Approval Date (if applicable):

****Be sure to attach this form to the National Environmental Policy Act documentation for this project. Also, the NEPA documentation shall reference and include the description of the specific stipulation in the PA that qualifies the project as exempt from further Section 106 review.*

Please attach the following to this form:

see Appendix B for graphics

INDIANA ARCHAEOLOGICAL SHORT REPORT

State Form 54566 (R2 / 11-20)

Excerpt

INDIANA DEPARTMENT OF NATURAL RESOURCES
DIVISION OF HISTORIC PRESERVATION AND ARCHAEOLOGY
402 West Washington Street, Room W274
Indianapolis, Indiana 46204-2739
Telephone Number: (317) 232-1646
Fax Number: (317) 232-0693
E-mail: dhpa@dnr.IN.gov

Where applicable, the use of this form is recommended but not required by the Division of Historic Preservation and Archaeology (DHPA).

Name(s) of author(s)	Date *(month, day, year)*
Lisa Kelley	April 13, 2022

Title of project
A Phase Ia Archaeological Reconnaissance for Two Proposed Intersection Improvement Projects (Vann Avenue and Stockwell Road) on SR 66, at 1.79 mi E of US 41, in Evansville, Vanderburgh County, Indiana (INDOT Des. Nos. 1900268 and 2000217)

This document is being used to report on the results of:
☐ Records check only ☒ Records check and Phase Ia archaeological reconnaissance
☐ An addendum to a previous archaeological report. *For an addendum, provide the following information.*

Name(s) of author(s) of previous report

Title of previous report

Date of previous report *(month, day, year)*	DHPA number

PROJECT OVERVIEW

Description of project
The purpose of this proposed project is to improve the intersections and roadways surrounding SR 66 (Lloyd Expressway) at both Vann Avenue (Indiana Department of Transportation [INDOT] Des. No. 1900268) and Stockwell Road (INDOT Des. No. 2000217) in Evansville, Indiana (Figures 1–3). The proposed work at the Vann Avenue location will include the removal of the concrete median barrier, removal and updating of signage, sidewalk and curb ramp removal, and drainage improvements including proposed inlets. For this intersection work, there will be minimal grading and all work will be contained within the existing disturbed right-of-way (ROW) (see Figure 3a). Because the work for this entire location will be limited to previously disturbed soils, an archaeological assessment was not required by INDOT (Matt Coon, personal communication, January 19, 2022). Therefore, the remainder of this report will address only the Stockwell Road location.

The proposed work near the Stockwell Road location will include all the similar work as previously described for Vann Avenue, as well as partial re-alignment of Division Street, widening along the south and north sides of SR 66, proposed ditch grading, and additional drainage improvements including roadside ditch detention, a new storm sewer drainage system, and proposed inlets, pipes, manholes, and trunklines. The proposed work for the Stockwell Road improvements will entail approximately 0.27 ha (0.66 acres) of new ROW on the south side of SR 66. The survey area for the Stockwell Road intersection covered 4.6 ha (11.5 acres) (see Figures 3b and 3c).

INDOT designation number(s)	Project number	DHPA number	DHPA plan number
1900268 and 2000217	I20L009		

Prepared for: *(Company / Institution / Agency)*
Lochmueller Group, Inc.

Name of contact
Gary Quigg

Address *(number and street, city, state, and ZIP code)*
3502 Woodview Trace, Suite 150

Telephone number	E-mail address
(317) 334-6807	GQuigg@lochgroup.com

Name of principal investigator
Lisa J. Kelley, RPA 4535

Name of company / institution
Cultural Resource Analysts, Inc.

Address *(number and street, city, state, and ZIP code)*
201 NW 4th Street, Suite 204

Telephone number	E-mail address
(812) 253-3009	amartin@crai-ky.com

Signature of principal investigator *(Required)*	Date *(month, day, year)*
Lisa Kelley	April 13, 2022

PROJECT LOCATION

County	USGS 7.5' series topographic quadrangle	Civil township
Vanderburgh	Evanvsille South and Newburgh	Knight

RECOMMENDATIONS

Records check *(Check all that apply.)*

- ☐ No archaeological investigation is recommended before the project is allowed to proceed because the records check has determined that the project area does not have the potential to contain archaeological resources.
- ☐ A Phase Ia archaeological reconnaissance is recommended.
- ☐ A cemetery development plan may be required under Indiana Code 14-21-1-26.5 because project ground disturbance will be within 100 feet of a cemetery.

Phase Ia archaeological reconnaissance *(Check all that apply.)*

- ☒ It is recommended that the project be allowed to proceed as planned because the Phase Ia archaeological reconnaissance has located no archaeological sites within the project area and/or previously recorded sites that were investigated warrant no additional investigation.
- ☐ It is recommended that Phase Ic archaeological subsurface reconnaissance be conducted before the project is allowed to proceed. The Phase Ia archaeological reconnaissance has determined that the project area includes landforms which have the potential to contain buried archaeological deposits.

Other recommendations / commitments

If any previously unrecorded archaeological materials are encountered during construction activities, the DHPA should be notified immediately at (317) 232-1646, as well as the INDOT Cultural Resources Office (CRO) at (317) 461-0876. If human remains are discovered, construction activities should cease immediately, and the DHPA, the local coroner, and the local law enforcement agency must be notified (see below).

PARSONS

Appendix E

Red Flag Investigation and Hazardous Materials

INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N758-ES
Indianapolis, Indiana 46204

PHONE: (855) 463-6848
(855) INDOT4U

Eric Holcomb, Governor
Michael Smith, Commissioner

Date: June 13, 2022

To: Site Assessment & Management (SAM)
Environmental Policy Office - Environmental Services Division (ESD)
Indiana Department of Transportation (INDOT)
100 N Senate Avenue, Room N758-ES
Indianapolis, IN 46204

From: Cedric Diefenbaugh
Parsons
101 W Ohio Street, Suite 2121
Indianapolis, IN 46204
cedric.diefenbaugh@parsons.com

Re: RED FLAG INVESTIGATION
DES #1900268 and 2000217, State Project
Corridor Improvement Project
State Road (SR) 66 from 1.8 Miles East of US 41 to 2.7 Miles West of I-69
Vanderburgh County, Indiana

PROJECT DESCRIPTION

Brief Description of Project: The Indiana Department of Transportation (INDOT), in cooperation with the Federal Highway Administration (FHWA), proposes a corridor improvement project along SR 66/Lloyd Expressway (Lloyd Expy) in the City of Evansville, Vanderburgh County, Indiana, as part of the "Lloyd 4 U" project. This project is located in Sections 22, 23, 26 and 27 of Township 6 South, Range 10 West, in the City of Evansville, Vanderburgh County. It is shown on the Evansville South and Newburgh, Indiana United States Geological Survey (USGS) topographical 7.5 minute quadrangle maps. The study area begins along Lloyd Expy at Villa Drive and extends east to Congress Avenue. Study area limits also include Vann Avenue, from Sycamore Street to Division Street; Stockwell Road from John Street to approximately 100 feet north of Division Street; and Division Street from approximately 1,110 feet west of Stockwell Road to Stockwell Road. Surrounding properties include residential, institutional, and commercial uses.

The recommended alternative for Lloyd Expy and Stockwell Road would convert the traditional signalized intersection to a hybrid Displaced Left-Turn (DLT) intersection that includes both a displaced left-turn and a boulevard left-turn. This would maintain all existing movements through the intersection. The proposed work would include: a crossover in advance of the intersection in the eastbound (EB) direction to displace the left-turn lanes along Lloyd Expy to be on the opposite side of the through traffic, a bypass right-turn lane for movements from southbound (SB) Stockwell Road to westbound (WB) Lloyd Expy, two signals at the crossover to control the left-turn movements and the bypass right-turn lane, a boulevard left-turn in the westbound (WB) direction, one signal and a bump-out for turning movements (also known as a "truck loon") at the boulevard left-turn, modification of the existing signals to accommodate updated traffic movements, and concrete splitter islands to separate opposing directions of traffic. The WB Lloyd Expy left-turn onto SB Stockwell Road would be eliminated and replaced with the boulevard left-turn west of the intersection. The entrance

and exit to the private drive for the Boy Scouts of America and American Red Cross properties would be widened. There would be full depth replacement of the Stockwell Road pavement within the project limits. The proposed work would also realign and reconstruct Division Street, including pavement removal and full depth pavement construction from approximately 1,100 feet west of Stockwell Road to Stockwell Road. Existing drives to the athletic fields owned by the University of Evansville would be maintained. The existing guardrail and streetlights would be replaced or relocated as needed. Improvements to the existing storm water system are proposed in areas where there would be pavement widening. In these areas, there would be piping of existing roadside ditches and tributaries as needed. No work to the larger structures (i.e. 36-inches or greater and/or listed in BIAS), is proposed.

Bridge Work Included in Project: Yes ☐ No ☒ Structure #(s) _____

If this is a bridge project, is the bridge Historical? Yes ☐ No ☐ , Select ☐ Non-Select ☐

(Note: If the project involves a historical bridge, please include the bridge information in the Recommendations Section of the report).

Culvert Work Included in Project: Yes ☐ No ☒ Structure #(s) _____

Proposed right of way: Temporary ☒ # Acres <1.0 Permanent ☒ # Acres <1.0, Not Applicable ☐

Type and proposed depth of excavation: Up to 10 feet below grade for drainage work that may include upgrading existing storm sewers.

Maintenance of traffic (MOT): The proposed MOT includes phased construction to allow at least two lanes of EB and WB traffic along Lloyd Expy to remain open at all times. Detours may be needed for portions of Vann Avenue and Stockwell Road, as well as other local roads. Access to all properties would be maintained.

Work in waterway: Yes ☒ No ☐ Below ordinary high water mark: Yes ☒ No ☐

State Project: ☒ LPA: ☐

Any other factors influencing recommendations: N/A

INFRASTRUCTURE TABLE AND SUMMARY

Infrastructure
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:

Religious Facilities	**1**	Recreational Facilities	**6**
Airports[1]	**1**[1]	Pipelines	**3**
Cemeteries	**5**	Railroads	**1**
Hospitals	**1**	Trails	**17**
Schools	**1**	Managed Lands	**3**

[1]In order to complete the required airport review, a review of public-use airports within 3.8 miles (20,000 feet) is required.

Religious Facilities: One (1) religious facility is located within the 0.5 mile search radius. Grace Lutheran Church is 0.28 mile to the southwest of the project area near the South Boeke Road and East Sycamore Street intersection. No impact is expected.

Airports[1]: Although not located within the 0.5 mile search radius, one (1) public-use airport, Evansville Regional Airport, is located within 3.8 miles (20,000 feet) of the project area. The public use airport is located approximately 3.7 miles northwest of the project area; therefore, early coordination with INDOT Aviation will occur.

Cemeteries: Five (5) cemeteries are located within the 0.5 mile search radius. Four (4) cemeteries associated with the Evansville State Hospital are adjacent to the project area. A Cemetery Development Plan may be required since this project is within 100 feet of the cemetery. Coordination with INDOT Cultural Resources will occur.

Hospitals: One (1) hospital is located within the 0.5 mile search radius. The nearest hospital is approximately 0.12 mile south of the project area near the John Street and Stockwell Road intersection. Coordination with the Evansville State Hospital will occur.

Schools: One (1) school is located within the 0.5 mile search radius. Although the icon associated with Evansville Christian School is mapped 0.40 mile southeast of the project area, the facility is actually located approximately 3.1 miles to the southeast near the Outer Lincoln Avenue and Epworth Road intersection. No impact is expected.

Recreational Facilities: Six (6) recreational facilities are located within the 0.5 mile search radius. The nearest facilities, Harrison High School Ballfield and Evansville Sports Park, are adjacent to the project area near the Division Street and Vann Avenue intersection. Coordination with William Henry Harrison High School and Evansville Sports Park will occur.

Pipelines: Three (3) pipeline segments are located within the 0.5 mile search radius. One (1) pipeline segment, Southern Indiana Gas & Electric Co., is adjacent to the project area. Coordination with INDOT Utilities and Railroads should occur.

Railroads: One (1) railroad is located within the 0.5 mile search radius. One (1) railroad segment, Unknown RR which appears to be gone, crosses the project area. Standard coordination will occur with INDOT Utilities and Railroads by the Project Management Team or their consultant no later than the Ready for Contracts (RFC) date.

Trails: Seventeen (17) trail segments are located within the 0.5 mile search radius. One (1) open trail segment, State Hospital Walking Path, is located adjacent to the project area at the southeast corner of the Vann Avenue and Lloyd Expy intersection. Coordination with the Evansville Department of Parks and Recreation will occur.

Managed Lands: Three (3) Managed Lands are located within the 0.5 mile search radius. State Hospital Grounds Park is adjacent to the project area at the southeast corner of the Vann Avenue and Lloyd Expy intersection. Coordination with Evansville Department of Parks and Recreation is recommended.

WATER RESOURCES TABLE AND SUMMARY

Water Resources Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
NWI - Points	**N/A**	Canal Routes - Historic	**N/A**
Karst Springs	**N/A**	NWI - Wetlands	**8**
Canal Structures – Historic	**N/A**	Lakes	**3**
NPS NRI Listed	**N/A**	Floodplain - DFIRM	**1**
NWI-Lines	**N/A**	Cave Entrance Density	**N/A**
IDEM 303d Listed Streams and Lakes (Impaired)	**N/A**	Sinkhole Areas	**N/A**
Rivers and Streams	**2**	Sinking-Stream Basins	**N/A**

If unmapped water features are identified that might impact the project area, direct coordination with INDOT ESD Ecology and Waterway Permitting will occur.

Rivers and Streams: Two (2) stream segments are located within the 0.5 mile search radius. One (1) stream segment is located approximately 0.15 mile north of the project area. No impact is expected.

NWI – Wetlands: Eight (8) wetlands are located within the 0.5 mile search radius. One (1) wetland is located adjacent to the project area. A Waters of the US Report is recommended based on mapped features, and coordination with INDOT ESD Ecology and Waterway Permitting will occur.

Lakes: Three (3) lakes are located within the 0.5 mile search radius. The nearest lake is 0.10 mile north of the project area. No impact is expected.

Floodplain – DFIRM: One (1) floodplain polygon is located within the 0.5 mile search radius. The nearest floodplain polygon is located approximately 0.32 mile north of the project area. No impact is expected.

MINING AND MINERAL EXPLORATION TABLE AND SUMMARY

Mining/Mineral Exploration Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Petroleum Wells	**N/A**	Mineral Resources	**N/A**
Mines – Surface	**N/A**	Mines – Underground	**N/A**

Explanation: No mining and mineral exploration resources were identified within the 0.5 mile search radius.

HAZARDOUS MATERIAL CONCERNS TABLE AND SUMMARY

Hazardous Material Concerns Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Superfund	**N/A**	Manufactured Gas Plant Sites	**N/A**
RCRA Generator/ TSD	**3**	Open Dump Waste Sites	**N/A**
RCRA Corrective Action Sites	**N/A**	Restricted Waste Sites	**N/A**
State Cleanup Sites	**1**	Waste Transfer Stations	**N/A**
Septage Waste Sites	**N/A**	Tire Waste Sites	**N/A**
Underground Storage Tank (UST) Sites	**12**	Confined Feeding Operations (CFO)	**N/A**
Voluntary Remediation Program	**N/A**	Brownfields	**1**
Construction Demolition Waste	**N/A**	Institutional Controls	**2**
Solid Waste Landfill	**1**	NPDES Facilities	**3**
Infectious/Medical Waste Sites	**N/A**	NPDES Pipe Locations	**N/A**
Leaking Underground Storage (LUST) Sites	**15**	Notice of Contamination Sites	**N/A**

Unless otherwise noted, site specific details presented in this section were obtained from documents reviewed on the Indiana Department of Environmental Management (IDEM) Virtual File Cabinet (VFC).

RCRA Generator/TSD: Three (3) RCRA Generator/TSD sites are located within the 0.5 mile search radius. The nearest site, General Electric Company (ABB), 401 North Congress Avenue, Agency Interest Identification (AID) 12618, is located 0.24 mile to the northeast of the project area near the Virginia Street and Congress Avenue intersection. Based on the January 28, 2019 Inspection Summary Letter, no violations were observed. No impact is expected.

State Cleanup Sites: One (1) State Cleanup Site is located within the 0.5 mile search radius. Evansville State Hospital, 3400 Lincoln Avenue, AID 12506, is incorrectly mapped in GIS. This facility is located 0.12 mile to the south of the project area near the John Street and Stockwell Road intersection. Two (2) 700 gallon fuel oil USTs and petroleum contaminated soil were removed in 2005. No impact is expected.

Underground Storage Tank (UST) Sites: Twelve (12) UST sites are located within the 0.5 mile search radius. Armed Forces Reserve Center, 2900 East Division Street, AID 40226, is located adjacent to the north of the project area near the Vann Avenue and East Division Street intersection. One (1) 10,000 gallon fuel oil UST was removed in 1993, and confirmatory soil samples were all less than detection limits for Total Petroleum Hydrocarbons (TPH). No impact is expected.

Robert Stadium, 2600 East Division Street, AID 41535, is located adjacent to the north of the project area near the Vann Avenue and East Division Street intersection. According to the October 22, 1992, Notification for Underground Storage Tanks, two USTs were removed. No additional information was found in the VFC. If excavation occurs in this area, proper handling, removal, and disposal of soil and/or groundwater may be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.

Solid Waste Landfill: One (1) solid waste landfill site is located within the 0.5 mile search radius. Wesselman Nature Center, 551 North Boeke Road, AID 1328, is located 0.25 mile to the north of the project area. No information was found in the VFC for this site. No impact is expected.

Leaking Underground Storage (LUST) Sites: Fifteen (15) LUST sites are located within the 0.5 mile search radius. The nearest site, Evansville C-P Inc., 4000 East Division Street, AID 40791, is located adjacent to the northeast of the project area near the East Division Street and Stockwell Road intersection. One UST was removed from the facility in July, 26 1990 and nineteen (19) tons of contaminated soil was removed from the tank area. According to the August 30, 1990, Site Assessment Results letter, IDEM determined that no further corrective action was required due to the fact that all confirmatory soil samples were less than 10 ppm TPH, which was the closure level in effect at that time. No impact is expected.

Brownfields: One (1) brownfield site is located within the 0.5 mile search radius. Wright Motors, 4500 East Division Street, AID 41429, is located 0.13 mile east of the project area near the Congress Avenue and Division Street intersection. On October 12, 2017 IDEM issued a Comfort Letter – Bona Fide Prospective Purchaser to Magna Motors Properties LLC to outline applicable limitations on liability with respect to hazardous substances and/or petroleum products found on site. No impact is expected.

Institutional Control: Two (2) institutional control sites are located within the 0.5 mile search radius. The nearest site, Formerly Consolidated Freightways (Premier Transportation), 201 North Congress Avenue, AID 40570, is located 0.13 mile to the north of the project area near the Indiana Street and Congress Avenue intersection. No impact is expected.

NPDES Facilities: Three (3) National Pollutant Discharge Elimination System (NPDES) facilities are located within the 0.5 mile search radius. The Pedestrian Trail over SR 66 (Lloyd Expressway) is located within the project area near the Vann Avenue and Lloyd Expy intersection. The permit expired on July 3, 2019. No impact is expected.

ECOLOGICAL INFORMATION SUMMARY

The Vanderburgh County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high quality natural communities is provided at https://www.in.gov/dnr/nature-preserves/files/np_vanderburgh.pdf. A preliminary review of the Indiana Natural Heritage Database by INDOT ESD did indicate the presence of ETR species within the 0.5 mile search radius. Coordination with USFWS and IDNR will occur.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

RECOMMENDATIONS SECTION

Include recommendations from each section. If there are no recommendations, please indicate N/A:

INFRASTRUCTURE:

Airports[1]: One (1) public-use airport, Evansville Regional Airport, is located within 3.7 miles northwest of the project area. Coordination with INDOT Aviation will occur.

Cemeteries: Four (4) cemeteries associated with the Evansville State Hospital are adjacent to the project area. A Cemetery Development Plan may be required since this project is within 100 feet of the cemetery. Coordination with INDOT Cultural Resources will occur.

Hospitals: One (1) hospital is located approximately 0.12 mile south of the project area near the John Street and Stockwell Road intersection. Coordination with the Evansville State Hospital will occur.

Recreational Facilities: Two (2) recreational facilities are located adjacent to the project area near the Division Street and Vann Avenue intersection. Coordination with William Henry Harrison High School and Evansville Sports Park will occur.

Pipelines: One (1) pipeline segment, Southern Indiana Gas & Electric Co., is adjacent to the project area. Coordination with INDOT Utilities and Railroads should occur.

Railroads: One (1) railroad segment, Unknown RR which appears to be gone, crosses the project area. Standard coordination will occur with INDOT Utilities and Railroads by the Project Management Team or their consultant no later than the Ready for Contracts (RFC) date.

Trails: One (1) open trail segment, State Hospital Walking Path, is located adjacent to the project area at the southeast corner of the Vann Avenue and Lloyd Expy intersection. Coordination with the Evansville Department of Parks and Recreation will occur.

Managed Lands: One (1) Managed Land, State Hospital Grounds Park, is located adjacent to the project area at the southeast corner of the Vann Avenue and Lloyd Expy intersection. Coordination with Evansville Department of Parks and Recreation is recommended.

WATER RESOURCES: If unmapped water features are identified that might impact the project area, direct coordination with INDOT ESD Ecology and Waterway Permitting will occur. A Waters of the US Report is recommended based on the presence of mapped features, and coordination with INDOT ESD Ecology and Waterway Permitting will occur for the following features:

- One (1) wetland is located adjacent to the project area.

MINING/MINERAL EXPLORATION: N/A

HAZARDOUS MATERIAL CONCERNS:

- UST: Robert Stadium, 2600 East Division Street, AID 41535, is located adjacent to the north of the project area near the Vann Avenue and East Division Street intersection. According to the October 22, 1992, Notification for Underground Storage Tanks, two USTs were removed. No additional information was found in the VFC. If excavation occurs in this area, it is possible that petroleum may be encountered. Before proper removal and disposal of soil and/or groundwater, analysis for lead will be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.

ECOLOGICAL INFORMATION: Coordination with USFWS and IDNR will occur. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

INDOT ESD concurrence: Nicole Fohey-Breting Digitally signed by Nicole Fohey-Breting Date: 2022.06.29 15:18:25 -04'00' (Signature)

Prepared by:

Cedric Diefenbaugh

Cedric Diefenbaugh
Environmental Planner
Parsons

Graphics:

A map for each report section with a 0.5 mile search radius buffer around all project area(s) showing all items identified as possible items of concern is attached. If there is not a section map included, please change the YES to N/A:

SITE LOCATION: YES See Appendix B

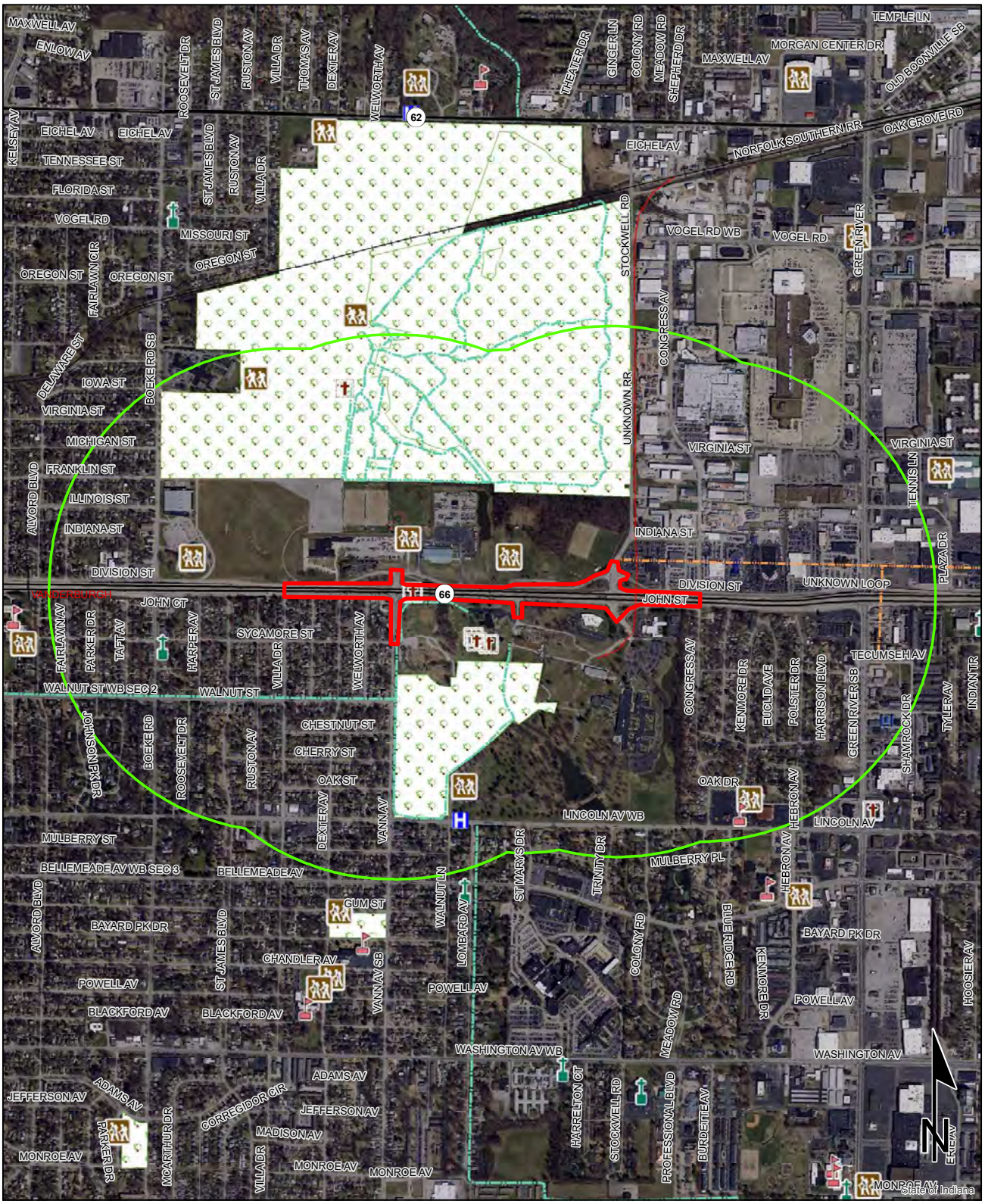
INFRASTRUCTURE: YES

WATER RESOURCES: YES

MINING/MINERAL EXPLORATION: N/A

HAZARDOUS MATERIAL CONCERNS: YES

Red Flag Investigation - Infrastructure
SR 66, From 1.8 Miles East of US 41 to 2.7 Miles West of I-69
Des. No. 1900268 and 2000217, Corridor Improvement
Vanderburgh County, Indiana



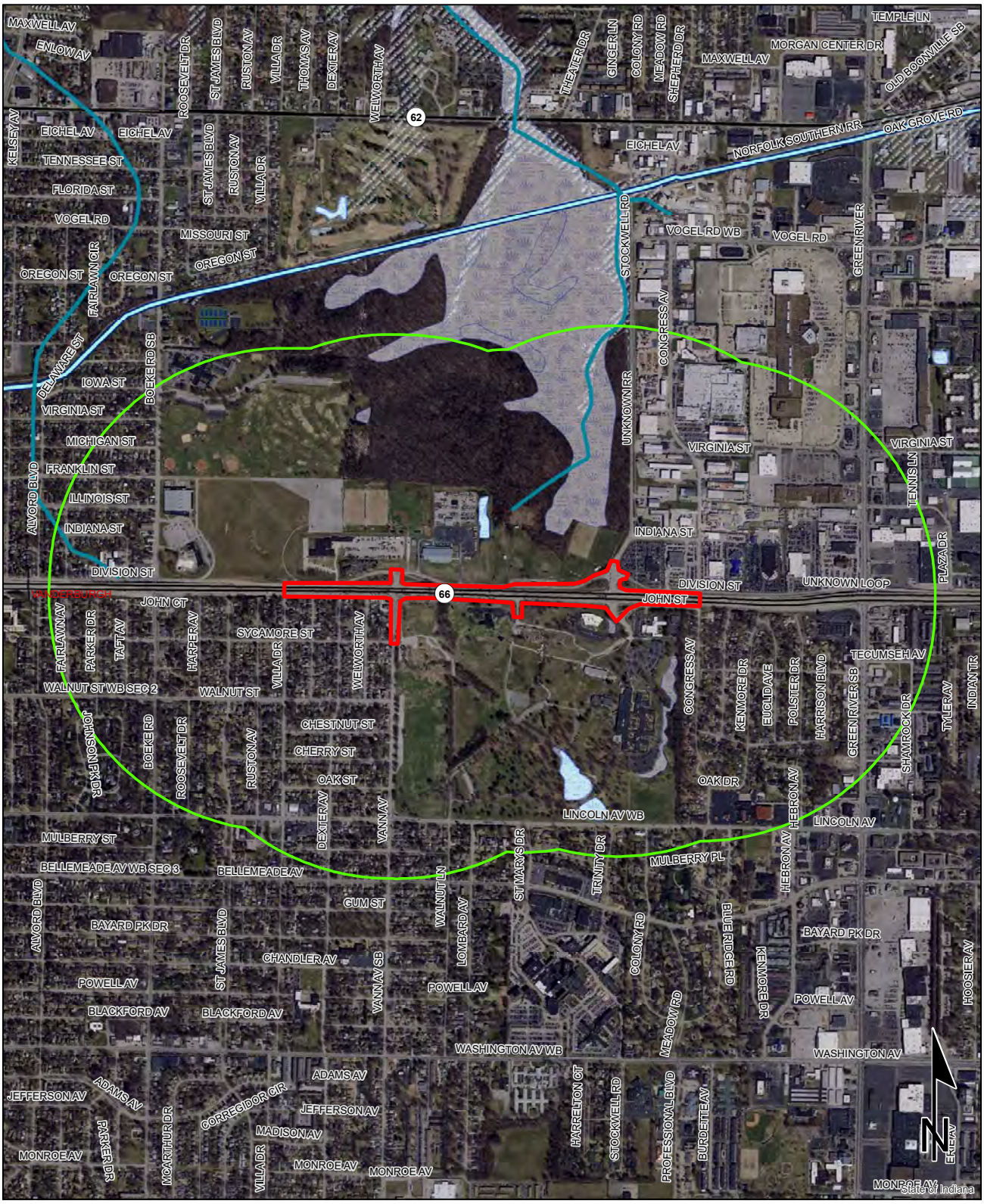
0.25 0.125 0 0.25 Miles

Sources:
Non Orthophotography Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

- Religious Facility
- Airport
- Cemeteries
- Hospital
- School
- Recreation Facility
- Pipeline
- Railroad
- Trails
- Managed Lands
- County Boundary
- Project Area
- Half Mile Radius
- Toll
- Interstate
- State Route
- US Route
- Local Road

Red Flag Investigation - Water Resources
SR 66, From 1.8 Miles East of US 41 to 2.7 Miles West of I-69
Des. No. 1900268 and 2000217, Corridor Improvement
Vanderburgh County, Indiana



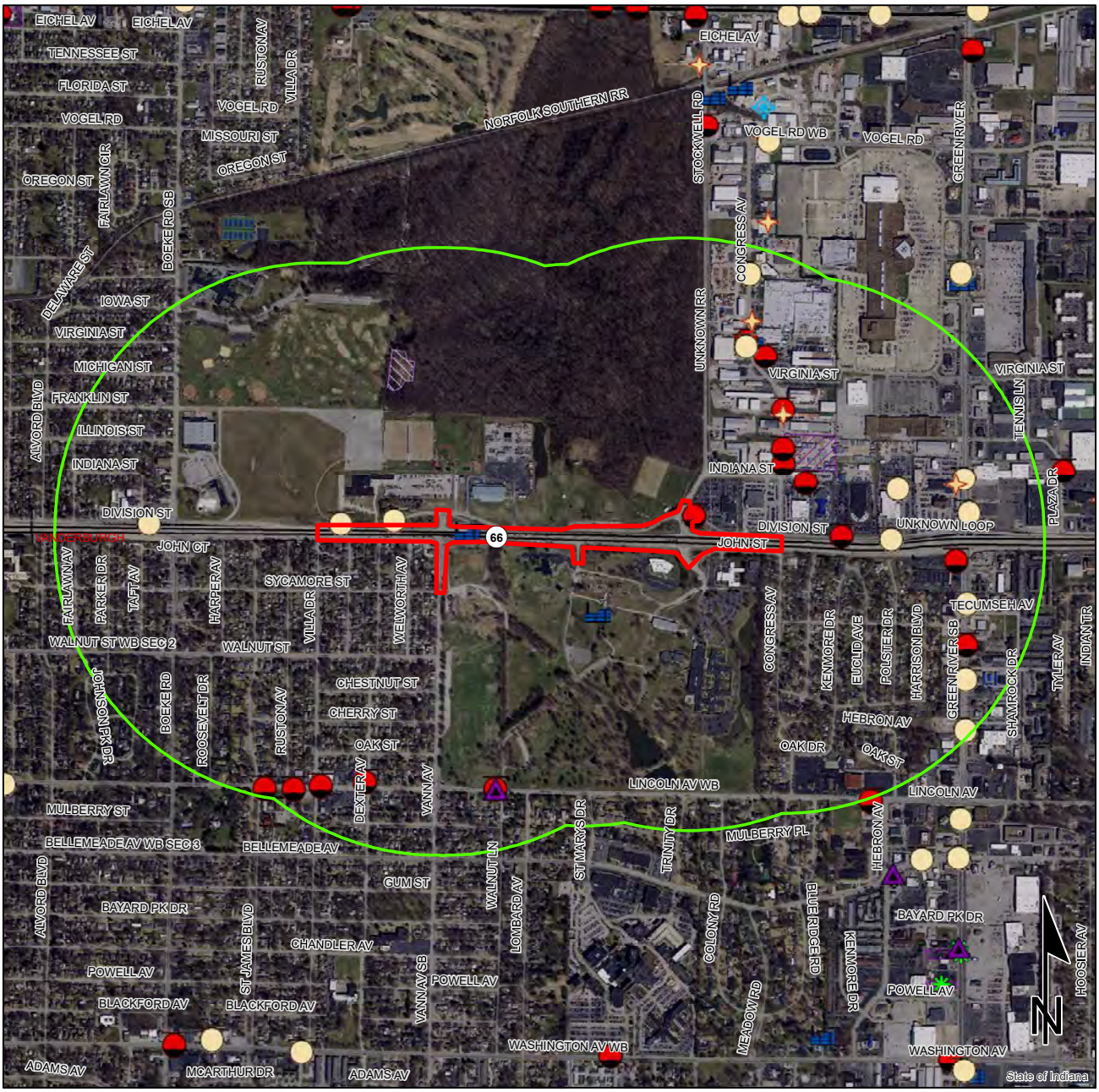
0.25 0.125 0 0.25 Miles

Sources:
Non Orthophotography Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

- NWI - Point
- Karst Spring
- NWI- Line
- Impaired_Stream_Lake
- NPS NRI listed
- River
- Canal Structure - Historic
- Canal Route - Historic
- Wetlands
- Lake
- Floodplain - DFIRM
- Cave Entrance Density
- Sinkhole Area
- Sinking-Stream Basin
- County Boundary
- Project Area
- Half Mile Radius
- Toll
- Interstate
- State Route
- US Route
- Local Road

Red Flag Investigation - Hazardous Material Concerns
SR 66, From 1.8 Miles East of US 41 to 2.7 Miles West of I-69
Des. No. 1900268 and 2000217, Corridor Improvement
Vanderburgh County, Indiana



- Brownfield
- RCRA Corrective Action Sites
- Confined Feeding Operation
- Notice_Of_Contamination
- Construction/Demolition Site
- Infectious/Medical Waste Site
- Leaking Underground Storage Tank
- Manufactured Gas Plant
- NPDES Facilites
- NPDES Pipe Locations
- Open Dump Waste Site
- RCRA Generator/TSD
- Restricted Waste Site
- Septage Waste Site
- Solid Waste Landfill
- State Cleanup Site
- Superfund
- Tire Waste Site
- Underground Storage Tank
- Voluntary Remediation Program
- Waste Transfer Station
- Institutional Controls
- County Boundary
- Project Area
- Half Mile Radius
- Toll
- Interstate
- State Route
- US Route
- Local Road

0.25 0.125 0 0.25 Miles

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

PARSONS

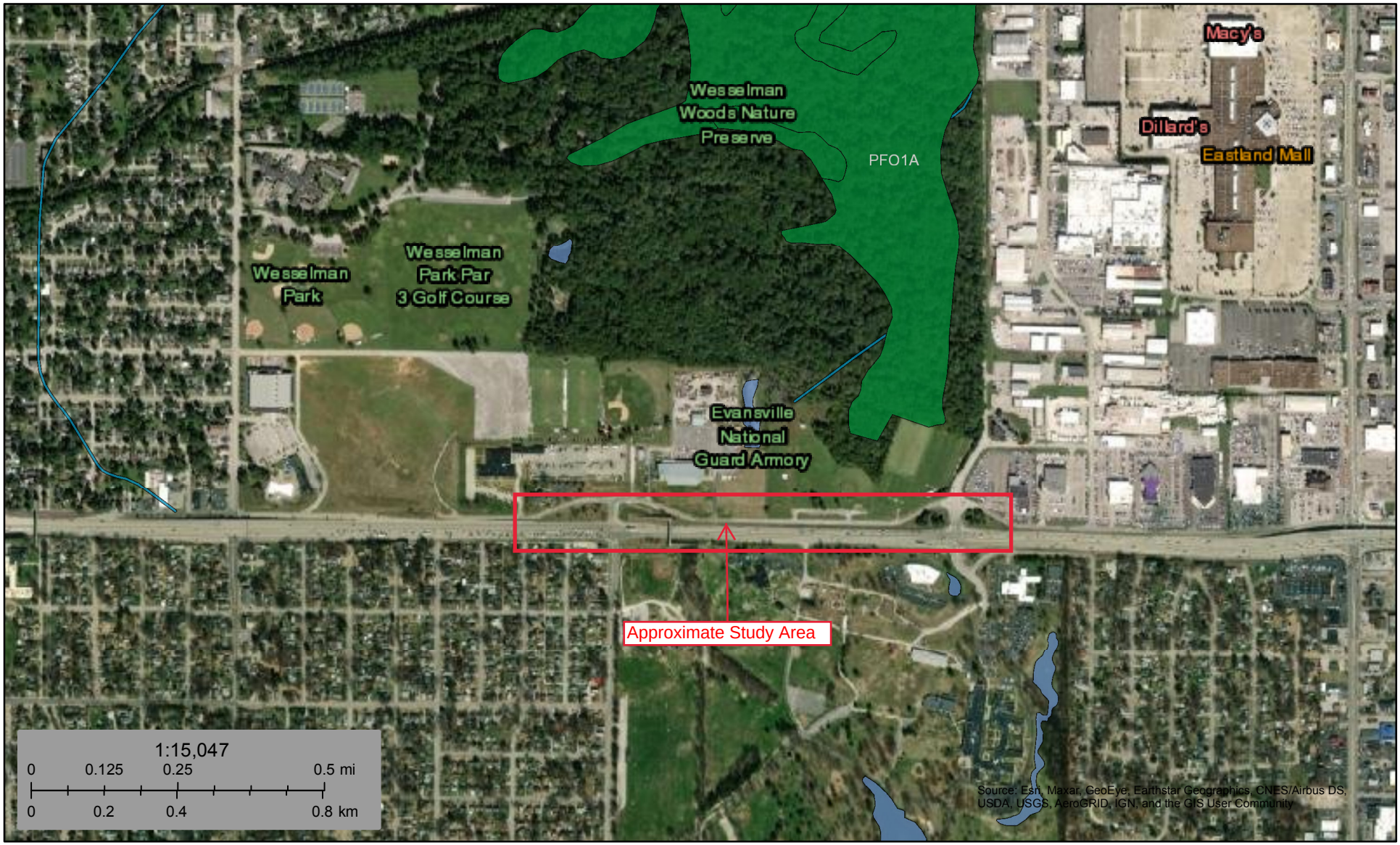
Appendix F

Water Resources

U.S. Fish and Wildlife Service

National Wetlands Inventory

Vann Ave./ Stockwell Rd. Intersections



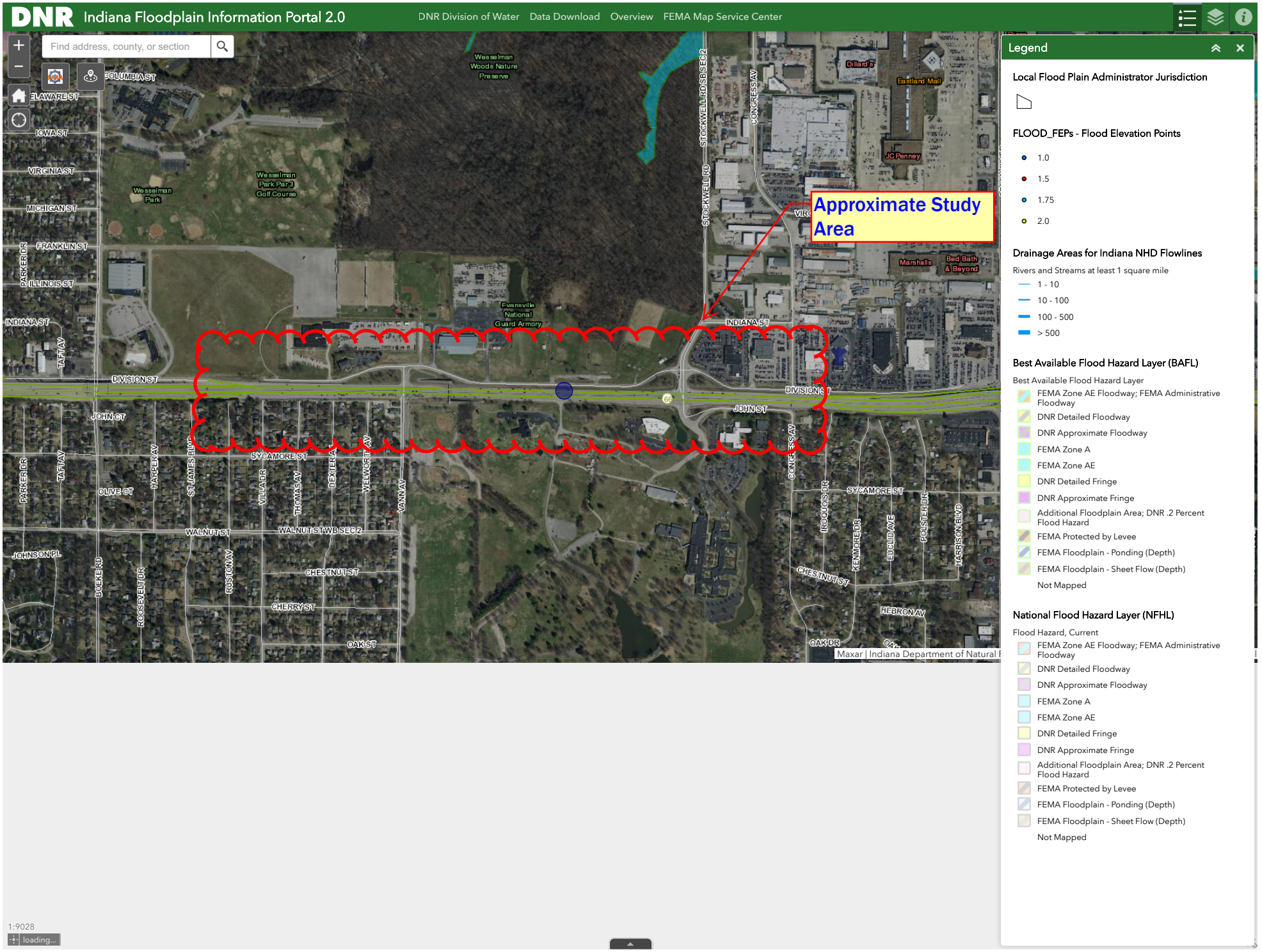
May 5, 2022

Wetlands

- Estuarine and Marine Deepwater
- Estuarine and Marine Wetland
- Freshwater Emergent Wetland
- Freshwater Forested/Shrub Wetland
- Freshwater Pond
- Lake
- Other
- Riverine

This map is for general reference only. The US Fish and Wildlife Service is not responsible for the accuracy or currentness of the base data shown on this map. All wetlands related data should be used in accordance with the layer metadata found on the Wetlands Mapper web site.

National Wetlands Inventory (NWI)
This page was produced by the NWI mapper


DNR Indiana Floodplain Information Portal 2.0
DNR Division of Water
Data Download
Overview
FEMA Map Service Center
Find address, county, or section
Approximate Study Area
Legend
Local Flood Plain Administrator Jurisdiction
FLOOD_FEPs - Flood Elevation Points
1.0
1.5
1.75
2.0
Drainage Areas for Indiana NHD Flowlines
Rivers and Streams at least 1 square mile
1 - 10
10 - 100
100 - 500
> 500
Best Available Flood Hazard Layer (BAFL)
Best Available Flood Hazard Layer
FEMA Zone AE Floodway; FEMA Administrative Floodway
DNR Detailed Floodway
DNR Approximate Floodway
FEMA Zone A
FEMA Zone AE
DNR Detailed Fringe
DNR Approximate Fringe
Additional Floodplain Area; DNR .2 Percent Flood Hazard
FEMA Protected by Levee
FEMA Floodplain - Ponding (Depth)
FEMA Floodplain - Sheet Flow (Depth)
Not Mapped
National Flood Hazard Layer (NFHL)
Flood Hazard, Current
FEMA Zone AE Floodway; FEMA Administrative Floodway
DNR Detailed Floodway
DNR Approximate Floodway
FEMA Zone A
FEMA Zone AE
DNR Detailed Fringe
DNR Approximate Fringe
Additional Floodplain Area; DNR .2 Percent Flood Hazard
FEMA Protected by Levee
FEMA Floodplain - Ponding (Depth)
FEMA Floodplain - Sheet Flow (Depth)
Not Mapped
Wesselman Woods Nature Preserve
Wesselman Park
Wesselman Park Par-3 Golf Course
Evansville National Guard Armory
COLUMBIA ST
DELAWARE ST
IOWA ST
VIRGINIA ST
MICHIGAN ST
FRANKLIN ST
ILLINOIS ST
INDIANA ST
DIVISION ST
JOHN CT
JOHN ST
SYCAMORE ST
OLIVE CT
WALNUT ST
WALNUT ST WB SEC 2
JOHNSON PL
CHESTNUT ST
CHERRY ST
OAK ST
HEBRON AV
STOCKWELL RD SB SEC 2
CONGRESS AV
Dillard's
Eastland Mall
JC Penney
Marshalls
Bed Bath & Beyond
Maxar | Indiana Department of Natural
1:9028
loading...

APPROVED
Justus McGill
8/1/22

Excerpts

PARSONS

Waters of the U.S. Report

Lloyd Expressway Corridor Improvement Project

Des. 1900308 (Lead)

Vanderburgh County, Indiana

Vincennes District

This report includes areas that are outside of the project study area and covered under separate environmental documents. Pertinent information is highlighted.

Prepared for:
Indiana Department of Transportation and Federal Highway Administration

July 18, 2022

INDIANA DEPARTMENT OF TRANSPORTATION INDOT

THE LLOYD4U

Parsons • 101 West Ohio Street, Suite 2121 • Indianapolis, Indiana 46204 • (317) 616-1000

WATERS OF THE U.S. REPORT

LLOYD EXPRESSWAY CORRIDOR IMPROVEMENT PROJECT

Vanderburgh County, Indiana

INDOT Designation (Des.) Number 1900308 (Lead)

Prepared By: Gregory R. Moushon, Principal Environmental Planner, PWS

July 18, 2022

I. PROJECT INFORMATION

FIELDWORK DATE:

Fieldwork for this report was conducted on June 15-18, 2021.

CONTRIBUTORS:

Greg Moushon, Principal Environmental Planner
Keaton Veldkamp, Environmental Planner
Isaac Kitchel, Engineering Intern

PROJECT LOCATION:

Evansville South and Newburgh Quadrangles
Sections 19 and 30, Township 6 South, Range 9 West
Sections 22, 23, 24, 25, and 26, Township 6 South, Range 10 West
Vanderburgh County, Indiana
Latitude/Longitude: 37.97673 North and 87.46430 West (east portion)
Latitude/Longitude: 37.97674 North and 87.50664 West (west portion)

PROJECT DESCRIPTION:

INDOT, in cooperation with the Federal Highway Administration (FHWA), proposes a corridor improvement project along SR 66/Lloyd Expressway (Lloyd Expressway) in the City of Evansville, Vanderburgh County, Indiana, also known as the "Lloyd 4 U" project. The bundled corridor improvement project includes a road reconstruction project (Lead Des. No. 1900308), seven intersection improvement projects (Des. Nos. 2000187, 1900263, 1900264, 1900268, 2000217, 1900292, and 1900317), and three bridge replacements (Des. Nos. 1600060, 1602258, 1500041).

This document covers the following intersection improvement projects on Lloyd Expressway:

DES. NOS. SUMMARY TABLE

Des. No.	Intersection	Location (Approximate)
1900268	Lloyd Expressway & Vann Avenue	1.8 miles east of US 41 and 3.2 miles west of I-69
2000217	Lloyd Expressway & Stockwell Road	2.3 miles east of US 41 and 2.7 miles west of I-69
1900292	Lloyd Expressway & Burkhardt Road	3.8 miles east of US 41 and 1.2 miles west of I-69
1900317	Lloyd Expressway & Cross Pointe Boulevard	4.7 miles east of US 41 and 0.3 mile west of I-69

Lloyd Expressway Intersections at Vann Avenue and Stockwell Road (Des. Nos. 1900268 & 2000217)

This project is located in Sections 22, 23, 26 and 27 of Township 6 South, Range 10 West, in the City of Evansville, Vanderburgh County. It is shown on the Evansville South and Newburgh, Indiana United States Geological Survey (USGS) topographical 7.5 minute quadrangle maps. The study area begins along Lloyd Expressway at Villa Drive and extends east to Congress Avenue. Study area limits also include Vann Avenue, from Sycamore Street to Division Street; Stockwell Road from John Street to approximately 100 feet north of Division Street; and Division Street from approximately 1,110 feet west of Stockwell Road to Stockwell Road.

The intersection with Vann Avenue is signalized. There are dedicated left-turn and right-turn lanes onto Vann Avenue in both the eastbound (EB) and westbound (WB) directions. Vann Avenue has five lanes at the intersection, consisting of northbound (NB) and southbound (SB) through, left-turn, and right-turn lanes, with discontinuous sidewalk, curb and gutter.

The Stockwell Road intersection is also signalized. Lloyd Expressway has dedicated right-turn slip lanes onto Stockwell Road in both the EB and WB directions, as well as left-turn lanes in each direction (two NB and one SB). Stockwell Road has six lanes at the intersection, consisting of two through lanes in each direction, two left-turn lanes, and a right-turn lane, with curb and gutter. There are no pedestrian facilities at the Stockwell Road intersection, including Division Street.

The recommended alternative at Lloyd Expressway and Vann Avenue would convert the existing signalized intersection to a right-in/right-out (RIRO) intersection. This would eliminate left-turns and NB/SB through traffic through this intersection.

The recommended alternative for Lloyd Expressway and Stockwell Road would convert the traditional signalized intersection to a hybrid Displaced Left-Turn (DLT) intersection that includes both a displaced left-turn and a boulevard left-turn. This would maintain all existing movements through the intersection.

Lloyd Expressway Intersections at Burkhardt Road and Cross Pointe Boulevard (Des. No. 1900292 & 1900317)

This project is located in Sections 24 and 25 of Township 6 South, Range 10 West, and Sections 19 and 30 of Township 6 South, Range 9 West, in the City of Evansville, Vanderburgh County. It is shown on the Newburgh, Indiana USGS topographical 7.5 minute quadrangle map. The study area begins along Lloyd Expressway approximately 85 feet west of Brentwood Drive and it terminates at the west side of the Lloyd Expressway/I-69 interchange. The study area also includes the entrance to Kimber Lane, Williamsburg Drive from Jamestown Court to Lloyd Expressway; Burkhardt Road from 265 feet north of Williamsburg Drive to Lloyd Crossing (Walmart entrance); Frontage Road (aka Division Street) from Lloyd Expressway to 150 feet north of Lloyd Expressway (Kohl's entrance); Eagle Crest Boulevard from approximately 140 feet west to 180 feet east of Cross Pointe Boulevard; Cross Pointe Boulevard from Eagle Crest Boulevard to Indiana Street; the SB I-69 off-ramp to WB Lloyd Expressway; and, the EB Lloyd Expressway on-ramp to SB I-69.

The intersection with Burkhardt Road is signalized. In addition to the through lanes, Lloyd Expressway has one right-turn and two left-turn lanes in both the EB and WB directions. Burkhardt Road is an undivided road with two through lanes, two left-turn lanes, a painted splitter, and one right-turn lane in each direction. There are no pedestrian facilities at this intersection.

The Lloyd Expressway and Cross Pointe Boulevard intersection is also signalized. In addition to the through lanes, Lloyd Expressway has one right-turn and one left-turn lane in each direction. Cross Pointe Boulevard is a five-lane road with through, left-turn, and right-turn lanes, with curb and gutter. North of Lloyd Expressway, it has a landscaped median and sidewalks that begin at the INDOT right-of-way (ROW) on the west side and at Division Street on the east side. South of Lloyd Expressway, it has a raised concrete median and no sidewalk.

The recommended alternative at the intersection of Lloyd Expressway and Burkhardt Road would convert the traditional signalized intersection to a DLT intersection with bypass right-turn lanes. This would maintain all existing movements through the intersection.

The recommended alternative for Lloyd Expressway and Cross Pointe Boulevard would convert the traditional signalized intersection to a DLT intersection with bypass right-turn lanes. This would maintain all existing movements through the intersection.

The recommended alternative would also modify the off-ramp from SB I-69 to WB Lloyd Expressway from a free-flowing intersection to a signalized intersection in order to allow exiting traffic the opportunity to get to SB Cross Pointe Boulevard.

II. OFFICE EVALUATION

METHODOLOGY:

The study area was based on the design alternatives evaluated for the National Environmental Policy Act (NEPA) document. The study area was approximately 81.4 acres in total size. The west study area was approximately 23.1 acres in size and the east study area was approximately 58.3 acres in size.

A desktop review of the study area was conducted to identify potential waterways (streams, wetlands, ponds, etc.). This included a review of historic and recent aerial photography for any areas with a water signature or a sharp change in vegetation. United States Geological Survey (USGS) topographic mapping, National Wetlands Inventory (NWI) mapping, National Hydrography Dataset (NHD) mapping, floodplain mapping, Natural Resources Conservation Service (NRCS) mapped soil units, and historic drainage mapping were also reviewed. Any noted items were flagged for follow-up field reconnaissance.

AERIAL PHOTOGRAPHY:

During review of current and historical aerial photography, several areas were identified within the study area that displayed potential wetland signatures associated with water ponding, darkened soils, and/or shifts in vegetation. Additional areas were noted adjacent to the study area. Each flagged area was investigated during field reconnaissance.

USGS MAPPING:

During review of USGS 7.5-minute series topographic mapping (Appendix B, pages 3 to 5), one perennial (solid blue-line) stream and two intermittent (dashed blue-line) streams were noted within the study area. The solid blue-line corresponds to a drainage along Burkhardt Road. This feature was not observed during the field investigation. The dashed blue-line streams correspond to Stockfleith Ditch and Nurenbern Ditch, both flowing to the north through the study area.

NWI AND FLOODPLAIN MAPPING:

During review of NWI and floodplain mapping (Appendix B, pages 7 to 26), no wetland polygon or wetland lines were noted within the study area. Four stormwater basins were located adjacent to the study area. The first stormwater basin was located south of the Lloyd Expressway and west of Stockwell Road (Appendix B, page 12). A second stormwater basin was located south of the Lloyd Expressway and west of Burkhardt Road (Appendix B, page 16). A third stormwater basin was located north of the Lloyd Expressway and west of Cross Pointe Boulevard (Appendix B, page 19). The fourth stormwater basin was located north of the Lloyd Expressway and east of Division Street near the I-69 interchange (Appendix B, page 23). Three NWI-mapped streams that correspond with Stockfleith Ditch, Nurenbern Ditch, and the drainage along Burkhardt Road were noted within the study area. The 100-year floodplain associated with Stockfleith Ditch and Nurenbern Ditch are mapped within a majority of the study area.

MAPPED SOIL UNITS AND NHD MAPPING:

The NRCS classifies soil types as follows: hydric (100%), predominantly hydric (66-99%), partially hydric (33-65%), predominantly non-hydric (1-32%), and not-hydric (0%). According to the Soil Survey Geographic (SSURGO) Database for Vanderburgh County, Indiana, the study area is comprised of hydric, predominantly hydric, predominantly not hydric, and

not hydric soil types (Appendix B, pages 27 to 46). The mapped soil units within the study area are summarized in Table 1 (Appendix A, page 1).

NHD was mapped on the soils background (Appendix B, pages 27 to 46). Two potential drainage features were identified within the study area. Roadside ditches were also noted within the study area. These areas were investigated during the field reconnaissance and described as follows:

- The mapped NHD drainage south of the Lloyd Expressway and west of Stockwell Road captures surface water while draining north and outfalls into the roadside ditch.
- The mapped NHD drainage south of the Lloyd Expressway and east of Stockwell Road captures surface water while draining west and outfalls into detention basins located on either side of Stockwell Road.

HISTORIC DRAINAGE:

The Vanderburgh County Soil Survey (USDA, 1976) was reviewed for historic drainage features within the study area. Three intermittent features were identified within the study area (Appendix B, pages 47 and 48). This stream is described as follows:

- The mapped intermittent historic drainage that crosses through the south leg of Vann Avenue within the west portion of the study was not observed during the field investigation. Residential neighborhoods and recreational ball fields are located there now.
- The mapped intermittent historic drainage that crosses the Lloyd Expressway at Burkhardt Road within the east portion of the study area was not observed during the field investigation.
- The mapped intermittent historic drainage that crosses the Lloyd Expressway at Cross Pointe Boulevard within the east portion of the study area was not observed during the field investigation.

WATERSHED:

The study area is located within one hydrologic unit code 12-digit (HUC 12) watershed: Kleymeyer Park-Pigeon Creek (051402020306).

III. FIELD RECONNAISSANCE

METHODOLOGY:

Parsons conducted a field investigation on June 15-18, 2021 to determine the presence of waterways, including streams, wetlands, lakes, and ponds, within the study area. The entire study area was reviewed for resources via a walking survey. All areas flagged during desktop review were investigated and documented. Resource maps showing all identified features are attached for reference (Appendix B, pages 49 to 68).

The OHWM of each stream was determined using a measuring tape. The OHWM was recorded outside of any structures. A hand-held GPS unit (Trimble Geo 7 Series) was used to collect the location of each identified stream. Qualitative assessments of stream quality were done within the study area.

The upstream drainage area for each stream was calculated using *StreamStats Version 4.6.2* (USGS, 2021), if available. Streamstats identified six potential streams with the study area (Appendix B, pages 69 to 74). Two of these streams correlated with Stockfleith Ditch and Nurenbern Ditch (Appendix B, pages 70 and 73). However, the other four streams were investigated during field reconnaissance but did not identify any features with OHWM or wetland characteristics

(Appendix B, pages 69, 71, 72, and 74). These streams have most likely been disturbed by development including, but not limited to, piping the streams underground.

Vegetation, soil, and hydrology data were collected using the methods described in the *Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Midwest Region (Version 2.0)* (USACE, 2010). Wetland indicator statuses for plants were obtained from the National Wetland Plant List, Version 3.5 (USACE, 2020). A hand-held GPS unit (Trimble Geo 7 Series) was used to collect the boundary of each identified wetland, as well as all data points. Data forms for each data point are included in this report for reference (Appendix D). The area for each wetland was calculated. A qualitative assessment of each wetland's quality was conducted, which included grading them (poor, average, or excellent) based on ecological function, size, species diversity, invasive species prevalence, and amount of disturbance.

Photographs were taken throughout the study area. This included photographs of each feature identified within the study area (Appendix C, pages 21 to 121). Photograph orientation maps are included for additional reference (Appendix C, pages 1 to 20).

All culverts, if safely able to be inspected, were visually inspected for the presence of bats, e.g., guano piles, staining, and any bat noises. No indication of bats were identified during the field reconnaissance.

STREAMS: These streams are not within the project study area. They are covered under a separate environmental document.

Field investigations resulted in the identification of three likely jurisdictional streams (890 linear feet over 0.075 acre) within the study area. These features are summarized in the Stream Summary Table (Table 2, Appendix A, page 1). No other features exhibiting OHWM were observed within the study area. None of the documented streams were listed as a Federal *Wild and Scenic River*, a *State Natural, Scenic, and Recreational River*, or on the Indiana Register's listing of *Outstanding Rivers and Streams*, nor were they located within two miles of any such resources.

Stockfleith Ditch

The Lloyd Expressway crosses over Stockfleith Ditch within the study area (Appendix B, page 56). This stream originates south of the Lloyd Expressway and flows north as an open-channel stream before entering a concrete culvert (no asset number) under the Lloyd Expressway. On the north side of the Lloyd Expressway, it then again becomes an open-channel stream. It exhibited a 4-foot wide and 6-inch deep OHWM outside of the influence of the structure. Approximately 181 linear feet of this stream lies within the study area. USGS StreamStats lists its upstream drainage area as approximately 0.18 square mile.

Stockfleith Ditch has a narrow riparian corridor along both of its banks consisting of a mixture of herbaceous and scrub-shrub vegetation. The substrate consisted of clay, silt, and cobble. Minor, intermittent flow with shallow pools was observed. No riffles were present. The stream exhibited sparse overhead canopy cover and minor bank erosion. The stream appeared to have been previously relocated and channelized but was very stable, running perpendicular to the Lloyd Expressway. Based on these observations, Stockfleith Ditch was classified as a poor-quality stream.

Stockfleith Ditch is shown on USGS 7.5-minute topographic mapping as an intermittent stream (Appendix B, page 3 to 5). This was confirmed based on mapping, historic aerials, and field observations. Stockfleith Ditch contributes intermittent flow to Crawford Brandeis Ditch. Crawford Brandeis Ditch flows north eventually outfalling into Pigeon Creek. Pigeon Creek is a traditional navigable waterway and tributary to the Ohio River (a traditionally navigable waterway). Because of this connectivity and the presence of an OHWM, this stream is likely a water of the U.S.

Unnamed Tributary (UNT) to Stockfleith Ditch

UNT to Stockfleith Ditch is located within the roadside ditch north of the Lloyd Expressway and west of Kimber Lane. The stream captures surface water from the upstream drainage area, the Lloyd Expressway, and the adjacent residential parking lot located to the north and flows west, eventually outfalling into Stockfleith Ditch (Appendix B, page 56). UNT to Stockfleith Ditch exhibited a 1.5-foot wide and 4-inch deep OHWM outside of the influence of any structures. Approximately

411 linear feet of this stream lies within the study area. USGS StreamStats does not identify its upstream drainage area. Therefore, it is presumed to be less than 1.0 square mile.

UNT to Stockfleith Ditch has a narrow riparian area comprised of herbaceous vegetation along both banks. Its substrate consisted of clay, muck, and riprap. Minimal flow and ponding were observed. No riffles were present. Based on these observations, UNT to Stockfleith Ditch was classified as a poor-quality stream.

UNT to Stockfleith Ditch is not shown on USGS 7.5-minute topographic mapping (Appendix B, pages 3 to 5). Based on mapping, historic aerials, and field observations, it is presumed that the stream does not have consistent flow year-round and is ephemeral. UNT to Stockfleith Ditch contributes ephemeral flow to Stockfleith Ditch, which is a tributary to Pigeon Creek (a traditionally navigable waterway). Because of this connectivity and the presence of an OHWM, this stream is likely a water of the U.S.

These streams are not within the project study area. They are covered under a separate environmental document.

Nurenbern Ditch

The Lloyd Expressway crosses over Nurenbern Ditch within the study area (Appendix B, pages 63 and 64). This stream originates south of the Lloyd Expressway and flows north as an open-channel stream before entering a metal culvert (INDOT asset CV-066-0.82-31.60) under the Lloyd Expressway. On the north side of the Lloyd Expressway, it once again becomes an open-channel stream. It exhibited a 6.5-foot wide and 12-inch deep OHWM outside of the influence of the structure. Approximately 298 linear feet of this stream lies within the study area. USGS StreamStats lists its upstream drainage area as approximately 0.32 square mile.

Nurenbern Ditch has a narrow riparian corridor along both of its banks consisting of herbaceous vegetation. The substrate consisted of silt, gravel, and cobble. Minor, intermittent flow with shallow pools and riffles was observed. The stream exhibited sparse overhead canopy cover and minor bank erosion. The stream appeared to have been previously relocated and channelized but was very stable, running perpendicular to the Lloyd Expressway. Based on these observations, Nurebern Ditch was classified as a poor-quality stream.

Nurenbern Ditch is shown on USGS 7.5-minute topographic mapping as an intermittent stream (Appendix B, pages 3 to 5). This was confirmed based on mapping, historic aerials, and field observations. Nurenbern Ditch contributes intermittent flow to Lockwood Ditch. Lockwood Ditch flows west into Crawford Brandeis Ditch. Crawford Brandeis Ditch flows north eventually outfalling into Pigeon Creek. Pigeon Creek is a traditional navigable waterway and tributary to the Ohio River (a traditionally navigable waterway). Because of this connectivity and the presence of an OHWM, this stream is likely a water of the U.S.

WETLANDS:

Sampling locations were determined by the presence or absence of hydrophytic vegetation and hydrology indicators. A total of twenty-three likely jurisdictional wetlands, totaling 2.042 acres, were identified within the study area. All of the identified wetlands were located within deep roadside ditches along the Lloyd Expressway or adjacent roadways. Sixteen of the identified wetlands are likely waters of the U.S. The remaining seven wetlands are likely waters of the State. However, INDOT will request USACE take jurisdiction over them. The Wetland Summary Table (Table 3, Appendix A, page 2) and Data Point Summary Table (Table 4, Appendix A, pages 4 and 5) summarize the data collected on these features. INDOT will seek concurrence on the jurisdiction of all wetlands from USACE and IDEM. A pre-jurisdictional determination form is attached for reference (Appendix E, pages 1 to 4).

Wetland 1

Wetland 1 is an emergent wetland that is approximately 0.099 acre in size. It is located within the roadside ditch along the northside of the Lloyd Expressway and approximately 650 feet west of Stockwell Road (Appendix B, pages 52 and 53). Wetland 1 had low species diversity and is located within INDOT's maintained right-of-way. Because of this, it was classified as a poor-quality wetland. Wetland 1 is entirely contained within the roadside ditch. It does not directly abut, nor is it

hydrologically connected to a water of the U.S. Therefore, Wetland 1 is likely a water of the State. However, INDOT is requesting USACE take jurisdiction over it.

The area associated with Data Point 1 IN (DP-1-IN) was evaluated because it exhibited hydrophytic vegetation. The herbaceous stratum was dominated by *Leersia virginica* (white grass, FACW, 25%). This point met the hydrophytic vegetation criterion because it passed the rapid test, dominance test, and prevalence index. The soil profile met the hydric soil criterion because it exhibited the Redox Dark Surface (F6) indicator. Four secondary indicators (Surface Soil Cracks [B6], Crayfish Burrows [C8], Geomorphic Position [D2], and FAC-Neutral Test [D5]) of wetland hydrology were observed. Since all three wetland criteria were met at DP-1-IN, this area was identified as Wetland 1.

Data Point 1 OUT (DP-1-OUT) was taken up-slope and west from DP-1-IN. The herbaceous stratum was dominated by *Poa pratensis* (Kentucky blue grass, FAC, 30%) and *Schedonorus arundinaceus* (tall false rye grass, FACU, 30%). This point did not meet the hydrophytic vegetation criterion. The soil profile did not meet the hydric soil criterion. Two secondary indicators (Surface Soil Cracks [B6] and Geomorphic Position [D2]) of wetland hydrology were observed. Since only one of the three wetland criteria was met at DP-1-OUT, this point was determined to be non-wetland. This data point helped establish the boundary of Wetland 1, which was determined based on changes in vegetation and topography.

Wetland 2

Wetland 2 is an emergent wetland that is approximately 0.006 acre in size. It is located within the roadside ditch along the northside of the Division Street and approximately 650 feet west of Stockwell Road (Appendix B, pages 52 and 53). Wetland 2 had low species diversity and is located within the City of Evansville's maintained right-of-way. Because of this, it was classified as a poor-quality wetland. Wetland 2 is entirely contained within the roadside ditch. It does not directly abut, nor is it hydrologically connected to a water of the U.S. Therefore, Wetland 2 is likely a water of the State. However, INDOT is requesting USACE take jurisdiction over it.

The area associated with Data Point 2 IN (DP-2-IN) was evaluated because it exhibited hydrophytic vegetation. The herbaceous stratum was dominated by *Carex lurida* (shallow sedge, OBL, 50%) and *Schoenoplectus tabernaemontani* (soft-stem club-rush, OBL, 20%). This point met the hydrophytic vegetation criterion because it passed the rapid test, dominance test, and the prevalence index. The soil profile met the hydric soil criterion because it exhibited the Redox Dark Surface (F6) indicator. One primary indicator (Saturation [A3]) and three secondary indicators (Crayfish Burrows [C8], Geomorphic Position [D2], and FAC-Neutral Test [D5]) of wetland hydrology were observed. Since all three wetland criteria were met at DP-2-IN, this area was identified as Wetland 2.

Data Point 2 OUT (DP-2-OUT) was taken up-slope and south from DP-2-IN. The herbaceous stratum was dominated by *Schedonorus arundinaceus* (tall false rye grass, FACU, 30%), *Sorghum halepense* (Johnson grass, FACU, 25%), and *Trifolium pratense* (red clover, FACU, 20%). This point did not meet the hydrophytic vegetation criterion. The soil profile did not meet the hydric soil criterion. No indicators of wetland hydrology were observed. Since none of the three wetland criteria were met at DP-2-OUT, this point was determined to be non-wetland. This data point helped establish the boundary of Wetland 2, which was determined based on changes in vegetation and topography.

Wetland 3

Wetland 3 is an emergent wetland that is approximately 0.026 acre in size. It is located within the roadside ditch along the southside of the Lloyd Expressway and approximately 1,060 feet west of Stockwell Road (Appendix B, page 52). Wetland 3 had low species diversity and is located within INDOT's maintained right-of-way. Because of this, it was classified as a poor-quality wetland. Wetland 3 is entirely contained within the roadside ditch. It does not directly abut, nor is it hydrologically connected to a water of the U.S. Therefore, Wetland 3 is likely a water of the State. However, INDOT is requesting USACE take jurisdiction over it.

The area associated with Data Point 3 IN (DP-3-IN) was evaluated because it exhibited hydrophytic vegetation. The herbaceous stratum was dominated by *Poa pratensis* (Kentucky blue grass, FAC, 75%) and *Juncus effusus* (lamp rush, OBL, 20%). This point met the hydrophytic vegetation criterion because it passed the dominance test and the prevalence

index. The soil profile met the hydric soil criterion because it exhibited the Redox Dark Surface (F6) indicator. Two primary indicators (Algal Mat or Crust [B4] and Oxidized Rhizospheres on Living Roots [C3]) and three secondary indicators (Crayfish Burrows [C8], Geomorphic Position [D2], and FAC-Neutral Test [D5]) of wetland hydrology were observed. Since all three wetland criteria were met at DP-3-IN, this area was identified as Wetland 3.

Data Point 3 OUT (DP-3-OUT) was taken up-slope and south from DP-3-IN. The herbaceous stratum was dominated by *Poa pratensis* (Kentucky blue grass, FAC, 35%), *Trifolium pratense* (red clover, FACU, 30%), and *Trifolium repens* (white clover, FACU, 20%). This point did not meet the hydrophytic vegetation criterion. The soil profile did not meet the hydric soil criterion. No indicators of wetland hydrology were observed. Since none of the three wetland criteria were met at DP-3-OUT, this point was determined to be non-wetland. This data point helped establish the boundary of Wetland 3, which was determined based on changes in vegetation and topography.

Wetland 4

Wetland 4 is an emergent wetland that is approximately 0.097 acre in size. It is located within the roadside ditch along the southside of the Lloyd Expressway and approximately 80 feet west of Stockwell Road (Appendix B, pages 52 and 54). Wetland 4 had low species diversity and is located within INDOT's maintained right-of-way. Because of this, it was classified as a poor-quality wetland. Wetland 4 is entirely contained within the roadside ditch. It does not directly abut, nor is it hydrologically connected to a water of the U.S. Therefore, Wetland 4 is likely a water of the State. However, INDOT is requesting USACE take jurisdiction over it.

The area associated with Data Point 4A IN (DP-4A-IN) was evaluated because it exhibited hydrophytic vegetation. The herbaceous stratum was dominated by *Poa pratensis* (Kentucky blue grass, FAC, 60%). This point met the hydrophytic vegetation criterion because it passed the dominance test and the prevalence index. The soil profile met the hydric soil criterion because it exhibited the Redox Dark Surface (F6) indicator. Two primary indicators (Algal Mat or Crust [B4] and Oxidized Rhizospheres on Living Roots [C3]) and three secondary indicators (Surface Soil Cracks [B6], Crayfish Burrows [C8], and Geomorphic Position [D2]) of hydrology were observed. Since all three wetland criteria were met at DP-4A-IN, this area was identified as Wetland 4.

Data Point 4A OUT (DP-4A-OUT) was taken up-slope and south from DP-4A-IN. The herbaceous stratum was dominated by *Poa pratensis* (Kentucky blue grass, FAC, 40%) and *Trifolium pratense* (red clover, FACU, 40%). This point did not meet the hydrophytic vegetation criterion. The soil profile did not meet the hydric soil criterion. No indicators of wetland hydrology were observed. Since none of the three wetland criteria were met at DP-4A-OUT, this point was determined to be non-wetland. This data point helped establish the boundary of Wetland 4, which was determined based on changes in vegetation and topography.

The area associated with Data Point 4B IN (DP-4B-IN) was evaluated because it exhibited hydrophytic vegetation. The herbaceous stratum was dominated by *Poa pratensis* (Kentucky blue grass, FAC, 70%) and *Carex lurida* (shallow sedge, FACW, 25%). This point met the hydrophytic vegetation criterion because it passed the dominance test and the prevalence index. The soil profile met the hydric soil criterion because it exhibited the Redox Dark Surface (F6) indicator. Four secondary indicators (Drainage Patterns [B10], Crayfish Burrows [C8], Geomorphic Position [D2], and FAC-Neutral Test [D5]) of hydrology were observed. Since all three wetland criteria were met at DP-4B-IN, this area was identified as Wetland 4.

Data Point 4B OUT (DP-4B-OUT) was taken up-slope and south from DP-4B-IN. The herbaceous stratum was dominated by *Poa pratensis* (Kentucky blue grass, FAC, 40%) and *Plantago lanceolata* (English plantain, FACU, 20%). This point did not meet the hydrophytic vegetation criterion. The soil profile did not meet the hydric soil criterion. No indicators of wetland hydrology were observed. Since none of the three wetland criteria were met at DP-4B-OUT, this point was determined to be non-wetland. This data point helped establish the boundary of Wetland 4, which was determined based on changes in vegetation and topography.

Wetland 5

Wetland 5 is an emergent wetland that is approximately 0.010 acre in size. It is located within the roadside ditch along the northside of the Division Street and approximately 600 feet west of Stockwell Road (Appendix B, page 53). Wetland 5 had low species diversity and is located within the City of Evansville's maintained right-of-way. Because of this, it was classified as a poor-quality wetland. Wetland 5 is entirely contained within the roadside ditch. It does not directly abut, nor is it hydrologically connected to a water of the U.S. Therefore, Wetland 5 is likely a water of the State. However, INDOT is requesting USACE take jurisdiction over it.

The area associated with Data Point 5 IN (DP-5-IN) was evaluated because it exhibited hydrophytic vegetation. The herbaceous stratum was dominated by *Carex lurida* (shallow sedge, OBL, 25%) and *Persicaria maculosa* (spotted lady's thumb, FACW, 25%), and *Schedonorus arundinaceus* (tall false rye grass, FACU, 20%). This point met the hydrophytic vegetation criterion because it passed the dominance test and the prevalence index. The soil profile met the hydric soil criterion because it exhibited the Depleted Matrix (F3) indicator. One primary indicator (Saturation [A3]) and three secondary indicators (Crayfish Burrows [C8], Geomorphic Position [D2], and FAC-Neutral Test [D5]) of wetland hydrology were observed. Since all three wetland criteria were met at DP-5-IN, this area was identified as Wetland 5.

Data Point 5 OUT (DP-5-OUT) was taken up-slope and south from DP-5-IN. The herbaceous stratum was dominated by *Schedonorus arundinaceus* (tall false rye grass, FACU, 60%) and *Plantago lanceolata* (English plantain, FACU, 20%). This point did not meet the hydrophytic vegetation criterion. The soil profile did not meet the hydric soil criterion. No indicators of wetland hydrology were observed. Since none of the three wetland criteria were met at DP-5-OUT, this point was determined to be non-wetland. This data point helped establish the boundary of Wetland 5, which was determined based on changes in vegetation and topography.

Wetland 6

Wetland 6 is an emergent wetland that is approximately 0.012 acre in size. It is located within the roadside ditch along the northside of the Division Street and approximately 55 feet east of Stockwell Road (Appendix B, page 53). Wetland 6 had low species diversity and is located within the City of Evansville's maintained right-of-way. Because of this, it was classified as a poor-quality wetland. Wetland 6 is entirely contained within the roadside ditch. It does not directly abut, nor is it hydrologically connected to a water of the U.S. Therefore, Wetland 6 is likely a water of the State. However, INDOT is requesting USACE take jurisdiction over it.

The area associated with Data Point 6 IN (DP-6-IN) was evaluated because it exhibited hydrophytic vegetation. The herbaceous stratum was dominated by *Persicaria maculosa* (spotted lady's-thumb, FACW, 70%). This point met the hydrophytic vegetation criterion because it passed the rapid test, dominance test and the prevalence index. The soil profile met the hydric soil criterion because it exhibited the Depleted Below Dark Surface (A11), Depleted Matrix (F3), and Redox Dark Surface (F6) indicators. Two primary indicators (Saturation [A3] and Algal Mat or Crust [B4]) and four secondary indicators (Surface Soil Cracks [B6], Crayfish Burrows [C8], Geomorphic Position [D2], and FAC-Neutral Test [D5]) of wetland hydrology were observed. Since all three wetland criteria were met at DP-6-IN, this area was identified as Wetland 6.

Data Point 6 OUT (DP-6-OUT) was taken up-slope and south from DP-6-IN. The herbaceous stratum was dominated by *Poa pratensis* (Kentucky blue grass, FAC 20%), *Schedonorus arundinaceus* (tall false rye grass, FACU, 20%), *Euphorbia maculata* (spotted sandmat, FACU, 20%), and *Persicaria maculosa* (spotted lady's-thumb, FACW, 15%). This point did not meet the hydrophytic vegetation criterion. The soil profile did not meet the hydric soil criterion. No indicators of wetland hydrology were observed. Since none of the three wetland criteria were met at DP-6-OUT, this point was determined to be non-wetland. This data point helped establish the boundary of Wetland 6, which was determined based on changes in vegetation and topography.

Wetland 7

Wetland 7 is an emergent wetland that is approximately 0.155 acre in size. It is located within the roadside ditch along the southside of the Lloyd Expressway and approximately 35 feet east of Stockwell Road (Appendix B, pages 54 and 55). Wetland 7 had low species diversity and is located within INDOT's maintained right-of-way. Because of this, it was classified as a poor-quality wetland. Wetland 7 is entirely contained within the roadside ditch. It does not directly abut, nor is it hydrologically connected to a water of the U.S. Therefore, Wetland 7 is likely a water of the State. However, INDOT is requesting USACE take jurisdiction over it.

The area associated with Data Point 7A IN (DP-7A-IN) was evaluated because it exhibited hydrophytic vegetation. The sapling/shrub stratum was dominated by *Acer saccharinum* (silver maple, FACW, 10%), *Acer rubrum* (red maple, FAC 5%), *Fraxinus pensylvanica* (green ash, FACW, 5%), and *Morus rubra* (red mulberry, FACU, 5%). The herbaceous stratum was dominated by *Leersia oryzoides* (rice cut grass, OBL, 90%). This point met the hydrophytic vegetation criterion because it passed the dominance test and the prevalence index. No soil sample was taken due to the presence of riprap substrate in this depressional wetland. The indicator for problematic soils was checked due to the presence of hydrophytic vegetation and wetland hydrology indicators. One primary indicator (Drift Deposits [B3]) and three secondary indicators (Crayfish Burrows [C8], Geomorphic Position [D2], and FAC-Neutral Test [D5]) of hydrology were observed. Since all three wetland criteria were met at DP-7A-IN, this area was identified as Wetland 7.

Data Point 7A OUT (DP-7A-OUT) was taken up-slope and southeast from DP-7A-IN. The herbaceous stratum was dominated by *Poa pratensis* (Kentucky blue grass, FAC, 40%) and *Trifolium pratense* (red clover, FACU, 30%). This point did not meet the hydrophytic vegetation criterion. The soil profile did not meet the hydric soil criterion. No indicators of wetland hydrology were observed. Since none of the three wetland criteria were met at DP-7A-OUT, this point was determined to be non-wetland. This data point helped establish the boundary of Wetland 7, which was determined based on changes in vegetation and topography.

The area associated with Data Point 7B IN (DP-7B-IN) was evaluated because it exhibited hydrophytic vegetation. The herbaceous stratum was dominated by *Juncus effusus* (lamp rush, OBL, 20%) and *Leersia virginica* (white grass, FACW, 20%). This point met the hydrophytic vegetation criterion because it passed the rapid test, dominance test, and the prevalence index. The soil profile met the hydric soil criterion because it exhibited the Redox Dark Surface (F6) indicator. One primary indicator (Algal Mat or Crust [B4]) and four secondary indicators (Surface Soil Cracks [B6], Crayfish Burrows [C8], Geomorphic Position [D2], and FAC-Neutral Test [D5]) of hydrology were observed. Since all three wetland criteria were met at DP-7B-IN, this area was identified as Wetland 7.

Data Point 7B OUT (DP-7B-OUT) was taken up-slope and east from DP-7B-IN. The herbaceous stratum was dominated by *Leersia virginica* (white grass, FACW, 30%). This point met the hydrophytic vegetation criterion because it passed the rapid test and dominance test. The soil profile did not meet the hydric soil criterion. One primary indicator (Algal Mat or Crust [B4]) and three secondary indicators (Surface Soil Cracks [B6], Geomorphic Position [D2], and FAC-Neutral Test [D5]) of hydrology were observed. Since only two of the three wetland criteria were met at DP-7B-OUT, this point was determined to be non-wetland. This data point helped establish the boundary of Wetland 7, which was determined based on changes in vegetation and topography.

Wetland 8

Located outside of project study area.

Wetland 8 is an emergent wetland that is approximately 0.012 acre in size. It is located within the roadside ditch along the southside of the Lloyd Expressway and approximately 75 feet east of Brentwood Drive (Appendix B, page 56). Wetland 8 had low species diversity and is located within INDOT's maintained right-of-way. Because of this, it was classified as a poor-quality wetland. Wetland 8 is entirely contained within the roadside ditch and is likely hydrologically connected to Stockfleith Ditch by surface flow through roadside ditch 5. Therefore, Wetland 8 is likely a water of the U.S.

The area associated with Data Point 8 IN (DP-8-IN) was evaluated because it exhibited hydrophytic vegetation. The herbaceous stratum was dominated by *Persicaria maculosa* (spotted lady's-thumb, FACW, 100%). This point met the

[D5]) of wetland hydrology were observed. Since all three wetland criteria were met at DP-23-IN, this area was identified as Wetland 23.

Data Point 23 OUT (DP-23-OUT) was taken up-slope and northwest from DP-23-IN. The herbaceous stratum was dominated by *Agrostis gigantea* (black bent, FACW, 50%) and *Juncus tenuis* (lesser poverty rush, FAC, 25%). This point met the hydrophytic vegetation criterion because it passed the dominance test. The soil profile did not meet the hydric soil criterion. Three secondary indicators (Crayfish Burrows [C8], Geomorphic Position [D2], and FAC-Neutral Test [D5] of wetland hydrology were observed. Since only two of the three wetland criteria were met at DP-23-OUT, this point was determined to be non-wetland. This data point helped establish the boundary of Wetland 23, which was determined based on changes in vegetation and topography.

NON-JURISDICTIONAL FEATURES:

Drainage Features

Nineteen roadside ditches (RSDs), totaling approximately 6,868 linear feet and one erosion feature (EF) totaling 328 linear feet within the study area, were investigated for potential water resources. The lengths of RSDs do not include the wetland lengths contained within the RSDs. Those that contained wetlands or UNTs were discussed earlier in this report. The remaining sections of the RSDs and the EF lacked either an OHWM or wetland characteristics. Therefore, they were considered to be non-jurisdictional features.

RSD-A is located along the northside of the Lloyd Expressway between Vann Avenue and Stockwell Road. It captures surface water between the Lloyd Expressway and Division Street. It runs west to east for approximately 1,224 linear feet. Wetland 1 is located in the central portion of RSD-A.

RSD-B is located along the northside of Division Street and west of Stockwell Road. It runs west to east for approximately 565 linear feet. Wetland 2 and Wetland 5 are located at the upstream end of RSD-B.

RSD-C is located along the southside of the Lloyd Expressway and east of Stockwell Road. It captures surface water between the Lloyd Expressway and John Street. It runs east to west for approximately 390 linear feet. Wetland 7 is located at the downstream end of RSD-C.

RSD-D is located along the northside of the Lloyd Expressway and west of Brentwood Drive. It captures surface water between the Lloyd Expressway and Division Street. It runs east to west for approximately 51 linear feet.

RSD-E is located along the southside of the Lloyd Expressway and east of Brentwood Drive. It runs east to west for approximately 325 linear feet. Wetland 8 is located at the downstream end of RSD-E.

RSD-F is located along the northside of the Lloyd Expressway and east of Brentwood Drive. It runs west to east for approximately 53 linear feet. Stockfleith Ditch is located at the downstream end of RSD-F.

RSD-G is located along the southside of the Lloyd Expressway and east of Brentwood Drive. It runs east to west for approximately 59 linear feet. Stockfleith Ditch is located at the downstream end of RSD-G.

RSD-H is located along the northside of the Lloyd Expressway and west of Burkhardt Road. It runs east to west for approximately 261 linear feet. Wetland 9 is located at the downstream end of RSD-H.

RSD-I is located along the southside of the Lloyd Expressway and west of Williamsburg Drive. It captures surface water between the Lloyd Expressway and the adjacent parking lot. It runs south to north for approximately 57 linear feet.

RSD-J is located along the southside of the Lloyd Expressway and east of Williamsburg Drive. It captures surface water between the Lloyd Expressway and the adjacent parking lot. It runs south to north for approximately 73 linear feet.

Geomorphic Position [D2], and FAC-Neutral Test [D5] of wetland hydrology were observed. Since only two of the three wetland criteria were met at UPL-1, this point was determined to be non-wetland.

Upland Data Point 2 (UPL-2) was taken within the flat roadside along the northside of the Lloyd Expressway and west of Crosspointe Boulevard (Appendix B, page 61). The herbaceous stratum was dominated by *Echinochloa crus-galli* (large barnyard grass, FACW, 60%) and *Persicaria maculosa* (spotted lady's thumb, FACW, 25%). This point met the hydrophytic vegetation criterion because it passed the rapid test and dominance test. The soil profile met the hydric soil criterion because it exhibited the Redox Dark Surface (F6) indicator. Only one secondary indicator (FAC-Neutral Test [D5] of wetland hydrology was observed. Since only two of the three wetland criteria were met at UPL-2, this point was determined to be non-wetland.

Upland Data Point 3 (UPL-3) was taken within a depresson along the northside of the Lloyd Expressway and west of Crosspointe Boulevard (Appendix B, page 61). No vegetation was obeserved within this dried mud depression. *Schedonorus arundinaceus* (tall false rye grass, 96%), *Cyperus acuminatus* (taper-tip sedge, 2%), and *Cyperus esculentus* (chufa, 2%) were observed around the boundary of the depression. This point did not meet the meet the hydrophytic vegetation criterion. No hydric soil indicators were observed. Two primary indicators (Algal Mat or Crust [B4] and Sparsely Vegetated Concave Surface [B8]) and two secondary indicators (Surface Soil Cracks [B6] and Crayfish Burrows [C8]) of hydrology was observed. Since only one of the three wetland criteria was met at UPL-3, this point was determined to be non-wetland.

IV. CONCLUSIONS

Based on the field investigations, the study area has features that are likely waters of the U.S. and waters of the State. Three likely jurisdictional streams (890 linear feet over 0.075 acre) were identified within the study area. A total of 23 likely jurisdictional wetlands totaling 2.042 acre were identified within the study area. Sixteen of the identified wetlands are likely waters of the U.S. totaling 1.637 acre, while seven of the identified wetlands is likely a water of the State totaling 0.405 acre. INDOT acknowledges that these seven wetlands are likely a water of the State. However, INDOT is requesting USACE take jurisdiction over it.

All jurisdictional waters of the U.S. are under the regulatory authority of USACE under Section 404 of the Clean Water Act. Every effort should be taken to avoid and minimize impacts to the resources outlined in this report. If impacts are necessary, then mitigation may be required. Impacts must be minimized before mitigation can be considered. The INDOT Environmental Services Division should be contacted immediately if impacts will occur. The final determination of jurisdictional waters is ultimately made by USACE and IDEM. This report is our best judgement based on the guidelines set forth by USACE.

A Preliminary Jurisdictional Determination Form is attached to the end of this report (Appendix E, pages 1 to 4).

V. REFERENCES

Cowardin, L.M, V. Carter, F.C. Golet, and E.T. LaRoe. 1979. *Classification of Wetlands and Deepwater Habitats of the United States.* US Department of the Interior, Fish and Wildlife Service, Washington DC.

U.S. Army Corps of Engineers 2020. National Wetland Plant List, version 3.5. *http://wetland-plants.usace.army.mil/;* U.S. Army Corps of Engineers. Engineer Research and Development Center. Cold Regions Research and Engineering Laboratory, Hanover, NH.

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United States Department of Agriculture, Soil Conservation Service. 1976. Soil Survey of Vanderburgh County, Indiana.

Ries, K.G., III, Newson J.K., Smith, M.J., Guthrie, J.D., Steeves, P.A., Haluska, T.L., Kolb, K.R., Thompson, R.F., Santoro, R.D., and Vraga, H.W., 2017, StreamStats, version 4: U.S. Geological Survey Fact 2017–3046, 4 p., https://doi.org/10.3133/fs20173046. [Supersedes USGS Fact Sheet 2008–3067.]

VI. ACKNOWLEDGEMENTS

This report has been prepared based on the best available information, interpreted in the light of the investigator's training, experience, and professional judgement in conformance with the 1987 Corps of Engineers Wetlands Delineation Manual, the appropriate regional supplement, the USACE Jurisdictional Determination Form Instructional Guidebook, and other appropriate agency guidelines.

7/18/2022

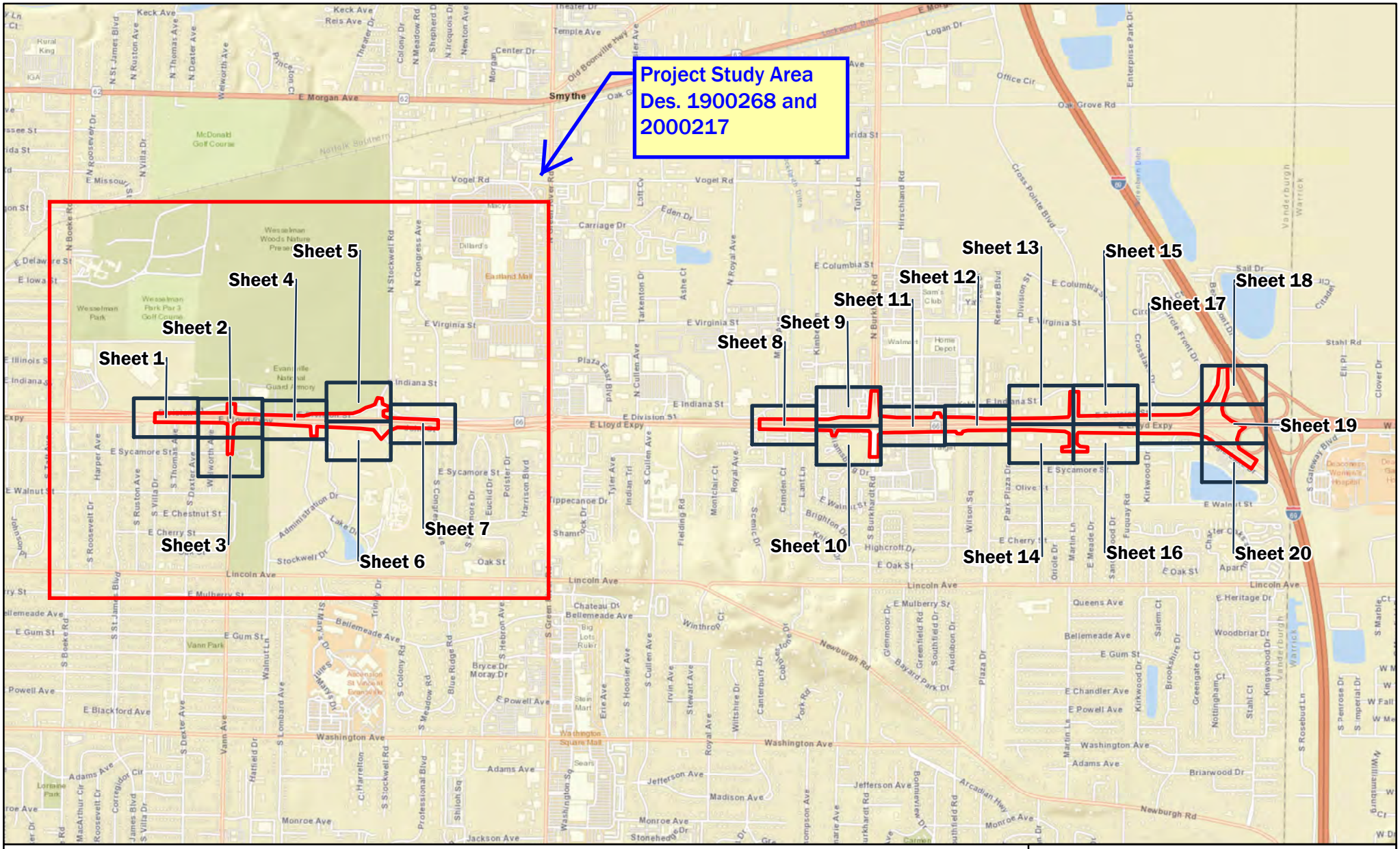
Gregory R. Moushon
Principal Environmental Planner, PWS
Parsons

Table 3: Wetland Summary Table

Name	Photograph Number	Latitude/ Longitude	Wetland Type (Palustrine)	Area (acre)	Quality	Likely Water of the U.S. (Y/N)	Isolated (Y/N) and Class I, II or III
Wetland 1	31, 33-36, 39, 40	37.97692/ -87.50628	Emergent	0.099	Poor	N*	N
Wetland 2	38, 54, 56-59	37.97705/ -87.50529	Emergent	0.006	Poor	N*	N
Wetland 3	43-46, 48, 49	37.97654/ -87.50665	Emergent	0.026	Poor	N*	N
Wetland 4	52, 53, 92, 94-100, 103	37.97647/ -87.50430	Emergent	0.097	Poor	N*	N
Wetland 5	59-62, 64	37.97705/ -87.50480	Emergent	0.010	Poor	N*	N
Wetland 6	82-87	37.97742/ -87.50197	Emergent	0.012	Poor	N*	N
Wetland 7	106, 108-114, 118-125	37.97638/ -87.50179	Emergent	0.155	Poor	N*	N
Wetland 8	145-150	37.97646/ -87.47989	Emergent	0.012	Poor	Y	N
Wetland 9	158-160, 162-165, 167	37.97685/ -87.47625	Emergent	0.053	Poor	Y	N
Wetland 10	183-186, 188, 189	37.97556/ -87.47429	Emergent	0.029	Poor	Y	N
Wetland 11	214-216, 218-220	37.97640/ -87.47285	Emergent	0.002	Poor	Y	N
Wetland 12	198, 199, 201-206	37.97688/ -87.47173	Emergent	0.049	Poor	Y	N
Wetland 13	209, 210, 212, 213, 224-226	37.97690/ -87.46950	Emergent	0.034	Poor	Y	N
Wetland 14	233-236, 238-240	37.97648/ -87.46735	Emergent	0.097	Poor	Y	N

Table 4: Data Point Summary Table

Data Point Name	Hydrophytic Vegetation (Y/N)	Hydric Soils (Y/N)	Wetland Hydrology (Y/N)	Wetland (Y/N)
DP-1-IN	Y	Y	Y	Y, Wetland 1
DP-1-OUT	N	N	Y	N
DP-2-IN	Y	Y	Y	Y, Wetland 2
DP-2-OUT	N	N	N	N
DP-3-IN	Y	Y	Y	Y, Wetland 3
DP-3-OUT	N	N	N	N
DP-4A-IN	Y	Y	Y	Y, Wetland 4
DP-4A-OUT	N	N	N	N
DP-4B-IN	Y	Y	Y	Y, Wetland 4
DP-4B-OUT	N	N	N	N
DP-5-IN	Y	Y	Y	Y, Wetland 5
DP-5-OUT	N	N	N	N
DP-6-IN	Y	Y	Y	Y, Wetland 6
DP-6-OUT	N	N	N	N
DP-7A-IN	Y	Y	Y	Y, Wetland 7
DP-7A-OUT	N	N	N	N
DP-7B-IN	Y	Y	Y	Y, Wetland 7
DP-7B-OUT	Y	N	Y	N
DP-8-IN	Y	Y	Y	Y, Wetland 8
DP-8-OUT	N	N	N	N
DP-9-IN	Y	Y	Y	Y, Wetland 9
DP-9-OUT	N	N	N	N
DP-10-IN	Y	Y	Y	Y, Wetland 10
DP-10-OUT	Y	N	N	N
DP-11-IN	Y	Y	Y	Y, Wetland 11
DP-11-OUT	N	N	N	N
DP-12-IN	Y	Y	Y	Y, Wetland 12
DP-12-OUT	N	N	N	N
DP-13-IN	Y	Y	Y	Y, Wetland 13
DP-13-OUT	N	N	N	N
DP-14-IN	Y	Y	Y	Y, Wetland 14
DP-14-OUT	N	N	N	N
DP-15-IN	Y	Y	Y	Y, Wetland 15
DP-15-OUT	Y	N	N	N
DP-16-IN	Y	Y	Y	Y, Wetland 16
DP-16-OUT	N	N	N	N
DP-17-IN	Y	Y	Y	Y, Wetland 17



Sources:
Non Orthophotography Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

Lloyd Expressway
Corridor Improvement Project
Vanderburgh County, Indiana
Index

Des. 1900308 (Lead)

Date: 10/11/2021

PARSONS

Created by: KDV

Study Area
Delineated Wetland
Data Point (OUT)
Data Point (IN)
Delineated Stream
Roadside Ditch
Erosion Feature
Feature Extends Off-Site

0 50 100
Feet

Sources:
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Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

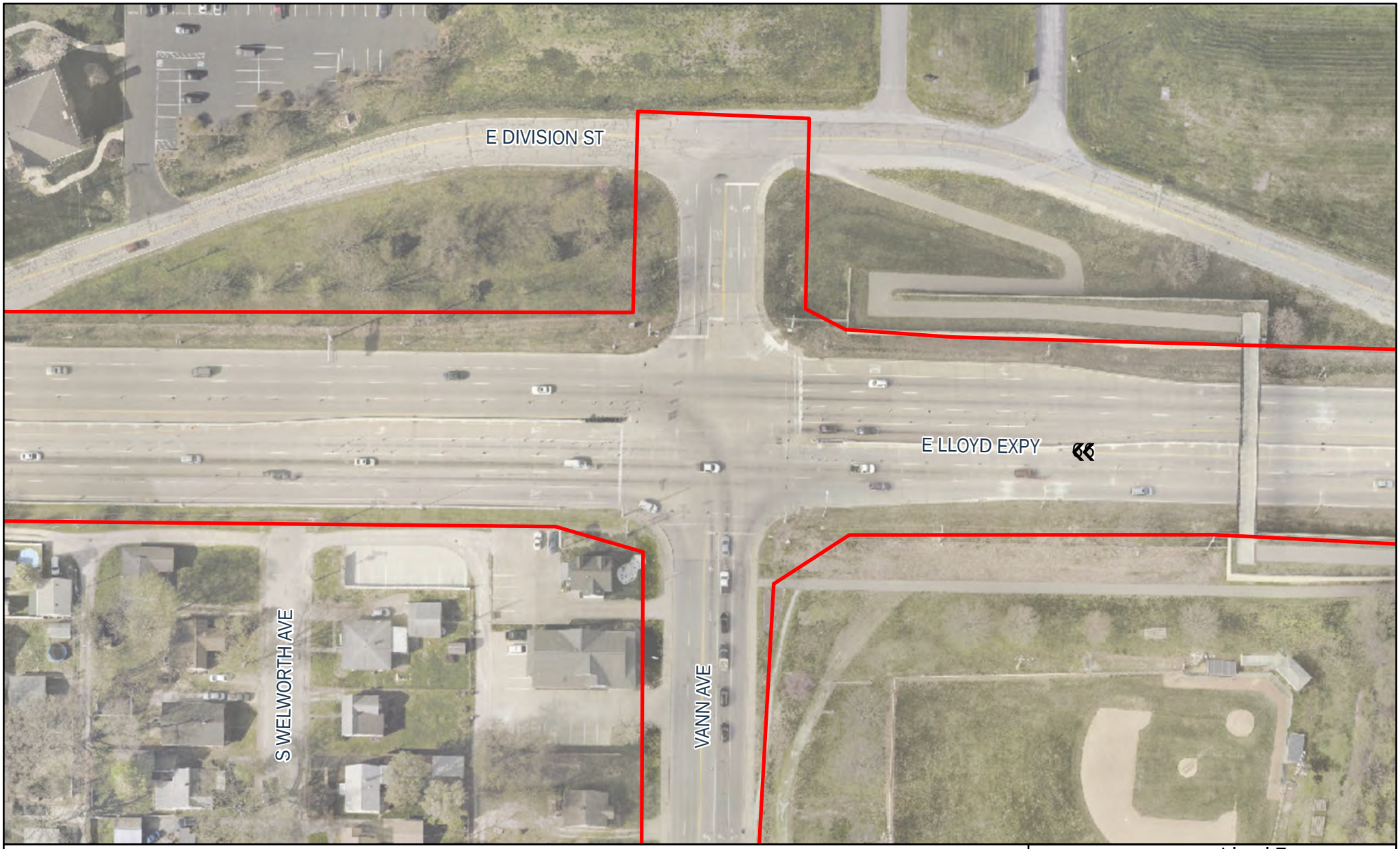
Lloyd Expressway
Corridor Improvement Project
Vanderburgh County, Indiana
Field-Identified Resources

Des. 1900308 (Lead)
Date: 7/12/2022

PARSONS

Created by: KDV

Sheet 1 of 20



Study Area
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Data Point (OUT)
Data Point (IN)
Delineated Stream
Roadside Ditch
Erosion Feature
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0 50 100
Feet

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Lloyd Expressway
Corridor Improvement Project
Vanderburgh County, Indiana
Field-Identified Resources

Des. 1900308 (Lead)
Date: 7/12/2022

PARSONS

Created by: KDV

Sheet 2 of 20

Study Area
Delineated Wetland
Data Point (OUT)
Data Point (IN)
Delineated Stream
Roadside Ditch
Erosion Feature
Feature Extends Off-Site

0 50 100
Feet

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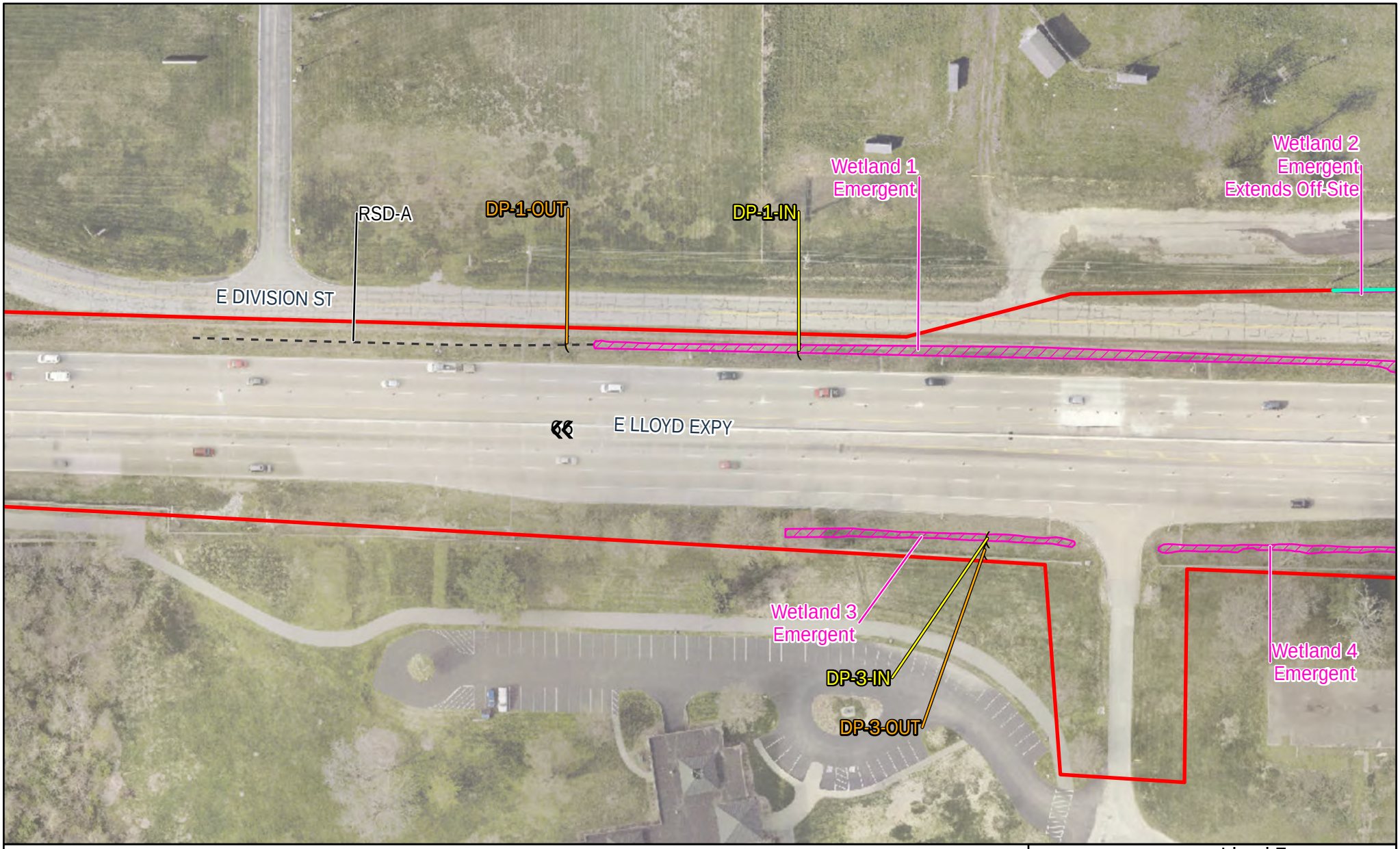
Lloyd Expressway
Corridor Improvement Project
Vanderburgh County, Indiana
Field-Identified Resources

Des. 1900308 (Lead)
Date: 7/12/2022

PARSONS

Created by: KDV

Sheet 3 of 20



Study Area
Delineated Wetland
(Data Point (OUT)
(Data Point (IN)
Delineated Stream
Roadside Ditch
Erosion Feature
Feature Extends Off-Site

0 50 100
Feet

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Lloyd Expressway
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Vanderburgh County, Indiana
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Des. 1900308 (Lead)
Date: 7/12/2022

PARSONS

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Sheet 4 of 20

Study Area
Delineated Wetland
Data Point (OUT)
Data Point (IN)
Delineated Stream
Roadside Ditch
Erosion Feature
Feature Extends Off-Site

0 50 100
Feet

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Lloyd Expressway
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PARSONS

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Sheet 5 of 20

Study Area
Delineated Wetland
Data Point (OUT)
Data Point (IN)
Delineated Stream
Roadside Ditch
Erosion Feature
Feature Extends Off-Site

0 50 100
Feet

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Lloyd Expressway
Corridor Improvement Project
Vanderburgh County, Indiana
Field-Identified Resources

Des. 1900308 (Lead)
Date: 7/12/2022

PARSONS

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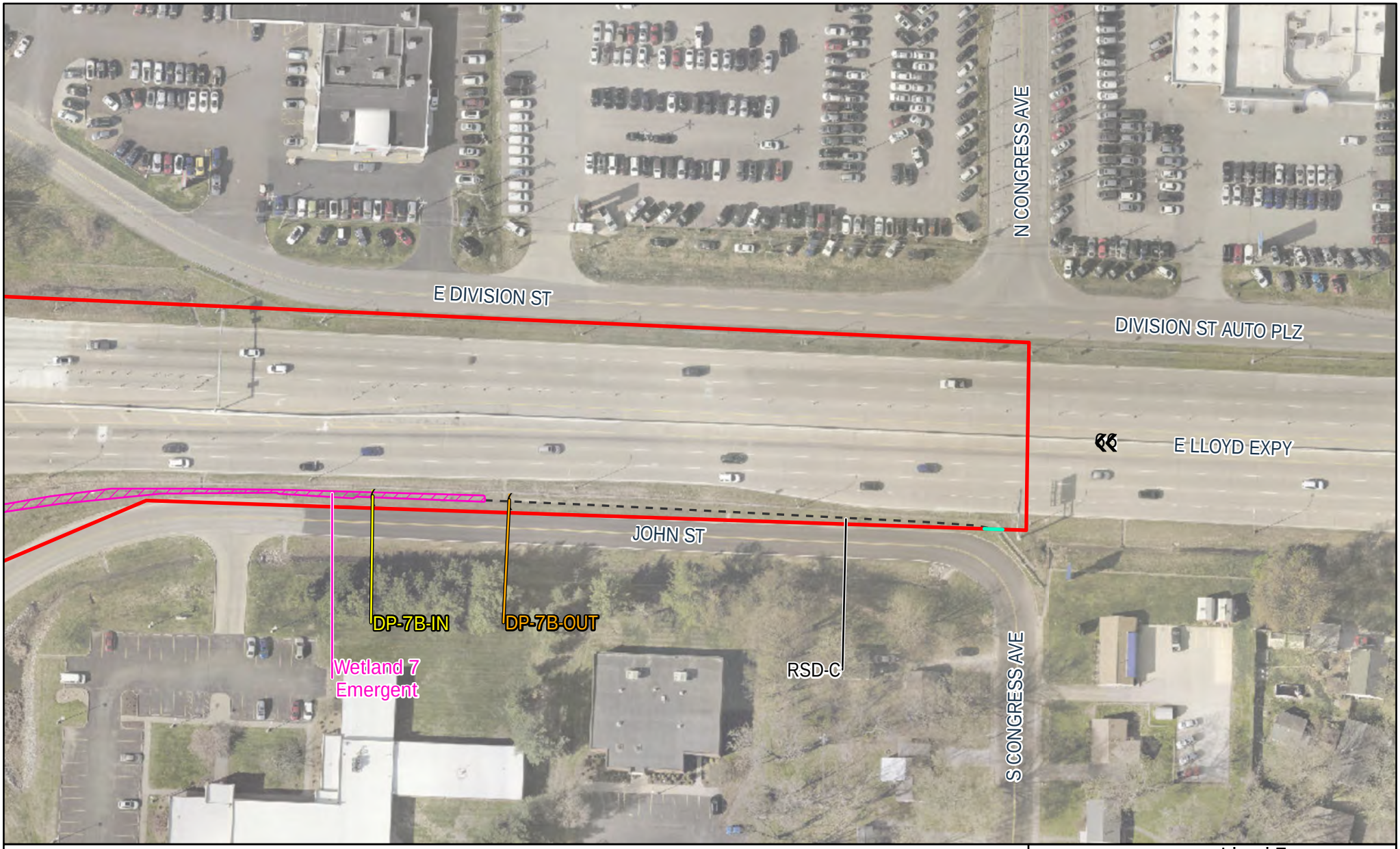
Sheet 6 of 20



Study Area
Delineated Wetland
(Data Point (OUT)
(Data Point (IN)
Delineated Stream
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Erosion Feature
Feature Extends Off-Site

0 50 100
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Lloyd Expressway
Corridor Improvement Project
Vanderburgh County, Indiana
Field-Identified Resources

Des. 1900308 (Lead)
Date: 7/12/2022

PARSONS

Created by: KDV

Sheet 7 of 20

Appendix 2 - PRELIMINARY JURISDICTIONAL DETERMINATION (PJD) FORM

BACKGROUND INFORMATION

A. REPORT COMPLETION DATE FOR PJD: July 18, 2022

B. NAME AND ADDRESS OF PERSON REQUESTING PJD: Gregory R. Moushon (Parsons), 101 West Ohio Street, Suite 2121, Indianapolis, IN 46204

C. DISTRICT OFFICE, FILE NAME, AND NUMBER:

D. PROJECT LOCATION(S) AND BACKGROUND INFORMATION:

INDOT, in cooperation with the Federal Highway Administration (FHWA), proposes a corridor improvement project along SR 66/Lloyd Expressway (Lloyd Expressway) in the City of Evansville, Vanderburgh County, Indiana, also known as the "Lloyd 4 U" project. The bundled corridor improvement project includes a road reconstruction project (Lead Des. No. 1900308), seven intersection improvement projects (Des. Nos. 2000187, 1900263, 1900264, 1900268, 2000217, 1900292, and 1900317), and three bridge replacements (Des. Nos. 1600060, 1602258, 1500041). This document includes improvements at Lloyd Expressway at Vann Avenue (Des.1900268), Stockwell Road (Des. 2000217), Burkhardt Road (Des. 1900292), and Cross Pointe Boulevard (Des.1900317). This project is located in Sections 22, 23, 26, and 27 of Township 6 South, Range 10 West, in the City of Evansville, Vanderburgh County. It is shown on the Evansville South and Newburgh, Indiana United States Geological Survey (USGS) topographical 7.5 minute quadrangle maps.

The recommended alternative at Lloyd Expressway and Vann Avenue would convert the existing signalized intersection to a right-in/right-out (RIRO) intersection. This would eliminate left-turns and NB/SB through traffic through this intersection. The recommended alternative for Lloyd Expressway and Stockwell Road would convert the traditional signalized intersection to a hybrid Displaced Left-Turn (DLT) intersection that includes both a displaced left-turn and a boulevard left-turn. This would maintain all existing movements through the intersection. The recommended alternative at the intersection of Lloyd Expressway and Burkhardt Road would convert the traditional signalized intersection to a DLT intersection with bypass right-turn lanes. This would maintain all existing movements through the intersection. The recommended alternative for Lloyd Expressway and Cross Pointe Boulevard would convert the traditional signalized intersection to a DLT intersection with bypass right-turn lanes. This would maintain all existing movements through the intersection.

(USE THE TABLE BELOW TO DOCUMENT MULTIPLE AQUATIC RESOURCES AND/OR AQUATIC RESOURCES AT DIFFERENT SITES)

State: IN County/parish/borough: Vanderburgh City: Evansville

Center coordinates of site (lat/long in degree decimal format):

Lat.: 37.97673 (east portion); 37.97674 (west portion) Long.: -87.46430 (east portion); -87.50664 (west portion)

Universal Transverse Mercator: NAD 1983 16S, 455509.96 E, 4203366.72 N

Name of nearest waterbody: Stockfleith Ditch

E. REVIEW PERFORMED FOR SITE EVALUATION (CHECK ALL THAT APPLY):

☐ Office (Desk) Determination. Date:

☐ Field Determination. Date(s):

Site Number	Latitude (decimal degrees)	Longitude (decimal degrees)	Estimated amount of aquatic resource in review area (acreage and linear feet, if applicable)	Type of aquatic resource (i.e., wetland vs. non-wetland waters)	Geographic authority to which the aquatic resource "may be" subject (i.e., Section 404 or Section 10/404)
Wetland 1	37.97692 N	87.50628 W	0.099 ac.	Wetland	Section 404
Wetland 2	37.97705 N	87.50529 W	0.006 ac.	Wetland	Section 404
Wetland 3	37.97654 N	87.50665 W	0.026 ac.	Wetland	Section 404
Wetland 4	37.97647 N	87.50430 W	0.097 ac.	Wetland	Section 404
Wetland 5	37.97705 N	87.50480 W	0.010 ac.	Wetland	Section 404
Wetland 6	37.97742 N	87.50197 W	0.012 ac.	Wetland	Section 404
Wetland 7	37.97638 N	87.50179 W	0.155 ac.	Wetland	Section 404
Wetland 8	37.97646 N	87.47989 W	0.012 ac.	Wetland	Section 404
Wetland 9	37.97685 N	87.47625 W	0.053 ac.	Wetland	Section 404
Wetland 10	37.97556 N	87.47429 W	0.029 ac.	Wetland	Section 404
Wetland 11	37.97640 N	87.47285 W	0.002 ac.	Wetland	Section 404
Wetland 12	37.97688 N	87.47173 W	0.049 ac.	Wetland	Section 404
Wetland 13	37.97690 N	87.46950 W	0.034 ac.	Wetland	Section 404
Wetland 14	37.97648 N	87.46735 W	0.097 ac.	Wetland	Section 404
Wetland 15	37.97695 N	87.46318 W	0.015 ac.	Wetland	Section 404
Wetland 16	37.97646 N	87.46347 W	0.114 ac.	Wetland	Section 404
Wetland 17	37.97699 N	87.46165 W	0.069 ac.	Wetland	Section 404
Wetland 18	37.97706 N	87.45972 W	0.027 ac.	Wetland	Section 404
Wetland 19	37.97743 N	87.45661 W	0.309 ac.	Wetland	Section 404
Wetland 20	37.97647 N	87.46102 W	0.390 ac.	Wetland	Section 404
Wetland 21	37.97643 N	87.45720 W	0.255 ac.	Wetland	Section 404
Wetland 22	37.97735 N	87.45458 W	0.062 ac.	Wetland	Section 404
Wetland 23	37.97644 N	87.45409 W	0.120 ac.	Wetland	Section 404
Stockfleith Ditch	37.97691 N	87.47866 W	181 l.f. (0.017 ac.)	Non-Wetland	Section 404
UNT to Stockfleith Ditch	37.97684 N	87.47772 W	411 l.f. (0.014 ac.)	Non-Wetland	Section 404
Nurenbern Ditch	37.97634 N	87.45950 W	298 l.f. (0.044 ac.)	Non-Wetland	Section 404

1) The Corps of Engineers believes that there may be jurisdictional aquatic resources in the review area, and the requestor of this PJD is hereby advised of his or her option to request and obtain an approved JD (AJD) for that review area based on an informed decision after having discussed the various types of JDs and their characteristics and circumstances when they may be appropriate.

2) In any circumstance where a permit applicant obtains an individual permit, or a Nationwide General Permit (NWP) or other general permit verification requiring "pre-construction notification" (PCN), or requests verification for a non-reporting NWP or other general permit, and the permit applicant has not requested an AJD for the activity, the permit applicant is hereby made aware that: (1) the permit applicant has elected to seek a permit authorization based on a PJD, which does not make an official determination of jurisdictional aquatic resources; (2) the applicant has the option to request an AJD before accepting the terms and conditions of the permit authorization, and that basing a permit authorization on an AJD could possibly result in less compensatory mitigation being required or different special conditions; (3) the applicant has the right to request an individual permit rather than accepting the terms and conditions of the NWP or other general permit authorization; (4) the applicant can accept a permit authorization and thereby agree to comply with all the terms and conditions of that permit, including whatever mitigation requirements the Corps has determined to be necessary; (5) undertaking any activity in reliance upon the subject permit authorization without requesting an AJD constitutes the applicant's acceptance of the use of the PJD; (6) accepting a permit authorization (e.g., signing a proffered individual permit) or undertaking any activity in reliance on any form of Corps permit authorization based on a PJD constitutes agreement that all aquatic resources in the review area affected in any way by that activity will be treated as jurisdictional, and waives any challenge to such jurisdiction in any administrative or judicial compliance or enforcement action, or in any administrative appeal or in any Federal court; and (7) whether the applicant elects to use either an AJD or a PJD, the JD will be processed as soon as practicable. Further, an AJD, a proffered individual permit (and all terms and conditions contained therein), or individual permit denial can be administratively appealed pursuant to 33 C.F.R. Part 331. If, during an administrative appeal, it becomes appropriate to make an official determination whether geographic jurisdiction exists over aquatic resources in the review area, or to provide an official delineation of jurisdictional aquatic resources in the review area, the Corps will provide an AJD to accomplish that result, as soon as is practicable. This PJD finds that there *"may be"* waters of the U.S. and/or that there *"may be"* navigable waters of the U.S. on the subject review area, and identifies all aquatic features in the review area that could be affected by the proposed activity, based on the following information:

SUPPORTING DATA. Data reviewed for PJD (check all that apply)

Checked items should be included in subject file. Appropriately reference sources below where indicated for all checked items:

- [x] Maps, plans, plots or plat submitted by or on behalf of the PJD requestor:
  Map: All attached mapping prepared by Parsons.
- [ ] Data sheets prepared/submitted by or on behalf of the PJD requestor.
  - [ ] Office concurs with data sheets/delineation report.
  - [ ] Office does not concur with data sheets/delineation report. Rationale: _____.
- [ ] Data sheets prepared by the Corps: _____.
- [ ] Corps navigable waters' study: _____.
- [x] U.S. Geological Survey Hydrologic Atlas: GIS Database, Indiana Map
  - [x] USGS NHD data.
  - [x] USGS 8 and 12 digit HUC maps.
- [x] U.S. Geological Survey map(s). Cite scale & quad name: 7.5-min., Evansville South and Newburgh Quadrangle.
- [x] Natural Resources Conservation Service Soil Survey. Citation: Vanderburgh County, 1976.
- [x] National wetlands inventory map(s). Cite name: USFWS NWI GIS Database.
- [ ] State/local wetland inventory map(s): _____.
- [ ] FEMA/FIRM maps: _____.
- [ ] 100-year Floodplain Elevation is: _____.(National Geodetic Vertical Datum of 1929)
- [x] Photographs: [x] Aerial (Name & Date): Orthos 2020.
  or [x] Other (Name & Date): Site Photos (June 15-18, 2021).
- [ ] Previous determination(s). File no. and date of response letter: _____.
- [ ] Other information (please specify): _____.

IMPORTANT NOTE: The information recorded on this form has not necessarily been verified by the Corps and should not be relied upon for later jurisdictional determinations.

_____	Craig R Moorhouse 7/18/2022
Signature and date of Regulatory staff member completing PJD	Signature and date of person requesting PJD (REQUIRED, unless obtaining the signature is impracticable)[1]

[1] Districts may establish timeframes for requestor to return signed PJD forms. If the requestor does not respond within the established time frame, the district may presume concurrence and no additional follow up is necessary prior to finalizing an action.